

Salisbury Pathways Committee

Sixty Fourth Meeting

Date and Time: Monday, April 11, 2022, at 5:30 p.m.

Location: Virtual via Zoom.

Present via Zoom: Natalia Smirnova, Kathy Trahan, Pat Hackett, Chris Williams, Gerry Stanton

Minutes:

Call to order -- 5:33 p.m.

1. Approval of the minutes of February 14, 2022.
Minutes were approved unanimously.

2. Status of the Connectivity Grant.

Application came back from DOT with 9 comments. Marc Mancini is working on answering the comments. **More comments may come after the review from Bridge Maintenance and Soil and Foundation Department.**

++++
From: Marc S. Mancini <mmancini@slrconsulting.com>
Sent: Thursday, March 31, 2022 2:29 PM
To: Curtis Rand <crand@salisburyct.us>
Subject: CTDOT District 4 Comments

Hi Curtis,
CTDOT District 4 reviewed our Route 44 Sidewalk & Pedestrian Bridge plan very quickly. We will address and await potential comments from the CTDOT Bridge Maintenance group before re-submitting.

Please see comments attached.

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Attached to these minutes are:

1. The comments from DOT.
2. The S.R. 44 Sidewalk FD Plan

Other items pertaining to the Connectivity grant:

- Property owners are all surveyed by the Town to get their approval for the sidewalk work. Everyone signed the agreement.
- DEEP is the longest holdup – waiting on the comments from Bridge Maintenance and Soil and Foundation Department.
- Hopefully break ground by late summer.

3. Status of Library to Salmon Kill Road Sidewalk.

Metcalf is ready to do the project. Matt Kiefer was asked to put the stakes for the sidewalk. Metcalf said mid-summer start. Pat Hackett is to communicate with Metcalf to find out when they are ready to start, then coordinate with Matt Kiefer.

4. Committee Priorities – revisit.

Natalia shared the minutes from September 15, 2014, where the following priorities were identified:

1. **“Connector”** – connecting villages of Salisbury and Lakeville.
2. **“Triangle with Horns”** – triangle that is comprised of Rt. 41 North, Cobble Road, Rt. 44 South with “horns” extending to Appalachian Trail parking lot on Rt. 41 and Lion’s Head community on Rt. 44.
3. The Hotchkiss School
4. Belgo Road
5. Around the Lake
6. Lime Rock

Committee discussed how these priorities are achieved and amended for the current year.

1. The **Connector** is progressing with the Connectivity grant. After that, we can do the connection from the Meadow to the Iron Bank. This will be the last stretch.
2. **Triangle with Horns:** achieve this work in small sections:
 - 2.1. From the Ambulance on Rt. 41 North to Grove Street including the parking lot for the employees of White Hart – connecting the Town Center to White Hart and parking.
 - 2.2. From Cobble Road down South to Grove Street by the Cemetery – connecting Sarum Village to Town.
 - 2.3. From Noble Horizons to the corner of Rt. 44 – safety issue for residents.
 - 2.4. Cross Rt. 44 to the Rail Trail – one crosswalk needed – safety issue.

Other priorities must wait as the Committee works on those two top priorities.

5. Citizens comments. No citizens were present.

Meeting adjourned at 6:12 p.m.

Minutes respectfully submitted by Natalia V. Smirnova, Secretary, on April 18, 2022.

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION



memorandum

subject: Town of Salisbury
Connectivity Grant
Route 44 from Lincoln City to Firehouse

date: March 29, 2022

to: Mr. Marc Mancini
Associate Transportation Engineer
SLR
Email: mmancini@slrconsulting.com

from: Aron J. Steeves
Special Services Manager
District IV
Bureau of Highway Operations

The following comments refer to the plan set entitled "CT S.R. 44 (Main St.) Sidewalk Improvements Salisbury, Connecticut CTDOT Project No. 0212-CCPI SLR Project No. 13039.00006 Final Design Plans" dated March 25, 2022, prepared by SLR.

No.	Comment	Inc.	Not Inc.
1.	<p>The proposed sidewalk runs adjacent to many trees/shrubs.</p> <ul style="list-style-type: none">• Where excavation is within 3-5 times the DBH (diameter breast height) of any tree within the project, a licensed Arborist, hired by the contractor will be required to be on site to inspect and properly prune any roots damaged.• Trees that had their root system damaged during excavation may require a fertilization and watering program.• Any tree that sustains more than 33% root damage may be required to be removed at contractor's expense.• At no time will excavated material be piled around the base of any tree or it's root system. <p>A written report of any roots damaged within the project limits will be submitted to the District Landscape Designer.</p>		
2.	It appears that the ramp on the northwest corner of Lincoln City Road should be a diagonal ramp. This ramp allows for the crossing of Lincoln City Road and Route 44.		
3	The horseshow shaped driveway at 227-229 Main Street appears to cross the property lines on both sides of the site. Please address.		
4	The pedestrian bridge has been sent to Bridge Maintenance for comments.		
5	Submit the pedestrian counter measure sheet to justify the RRFB at Brook Street and Route 44.		
6	See the marked up plans for the RRFB.		

TO:
FROM: Principal Engineer
DATE:

-2-

SUBJECT
Project No.:

7	Have all the requirements for the Connectivity Grant been satisfied?		
8	Has the PAL agreement been written?		
9	More comments may come after the review from Bridge Maintenance and Soil and Foundation Department.		

Gina Greenalch

cc: Aron Steeves – Ron Ferris –

C:\Users\jw\Documents\2014-06-DE\CAD\S8S-TITLE.DWG Layer1 Title S=DET
C:\Users\jw\Documents\2014-06-DE\CAD\S8S-TITLE.DWG Layer1 Title S=DET

MILONE & MACBROOM

99 Realty Drive
Cheshire, CT 06410
(203) 271-1173

BORING LOG

PROJECT: MAIN STREET SIDEWALK CONNECTION

LOCATION: SALISBURY, CONNECTICUT

PROJ. NO: 3039-06

CLIENT: TOWN OF SALISBURY

DATE: JUNE 9, 2020

BORING NO.: MM-1

CONTRACTOR: GENERAL BORINGS, INC.

FOREMAN: J. WYANT

INSPECTOR: J. MONTAGNO


GROUND SURFACE ELEVATION: ±117.0'

SHEET: 1 of 1

EQUIPMENT:		AUGER	CASING	SAMPLER	COREBRL	GROUNDWATER DEPTH (F.T.)			TYPE OF RIG:				
TYPE	HSA	-	SS	-	DATE	TIME	WATER DEPTH			TRUCK W/ AUTOHAMMER			
SIZE ID (IN.)	4 1/4	-	1 3/8	-	2020-06-09		±6.0'			RIG MODEL:			
HMR. WT (LB.)	-	-	140	-						DIEDRICH D-50			
HMR. FALL (IN.)	-	-	30	-									
SOIL AND ROCK CLASSIFICATION-DESCRIPTION													
BURMISTER SYSTEM (SOIL) U.S. CORPS OF ENGINEERS SYSTEM (ROCK)													
DEPTH (FT)	SAMPLE NUMBER	RECOVERY (IN)	BLOWS PER 6"							DEPTH (FT)	STRATUM DESCRIPTION	ELEV. (FT.)	Remarks
1	S-1	14	2	S-1: Loose, Top 6": Brown, fine to coarse SAND and SILT, trace fine Gravel, trace Rods. Bottom 6": Brown, fine to coarse SAND, some Silt, trace fine to coarse Gravel, trace Rods.						3.0'	TOPSOIL	714.0'	
2			3										
3	S-2	16	1	S-2: Top 7": Very loose, brown, fine to coarse SAND and SILT, trace fine Gravel, trace Rods. Bottom 9": Soft, brown, CLAY and SILT, little fine to coarse Gravel, little fine to coarse Sand, trace Rods.									
4			2										
5			3										
6	S-3	11	3	S-3: Loose, Top 5": Brown, fine to coarse SAND, some Clay and Silt, some fine to coarse Gravel. Bottom 6": Gray-brown, fine to coarse SAND, some Silt, trace fine to coarse Gravel.						6.0'	G.W.T. ▼ 711.0'		
7			2										
8	S-4	10	13	S-4: Top 6": Silty, gray-brown, CLAY and SILT, some fine to coarse Sand, trace fine Gravel. Bottom 4": Loose, gray-brown, fine to coarse GRAVEL, some Clayey SILT, little fine to coarse Sand.						8.0'		709.0'	
9			8										
10			4										
11	S-5	11	7	S-5: Medium dense, gray-brown, fine to coarse GRAVEL, some fine to coarse Sand, little Silt.						23.0'	GRAVEL WITH SAND & SILT	694.0'	1
12			8										
13			5										
14													
15			7										
16	S-6	15	6	S-6: Medium dense, gray-brown, fine to coarse GRAVEL, some fine to coarse Sand, little Silt.						23.0'	GRAVEL WITH SAND & SILT	694.0'	1
17			4										
18			10										
19													
20			7										
21	S-7	4	4	S-7: Medium dense, gray-brown, fine to coarse GRAVEL, little fine to coarse Sand, trace Silt.						23.0'	GRAVEL WITH SAND & SILT	694.0'	1
22			7										
			5										
Bottom of Exploration ±23.0'													

Remarks: 1. Auger refusal at ±23.0'.

NON-PLASTIC		PLASTIC		SAMPLE TYPE		PROPORTIONS	
N = 0 - 4 = VERY LOOSE	N = 0 - 2 = VERY SOFT	C = ROCK CORE	UP = UNDISTURBED	8 = SPLIT SPON	UP = UNDISTURBED PISTON	UT = UNDISTURBED THINWALL	UT = UNDISTURBED THINWALL
4 - 10 = LOOSE	2 - 4 = SOFT						
10 - 30 = MEDIUM DENSE	4 - 8 = MEDIUM						
30 - 50 = DENSE	8 - 15 = STIFF						
50+ = VERY DENSE	15 - 30 = VERY STIFF						
	30+ = HARD						

BORING LOG													
<div><div>MILONE & MACBROOM</div><div>99 Realty Drive Cheshire, CT 06410 (203) 271-1173</div></div>			<div>PROJECT: MAIN STREET SIDEWALK CONNECTION</div> <div>LOCATION: SALISBURY, CONNECTICUT</div> <div>PROJ. NO: 3039-06</div> <div>CLIENT: TOWN OF SALISBURY</div> <div>DATE: JUNE 9, 2020</div>				<div>BORING NO.: MM-2</div> <div>CONTRACTOR: GENERAL BORINGS, INC.</div> <div>FOREMAN: J. WYANT</div> <div>INSPECTOR: J. MONTAGNO</div> <div>GROUND SURFACE ELEVATION: ±718.0'</div>				<div>SHEET: 1 of 2</div>		
EQUIPMENT:		AUGER	CASING	SAMPLER	COREBRL	GROUNDWATER DEPTH (FT.)				TYPE OF RIG:			
TYPE	HSA	-	SS	-	DATE	TIME	WATER DEPTH				TRUCK W/ AUTOHAMMER		
SIZE ID (IN.)	4 1/4	-	1 3/8	-	2020-06-09		±7.0'				RIG MODEL:		
HMR. WT (LB.)	-	-	140	-							DIEDRICH D-50		
HMR. FALL (IN.)	-	-	30	-									
SOIL AND ROCK CLASSIFICATION-DESCRIPTION													
BURMISTER SYSTEM (SOIL) U.S. CORPS OF ENGINEERS SYSTEM (ROCK)													
DEPTH (FT.)	SAMPLE NUMBER	RECOVERY (IN)	BLOWS PER 6"							DEPTH (FT.)	STRATUM DESCRIPTION	ELEV. (FT.)	Remark
1	S-1	2	4	S-1: Loose, brown, fine to coarse SAND, some Silt, little fine Gravel, trace Roots.							TOPSOIL		
			4										
			5										
			3										
2			7	S-2: Loose, brown, fine to coarse SAND, some Silt, some fine to coarse Gravel, trace Roots.						2.0'		718.0'	
3	S-2	6	4										
			3										
4			3										
											FILL		
5			6	S-3: Medium dense, Top 7": Brown, fine to coarse SAND, some Silt, little fine Gravel, trace Debris (e.g., brick).									
6	S-3	18	13	Bottom 11": Gray-brown, fine to coarse SAND, some Silt, some fine to coarse Gravel.									
			7										
7			5	S-4: Medium dense, Top 6": Brown, fine to coarse SAND, some Silt, some fine to coarse Gravel.						7.0'	G.W.T. ▼	711.0'	
			14										
8	S-4	12	7	Bottom 4": Gray-brown, fine to coarse GRAVEL, little fine to coarse SAND, little Silt.						8.5'		709.5'	
			10										
9													
10			29	S-5: Very dense, Top 4": Brown, fine to coarse GRAVEL, some fine to coarse Sand, some Silt.							GRAVEL WITH SAND & SILT		
11	S-5	20	28	Middle 13": Gray, fine to coarse SAND, some fine to coarse Gravel, some Silt.									
			23	Bottom 3": Gray, fine to coarse GRAVEL, some fine to coarse SAND, some Silt.									
12			17										
13													
14													
15			18										
16	S-6	5	28	S-6: Dense, gray-brown, fine to coarse GRAVEL, little fine to coarse Sand, trace Silt.									
			14										
17			13										
18													
19													
20			7								SILT WITH GRAVEL & SAND		
21	S-7	10	4	S-7: Loose, Top 5": Gray, fine to coarse GRAVEL, some fine to coarse Sand, little Silt.									
			5	Bottom 5": Gray, SILT, some fine to coarse Sand, some fine to coarse Gravel.									
22			7										
			5										

Remarks:

NON-PLASTIC

N = 0 - 4 = VERY LOOSE

4 - 10 = LOOSE

10 - 30 = MEDIUM DENSE

30 - 50 = DENSE

50+ = VERY DENSE

PLASTIC

N = 0 - 2 = VERY SOFT

2 - 4 = SOFT

4 - 8 = MEDIUM

8 - 15 = STIFF

15 - 30 = VERY STIFF

30+ = HARD

SAMPLE TYPE

C = ROCK CORE

S = SPLIT SPON

UP = UNDISTURBED PISTON

UT = UNDISTURBED THINWALL

PROPORTIONS

trace = <10%


little = 10% - 20%

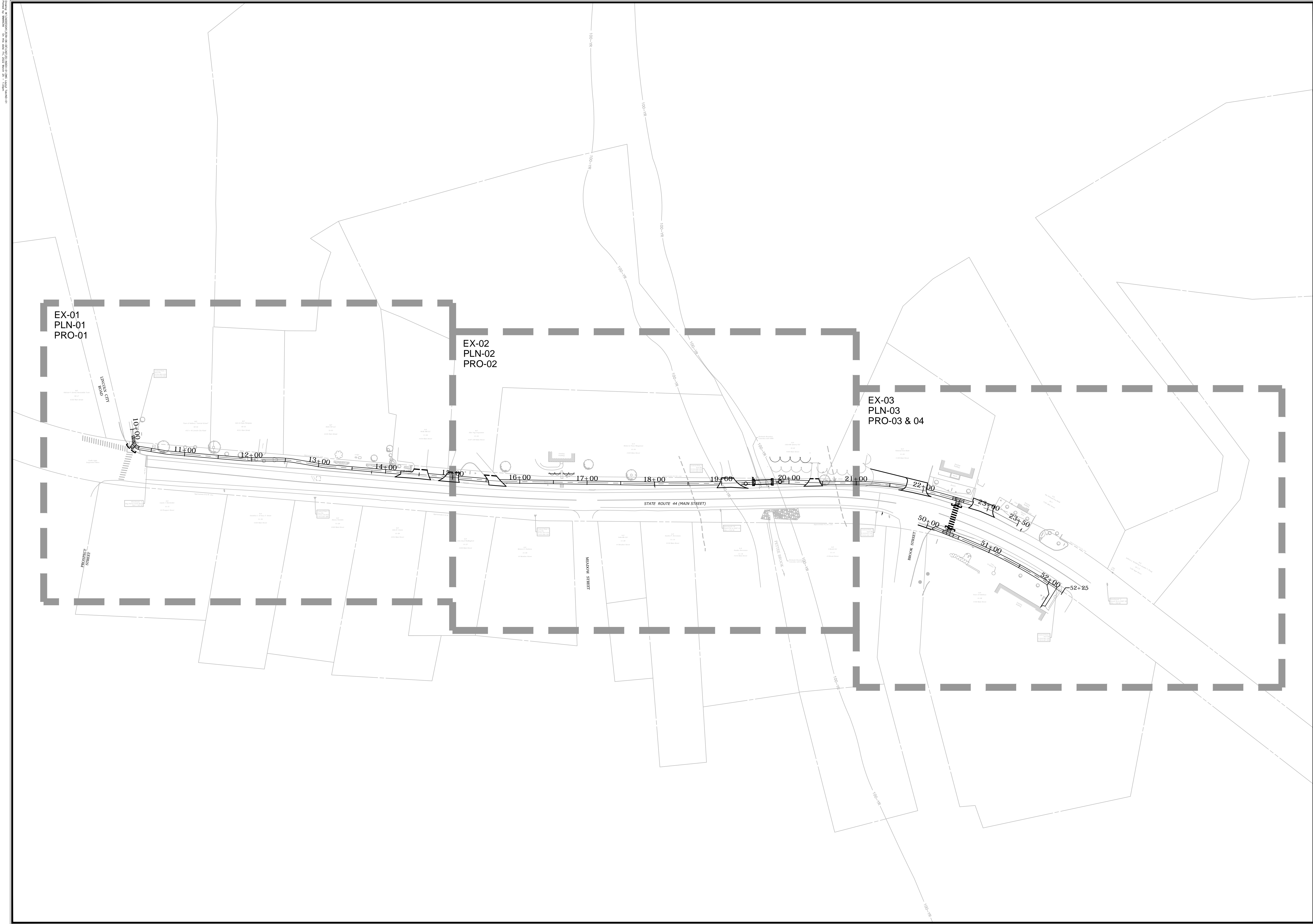
some = 20% - 35%

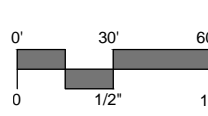
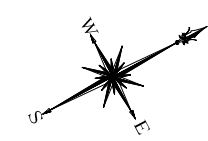
and = 35% - 50%


NOTES

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2. THE LOCATIONS OF THE BORINGS WERE DETERMINED BY TAPING/PACING FROM EXISTING SITE FEATURES. THESE LOCATIONS SHOULD BE CONSIDERED ACCURATE ONLY TO THE DEGREE IMPLIED BY THE METHOD USED.
3. THE LOCATIONS OF THE BORINGS ARE SHOWN ON SHEET EX-02.

<div>SLR 99 REALTY DRIVE CHESHIRE, CT 06410 TEL: 203.261.8800 WWW.SLRCONSULTING.COM</div>				
DESCRIPTION	DATE	BY		
BORING LOGS				
CT S.R. 44 (MAIN ST.) SIDEWALK IMPROVEMENTS				
CT S.R. 44 (MAIN ST.) SALISBURY, CONNECTICUT				
MSM DESIGNED	MSM DRAWN	AAC CHECKED		
NTS				
SCALE				
MARCH 25, 2022				
DATE				
13039.00006				
PROJECT NO.				
BOR-01				
DWG NO.				
02				
SHEET NO.				







99 REALTY DRIVE
SALISBURY, CT 06410
203.271.1771
SLRCONSULTING.COM

DESCRIPTION	DATE	BY

INDEX PLAN

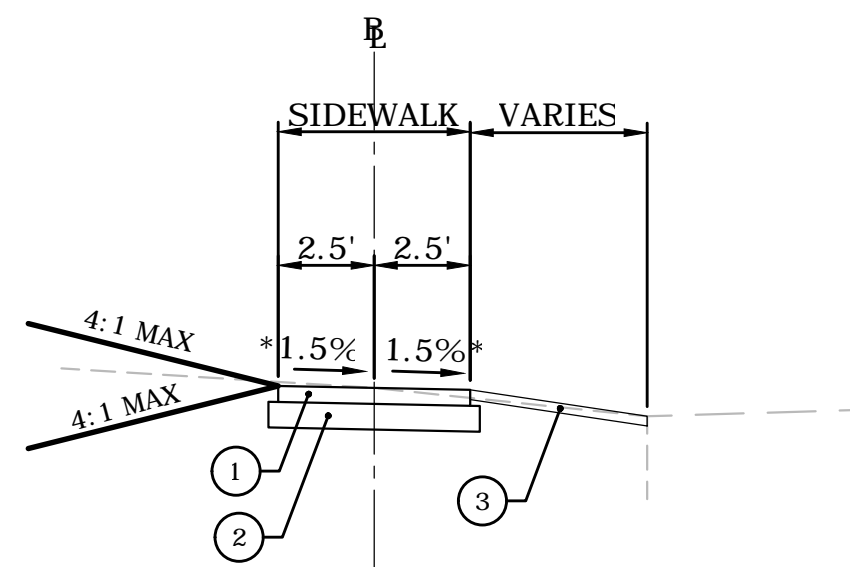
CT S.R. 44 (MAIN ST.) SIDEWALK IMPROVEMENTS

CT S.R. 44 (MAIN ST.)
SALISBURY, CONNECTICUT

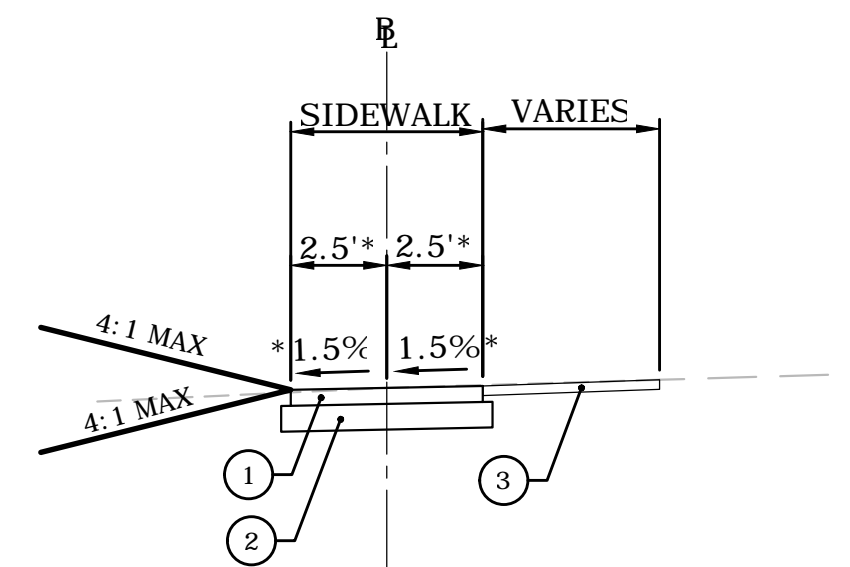
MSM DESIGNED	MSM DRAWN	AAC CHECKED
1"=60'		
MARCH 25, 2022		
13039.00006		
IND-01		
03		

SHEET NO.

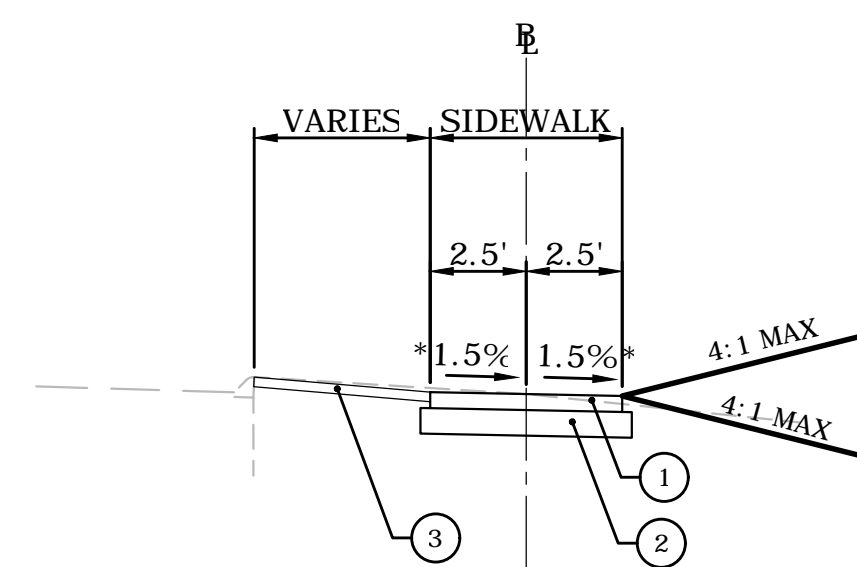
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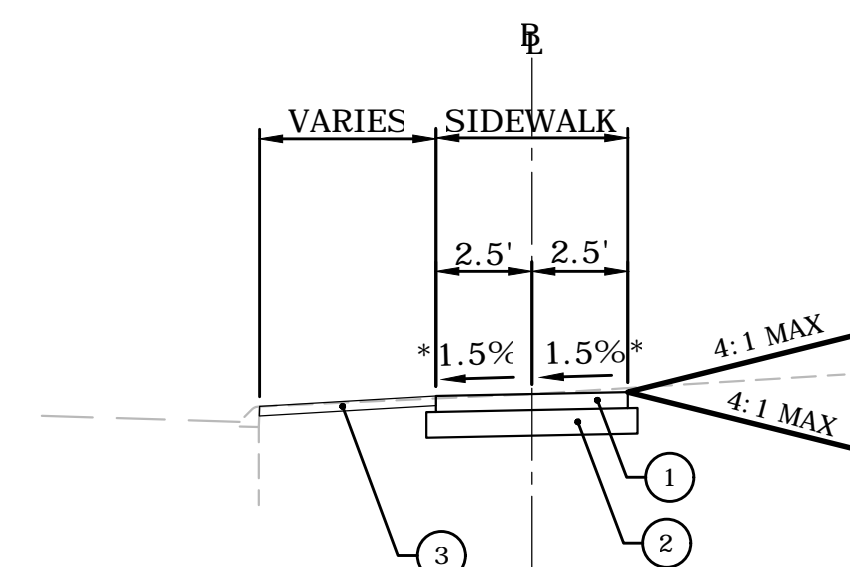
CT S.R. 44 (MAIN ST.) SIDEWALK
STA. 10+10 TO 13+15
STA. 14+20 TO 17+75
STA. 21+25 TO 22+76



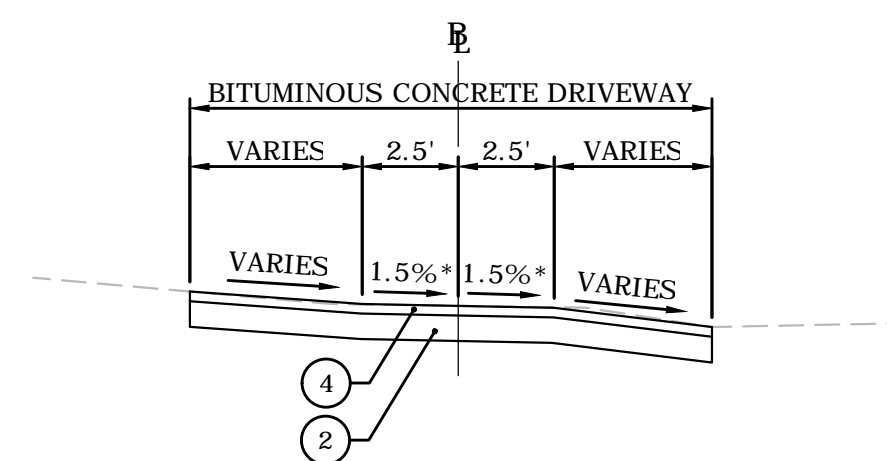
CT S.R. 44 (MAIN ST.) SIDEWALK
STA. 13+15 TO 14+20
STA. 17+75 TO 21+25
* 6' SIDEWALK STA. 19+21 TO 20+30



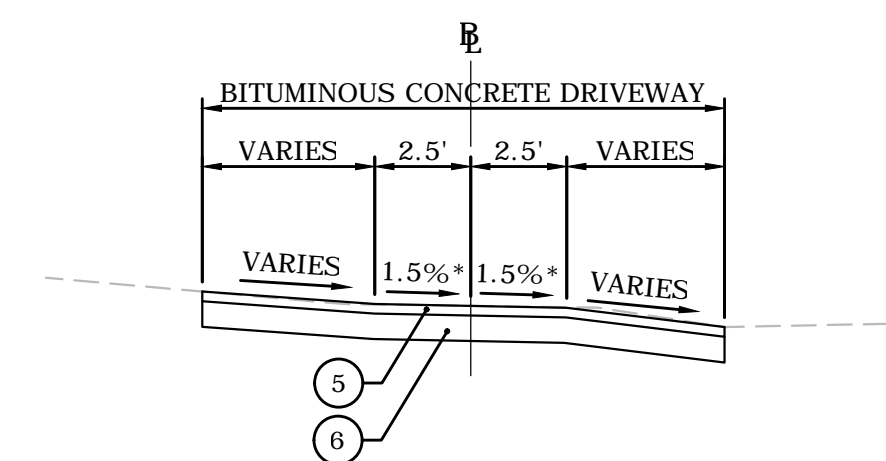
CT S.R. 44 (MAIN ST.) SIDEWALK
STA. 50+10 TO 50+75



CT S.R. 44 (MAIN ST.) SIDEWALK
STA. 50+75 TO 52+15



CT S.R. 44 (MAIN ST.) SIDEWALK
RESIDENTIAL DRIVEWAYS



CT S.R. 44 (MAIN ST.) SIDEWALK
COMMERCIAL DRIVEWAYS

LEGEND

- ① 2" BITUMINOUS CONCRETE SIDEWALK
 - ② 8" PROCESS AGGREGATE BASE
 - ③ 4" TOPSOIL AND TURF ESTABLISHMENT
 - ④ 3" HMA S0.375
 - ⑤ 4" HMA S0.375
 - ⑥ 12" PROCESS AGGREGATE BASE
- * 2% MAX CROSS SLOPE

NOTE:
SEE BITUMINOUS CONCRETE SIDEWALK AND
BITUMINOUS CONCRETE DRIVEWAY DETAILS
ON STANDARD SHEET NO. HW-921-01

[illegible]

TYPICAL CROSS SECTIONS

CT S.R. 44 (MAIN ST.) SIDEWALK IMPROVEMENTS

CT S.R. 44 (MAIN ST.)
SALISBURY, CONNECTICUT































MSM	MSM	AAC
DESIGNED	DRAWN	CHECKED
NTS		
SCALE		
MARCH 25, 2022		
DATE		
13039.00006		
PROJECT NO.		
TYP-01		
DWG NO.		

04

[illegible]

MSM	MSM	AA
DESIGNED	DRAWN	CHECKED
SCALE 1"=20'		
MARCH 25, 2022		
DATE		
13039.00006		
PROJECT NO.		
EX-01		
DWG NO.		
05		
SHEET NO.		



	STREET LINE		MANHOLE
	PROPERTY LINE		CATCH BASIN
	STONEWALL		ELECTRIC VAULT
	WATERCOURSE		WATER VALVE
	TREELINE		BOLLARD
	GUIDE RAIL		GATE POST
	CHAIN LINK FENCE		POST
	WIRE FENCE		PEDESTRIAN CROSSING POST
	OVERHEAD WIRES		UTILITY POLE
	ORDINARY HIGH WATER MARK		GUY WIRE
	100-YEAR FLOODPLAIN		HYDRANT
	LIMIT OF UPLAND REVIEW AREA		LIGHT
	TELEPHONE		SIGN
	WATER		MAILBOX
	SANITARY SEWER		MONUMENT FOUND

100-00 100-01 100-02 100-03 100-04 100-05 100-06 100-07 100-08 100-09 100-10 100-11 100-12 100-13 100-14 100-15 100-16 100-17 100-18 100-19 100-20 100-21 100-22 100-23 100-24 100-25 100-26 100-27 100-28 100-29 100-30 100-31 100-32 100-33 100-34 100-35 100-36 100-37 100-38 100-39 100-40 100-41 100-42 100-43 100-44 100-45 100-46 100-47 100-48 100-49 100-50 100-51 100-52 100-53 100-54 100-55 100-56 100-57 100-58 100-59 100-60 100-61 100-62 100-63 100-64 100-65 100-66 100-67 100-68 100-69 100-70 100-71 100-72 100-73 100-74 100-75 100-76 100-77 100-78 100-79 100-80 100-81 100-82 100-83 100-84 100-85 100-86 100-87 100-88 100-89 100-90 100-91 100-92 100-93 100-94 100-95 100-96 100-97 100-98 100-99 100-100

MATCH MARK (STA. 15+00) - SEE DRAWING EX-01

EXISTING CONDITIONS LEGEND			
	STREET LINE		MANHOLE
	PROPERTY LINE		CATCH BASIN
	STONEWALL		ELECTRIC VAULT
	WATERCOURSE		WATER VALVE
	TREELINE		BOLLARD
	GUIDE RAIL		GATE POST
	CHAIN LINK FENCE		POST
	WIRE FENCE		PEDESTRIAN CROSSING POST
	OVERHEAD WIRES		UTILITY POLE
	ORDINARY HIGH WATER MARK		GUY WIRE
	100-YEAR FLOODPLAIN		HYDRANT
	LIMIT OF UPLAND REVIEW AREA		LIGHT POST
	TELEPHONE		SIGN
	WATER		MAILBOX
	SANITARY SEWER		MONUMENT FOUND

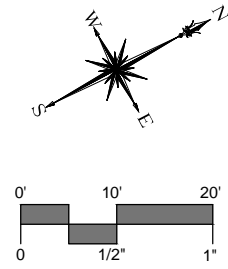
NOTES

- BORINGS BY SLR CONSULTING (MILONE & MACBROOM INC.) WERE PERFORMED BY GENERAL BORINGS, INC. ON 6/9/2020.
- THE LOCATIONS OF THE BORINGS WERE DETERMINED BY TAPING/PACING FROM EXISTING SITE FEATURES. THESE LOCATIONS SHOULD BE CONSIDERED ACCURATE ONLY TO THE DEGREE IMPLIED BY THE METHOD USED.
- 100-YEAR FLOODPLAIN LINE DERIVED FROM FEMA FLOOD INSURANCE RATE MAP, EFFECTIVE DATE: JANUARY 5, 1989.
- INLAND WETLANDS AND WATERCOURSES DELINEATED BY SLR CONSULTING (MILONE & MACBROOM INC.) REGISTERED SOIL SCIENTISTS ON APRIL 15, 2020.

LEGEND:



BORINGS BY SLR CONSULTING (MILONE & MACBROOM INC.)



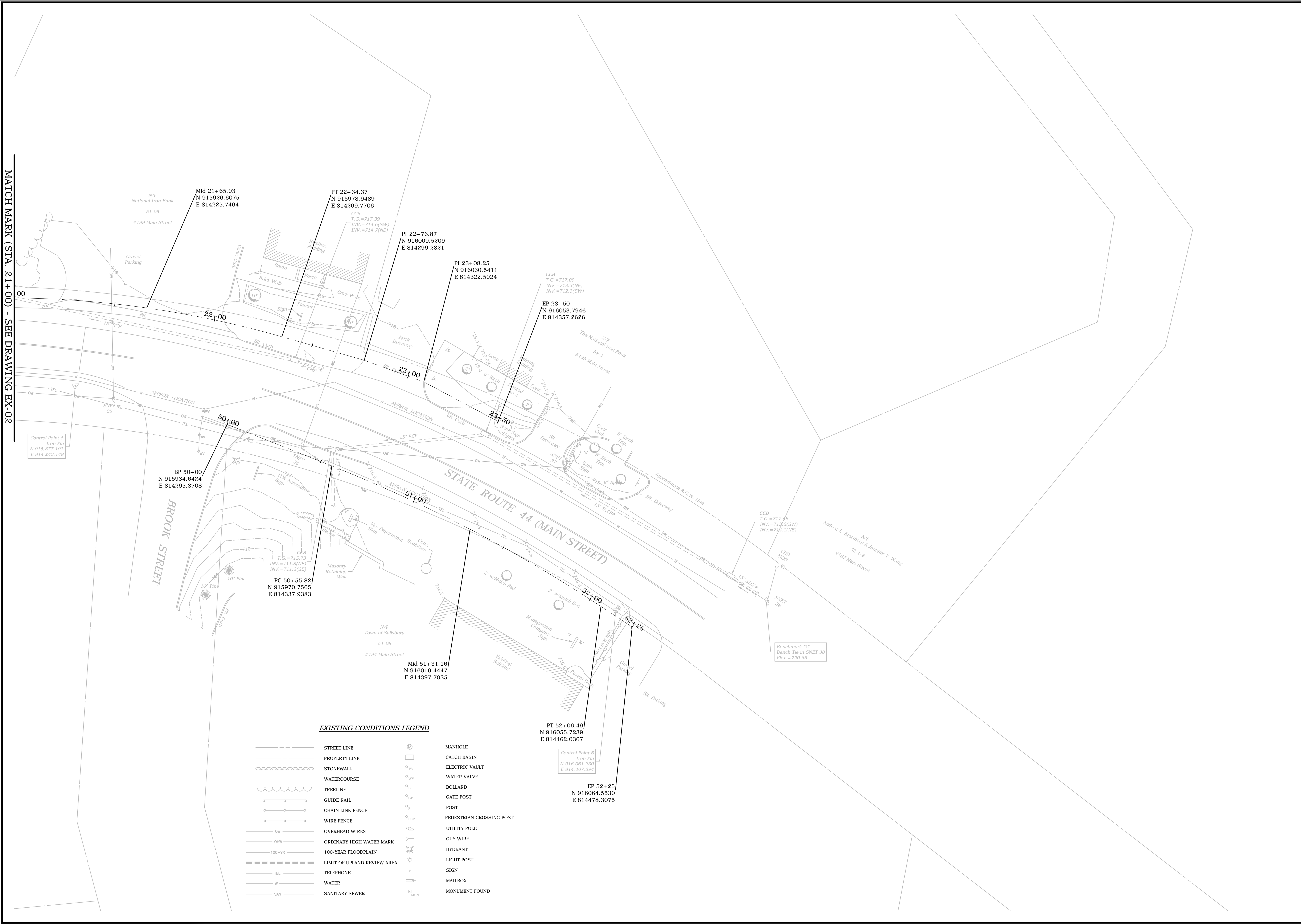
DESCRIPTION	DATE	BY

EXISTING CONDITIONS & BASELINE PLAN
CT S.R. 44 (MAIN ST.) SIDEWALK IMPROVEMENTS
CT S.R. 44 (MAIN ST.)
SALISBURY, CONNECTICUT

MSM	MSM	AAC
DESIGNED	DRAWN	CHECKED
1"=20'		
SCALE		
MARCH 25, 2022		
DATE		
13039.00006		
PROJECT NO.		
EX-02		
DWG NO.		
02		
SHEET NO.		

W:\PROJECTS\2022\22-0006\22-0006.dwg
Plotted by: J. MacBroom
On: 2/24/2022 10:00 AM
Plot Scale: 1"=20'

MATCH MARK (STA. 21+00) - SEE DRAWING EX-02



99 REALTY DRIVE
SUITE 100
SALISBURY, CT 06460
203.271.7777
SLRCONSULTING.COM

DESCRIPTION	DATE	BY

EXISTING CONDITIONS & BASELINE PLAN

CT S.R. 44 (MAIN ST.) SIDEWALK IMPROVEMENTS

CT S.R. 44 (MAIN ST.)

SALISBURY, CONNECTICUT

MSM	MSM	AAC
DESIGNED	DRAWN	CHECKED

SCALE: 1"=20'

DATE: MARCH 25, 2022

PROJECT NO.: 13039.00006

DWG NO.: EX-03

07

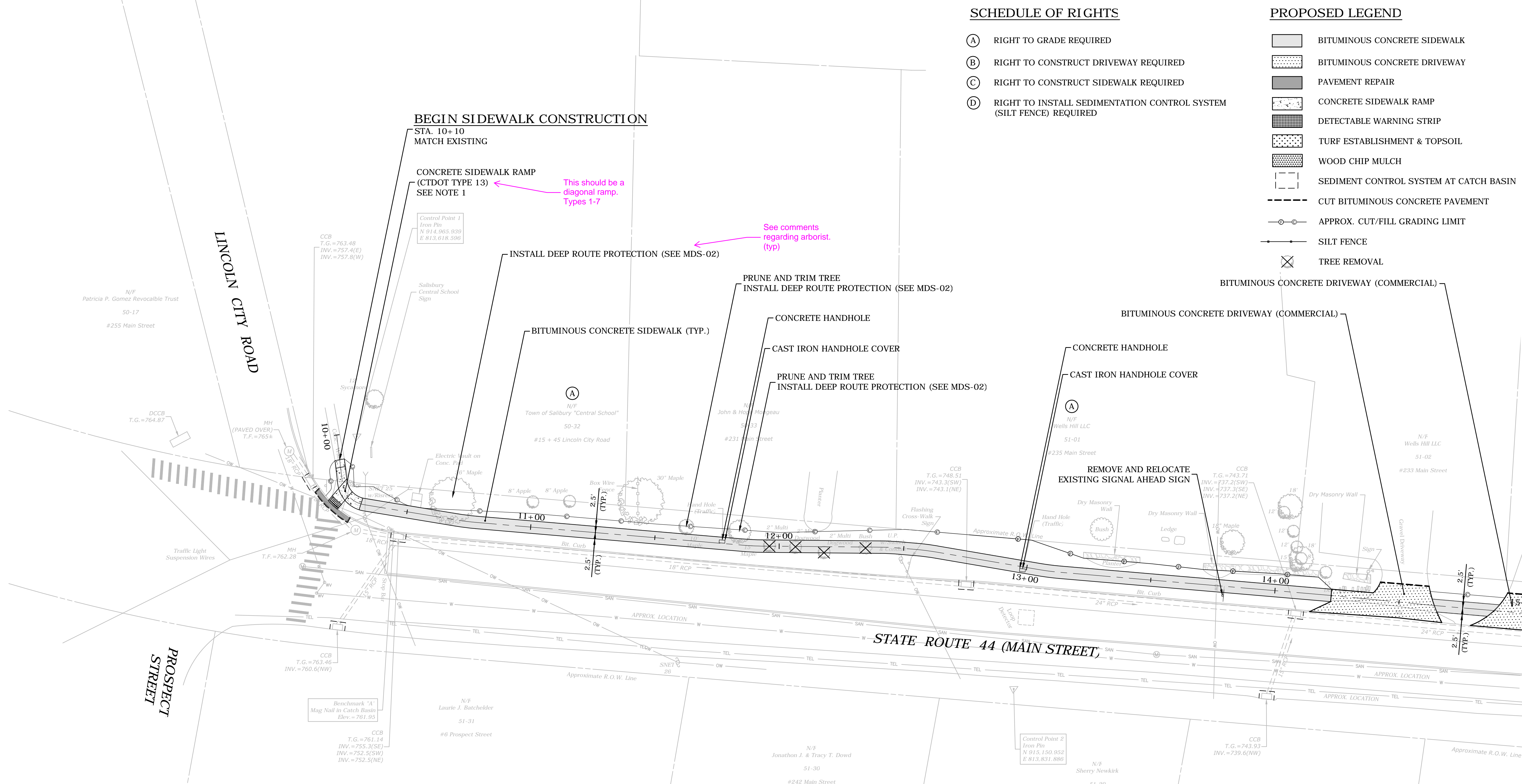
SHEET NO.

[illegible]

SIDEWALK PLAN

CT S.R. 44 (MAIN ST.) SIDEWALK IMPROVEMENTS

MSM	MSM	AAO
DESIGNED	DRAWN	CHECKED
1"=20'		
SCALE		
MARCH 25, 2022		
DATE		
13039.00006		
PROJECT NO.		
PLN-01		
DWG NO.		
08		
SHEET NO.		



- NOTES
1. THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR EACH SIDEWALK RAMP DEPICTING SPOT ELEVATIONS AT EACH CORNER AND CHANGE IN CROSS SLOPE OR SLOPE DIRECTION. THE COST FOR CONCRETE SIDEWALK RAMP SHOP DRAWINGS SHALL BE INCLUDED IN THE COST FOR "CONCRETE SIDEWALK RAMP"
 2. THE CONTRACTOR SHALL PROTECT EXISTING TRAFFIC SIGNAL EQUIPMENT. ANY DAMAGE TO THE EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE REPLACED.

PROPOSED LEGEND

- BITUMINOUS CONCRETE SIDEWALK
BITUMINOUS CONCRETE DRIVEWAY
PAVEMENT REPAIR
CONCRETE SIDEWALK RAMP
DETECTABLE WARNING STRIP
TURF ESTABLISHMENT & TOPSOIL
WOOD CHIP MULCH
SEDIMENT CONTROL SYSTEM AT CATCH BASIN
CUT BITUMINOUS CONCRETE PAVEMENT
APPROX. CUT/FILL GRADING LIMIT
SILT FENCE
TREE REMOVAL

SCHEDULE OF RIGHTS

- (A) RIGHT TO GRADE REQUIRED
(B) RIGHT TO CONSTRUCT DRIVEWAY REQUIRED
(C) RIGHT TO CONSTRUCT SIDEWALK REQUIRED
(D) RIGHT TO INSTALL SEDIMENTATION CONTROL SYSTEM (SILT FENCE) REQUIRED

REGULATED RESOURCE ACTIVITIES

ACTIVITIES WITHIN WETLANDS	0 SF = 0.00 AC
ACTIVITIES WITHIN 100-FT UPLAND REVIEW AREA	2,816 SF = 0.06 AC
NEW IMPERVIOUS AREA WITHIN 100-FT UPLAND REVIEW AREA	963 SF = 0.02 AC

FEMA FLOODPLAIN IMPACTS

NET CUT WITHIN FLOODPLAIN	15 C.Y.
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MATCH MARK (STA. 15+00) - SEE DRAWING PLN-01

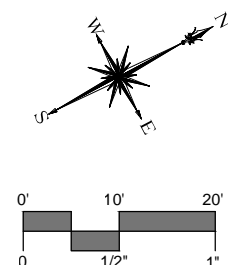
MATCH MARK (STA. 21+00) - SEE DRAWING PLN-03

NOTES

- THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR EACH SIDEWALK RAMP DEPICTING SPOT ELEVATIONS AT EACH CORNER AND CHANGE IN CROSS SLOPE OR SLOPE DIRECTION. THE COST FOR CONCRETE SIDEWALK RAMP SHOP DRAWINGS SHALL BE INCLUDED IN THE COST FOR "CONCRETE SIDEWALK RAMP"
- THE CONTRACTOR SHALL PROTECT EXISTING TRAFFIC SIGNAL EQUIPMENT. ANY DAMAGE TO THE EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE REPLACED.
- THE CONTRACTOR SHALL COORDINATE THE RELOCATION OF EXISTING UNDERGROUND ELECTRIC DOG FENCE ALONG 223 MAIN STREET WITH TOWN AND PROPERTY OWNER PRIOR TO THE START OF CONSTRUCTION

STOPPING SIGHT DISTANCE

85th PERCENTILE SPEED	DIRECTION	ROADWAY GRADE	REQ. SSD
41 M.P.H	SOUTHBOUND	-4%	335'
46 M.P.H	NORTHBOUND	0%	373'

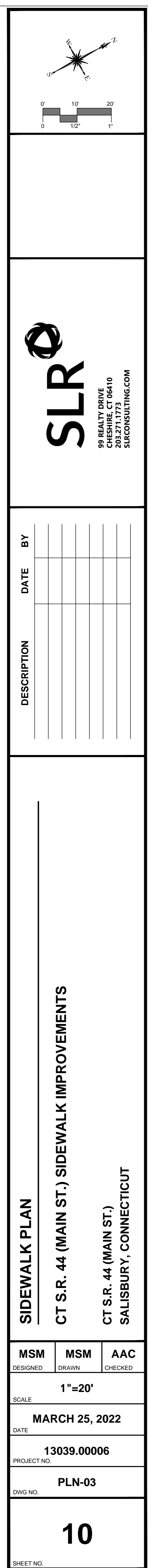
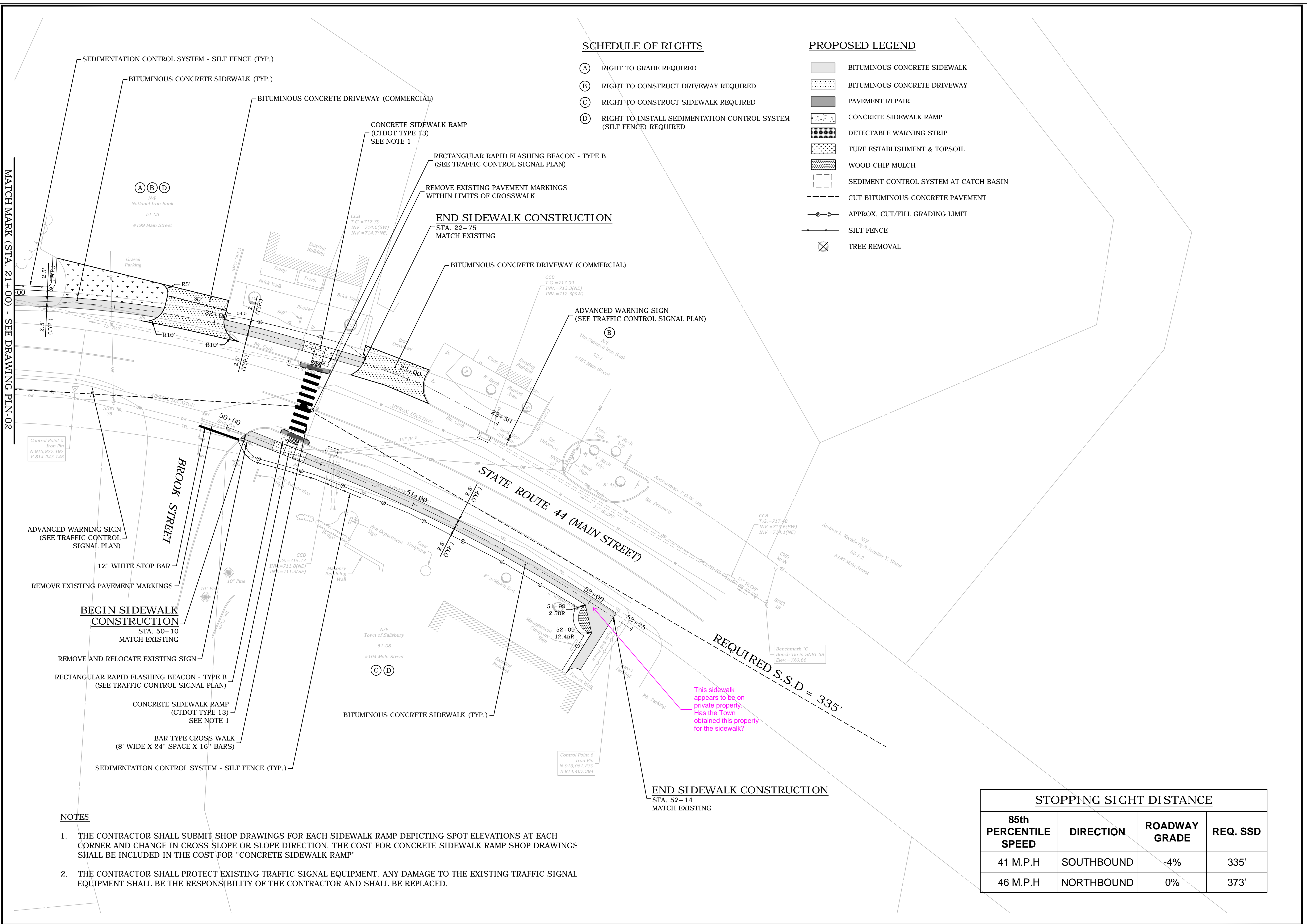


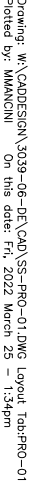
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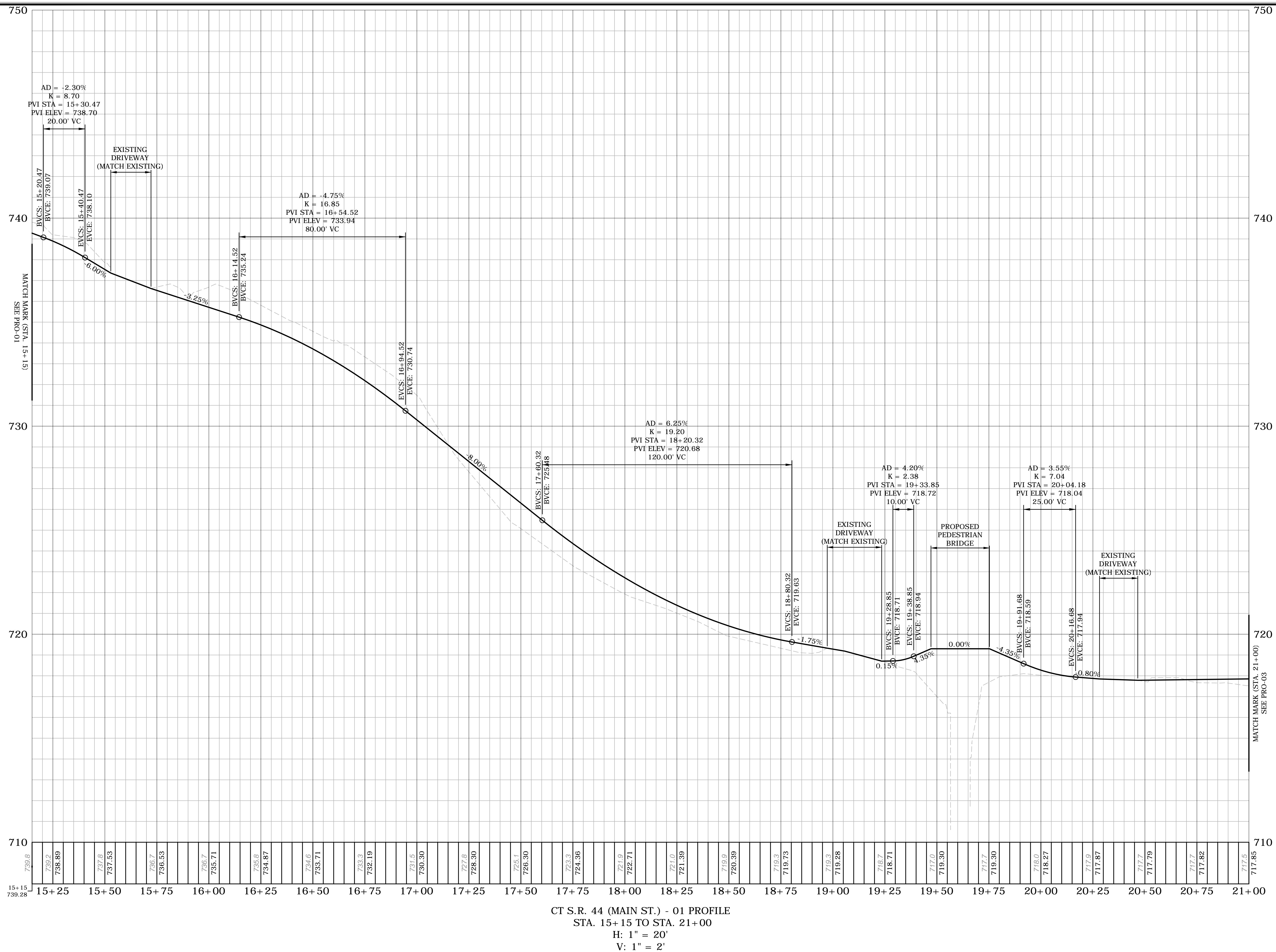
SIDEWALK PLAN
CT S.R. 44 (MAIN ST.) SIDEWALK IMPROVEMENTS

CT S.R. 44 (MAIN ST.)
SALISBURY, CONNECTICUT

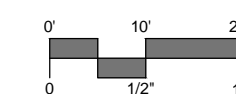
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SCALE: 1"=20'		
DATE: MARCH 25, 2022		
PROJECT NO.: 13039.00006		
DWG NO.: PLN-02		
SHEET NO.: 03		







CT S.R. 44 (MAIN ST.) - 01 PROFILE
STA. 15+15 TO STA. 21+00
H: 1" = 20'
V: 1" = 2'

[illegible]

SIDEWALK PROFILE

CT S.R. 44 (MAIN ST.) SIDEWALK IMPROVEMENTS

CT S.R. 44 (MAIN ST.)
SALISBURY, CONNECTICUT

MSM	MSM	AA
DESIGNED	DRAWN	CHECKED

1"=20'

SCALE

MARCH 25, 2022

DATE

13039.00006

PROJECT NO.

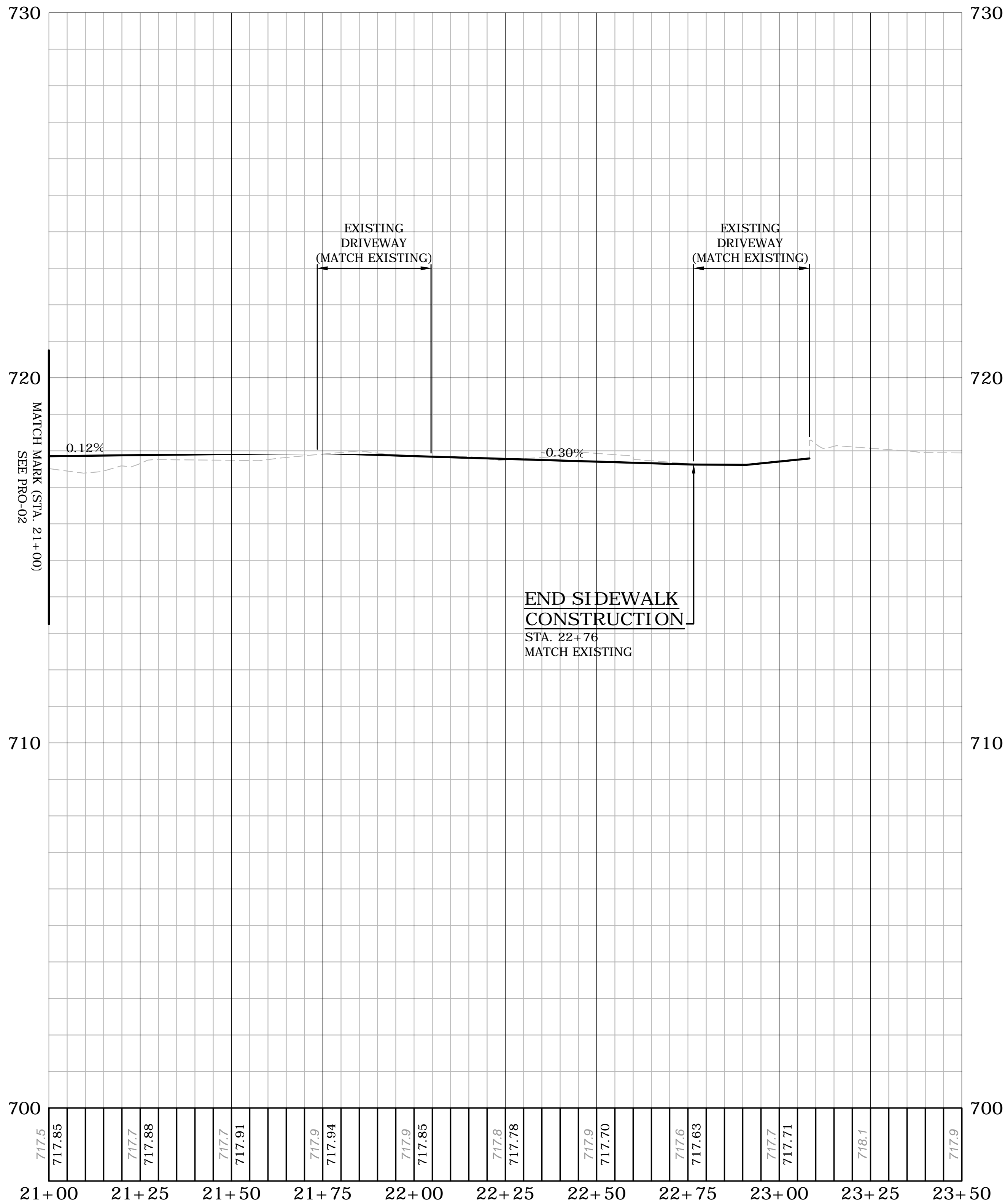
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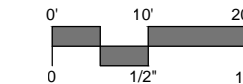
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SHEET NO.

13039.00006.ctb
C:\Users\jgordon\OneDrive\Documents\13039.00006.ctb



CT S.R. 44 (MAIN ST.) - 01 PROFILE
STA. 21+00 TO STA. 23+50
H: 1" = 20'
V: 1" = 2'

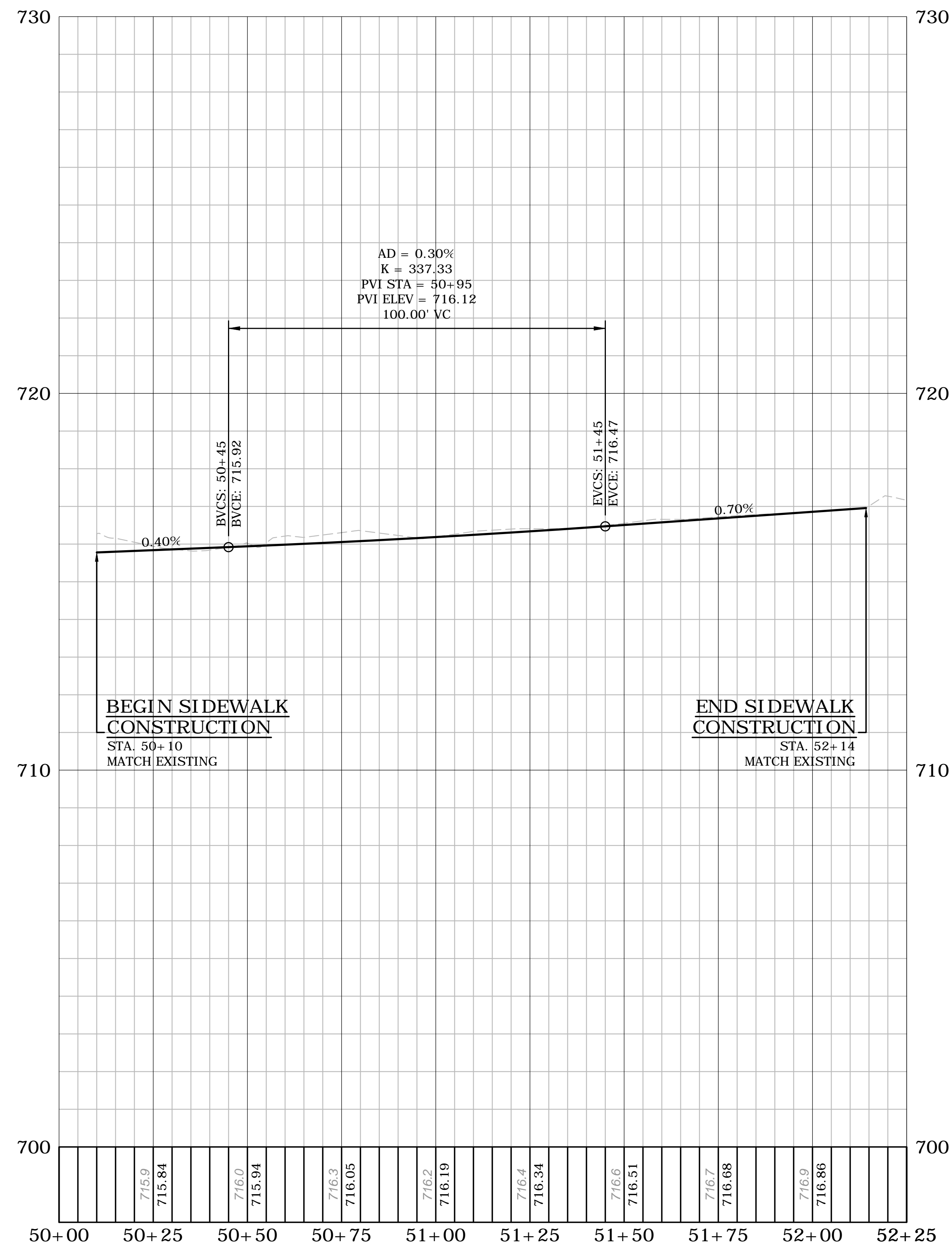


DESCRIPTION	DATE	BY

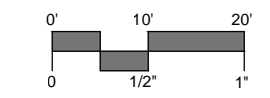
SIDEWALK PROFILE
CT S.R. 44 (MAIN ST.) SIDEWALK IMPROVEMENTS
CT S.R. 44 (MAIN ST.)
SALISBURY, CONNECTICUT

MSM DESIGNED	MSM DRAWN	AAC CHECKED
SCALE 1"=20'		
DATE MARCH 25, 2022		
PROJECT NO. 13039.00006		
DRAWING NO. PRO-03		

SHEET NO.
13



CT S.R. 44 (MAIN ST.) - 02 PROFILE
STA. 50+00 TO STA. 52+25
H: 1" = 20'
V: 1" = 2'

[illegible]

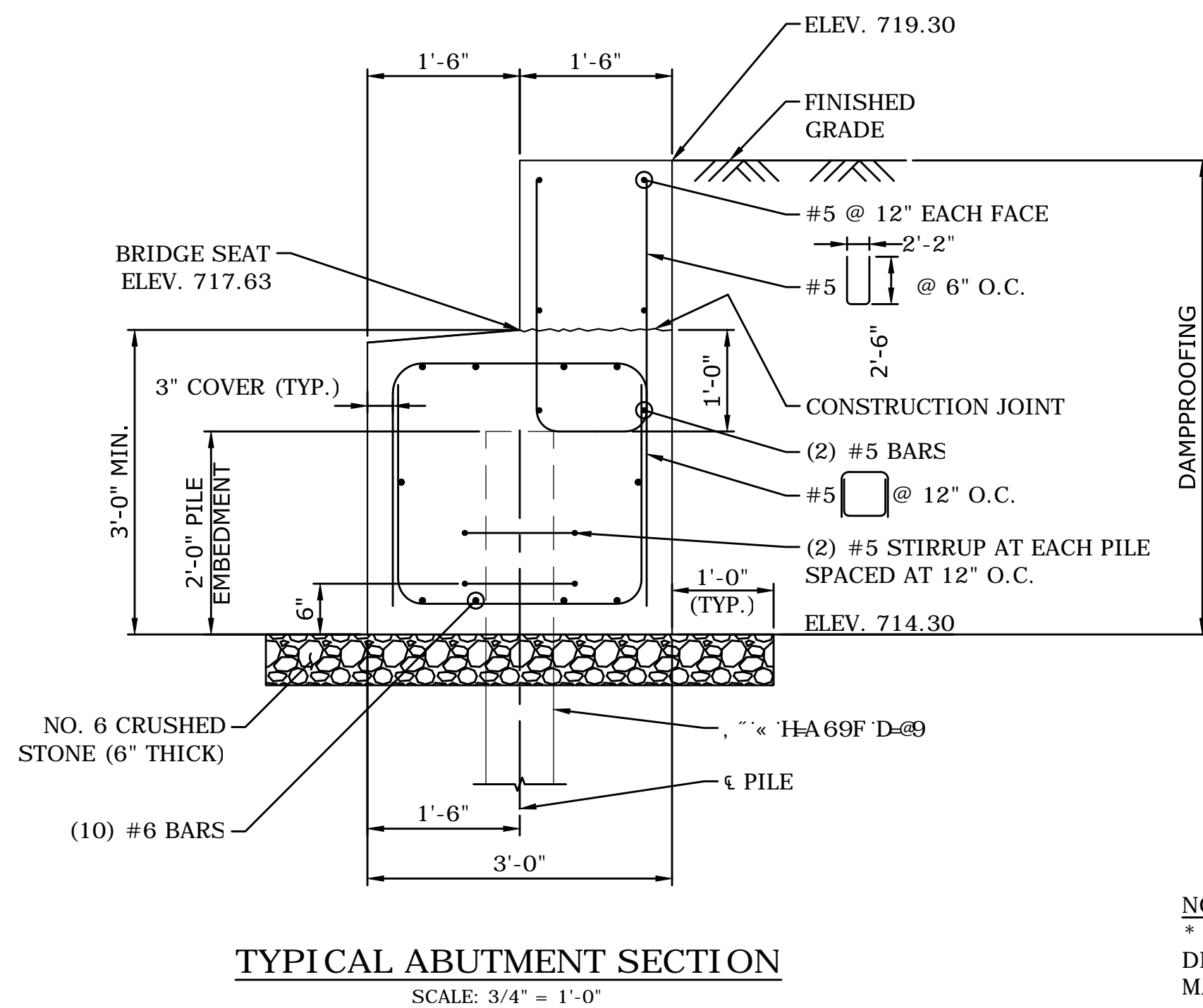
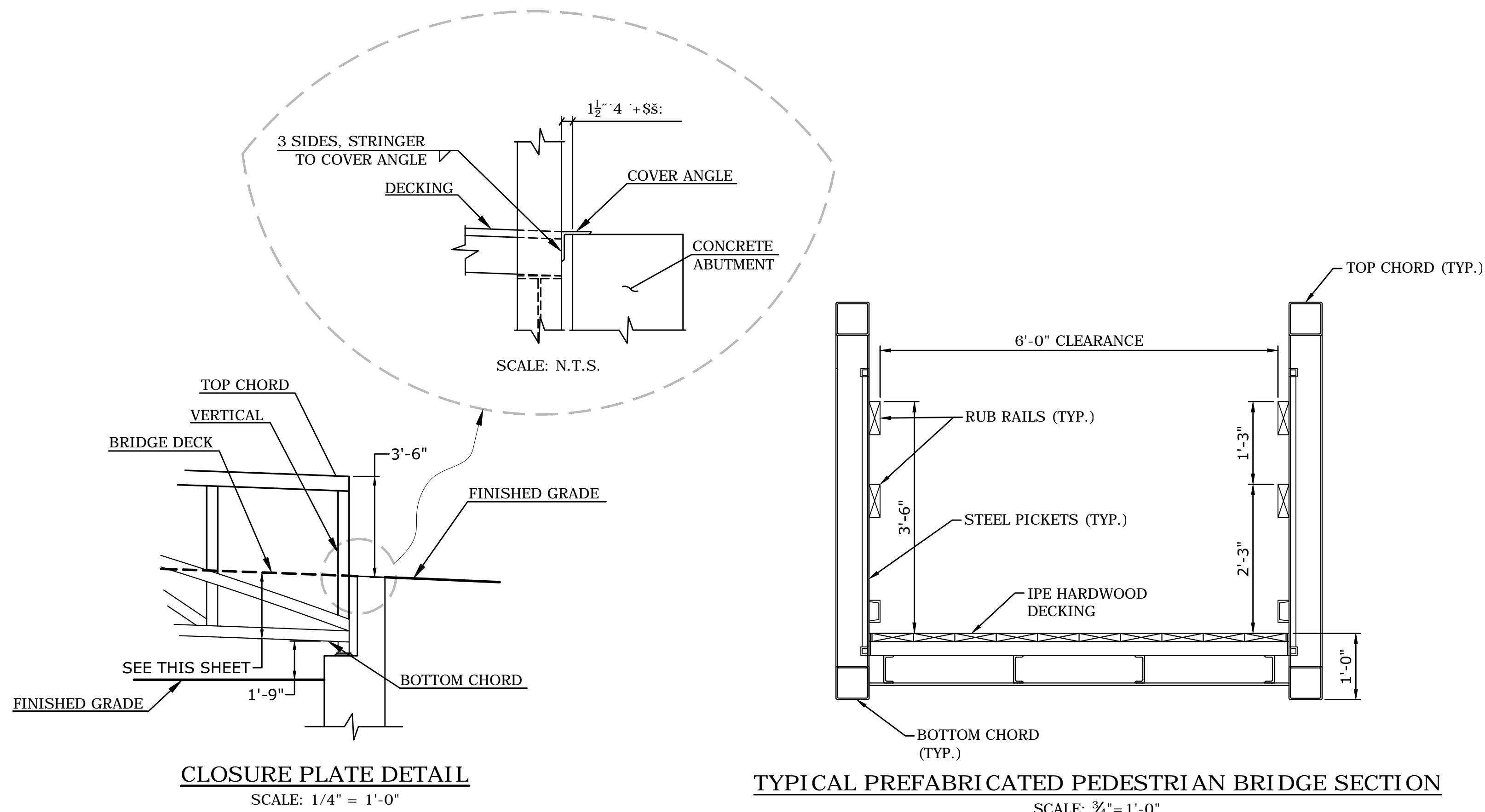
SIDEWALK PROFILE

CT S.R. 44 (MAIN ST.) SIDEWALK IMPROVEMENTS

CT S.R. 44 (MAIN ST.)
SALISBURY, CONNECTICUT

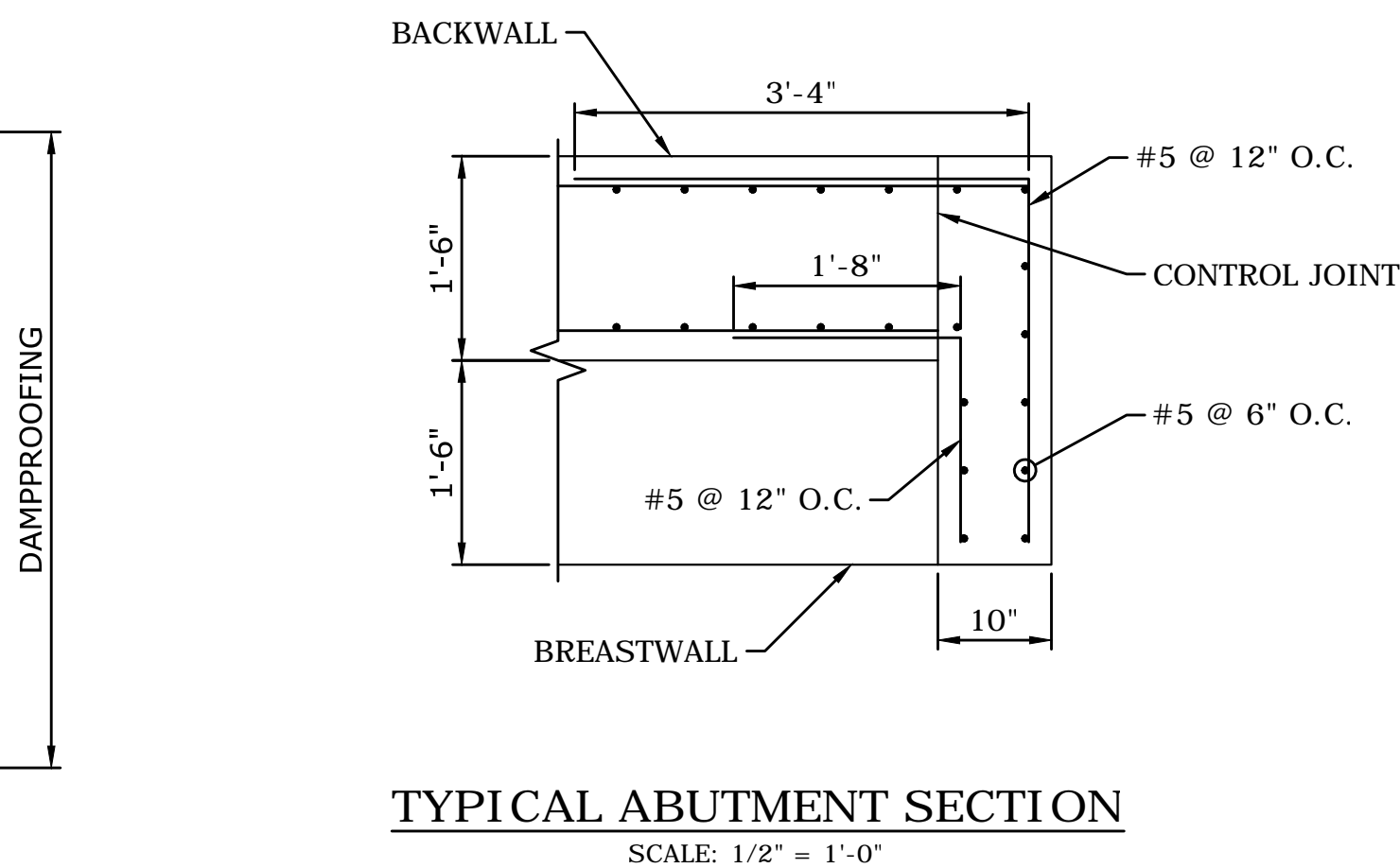
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SCALE		
MARCH 25, 2022		
DATE		
13039.00006		
PROJECT NO.		
PRO-04		
DRAWING NO.		

14

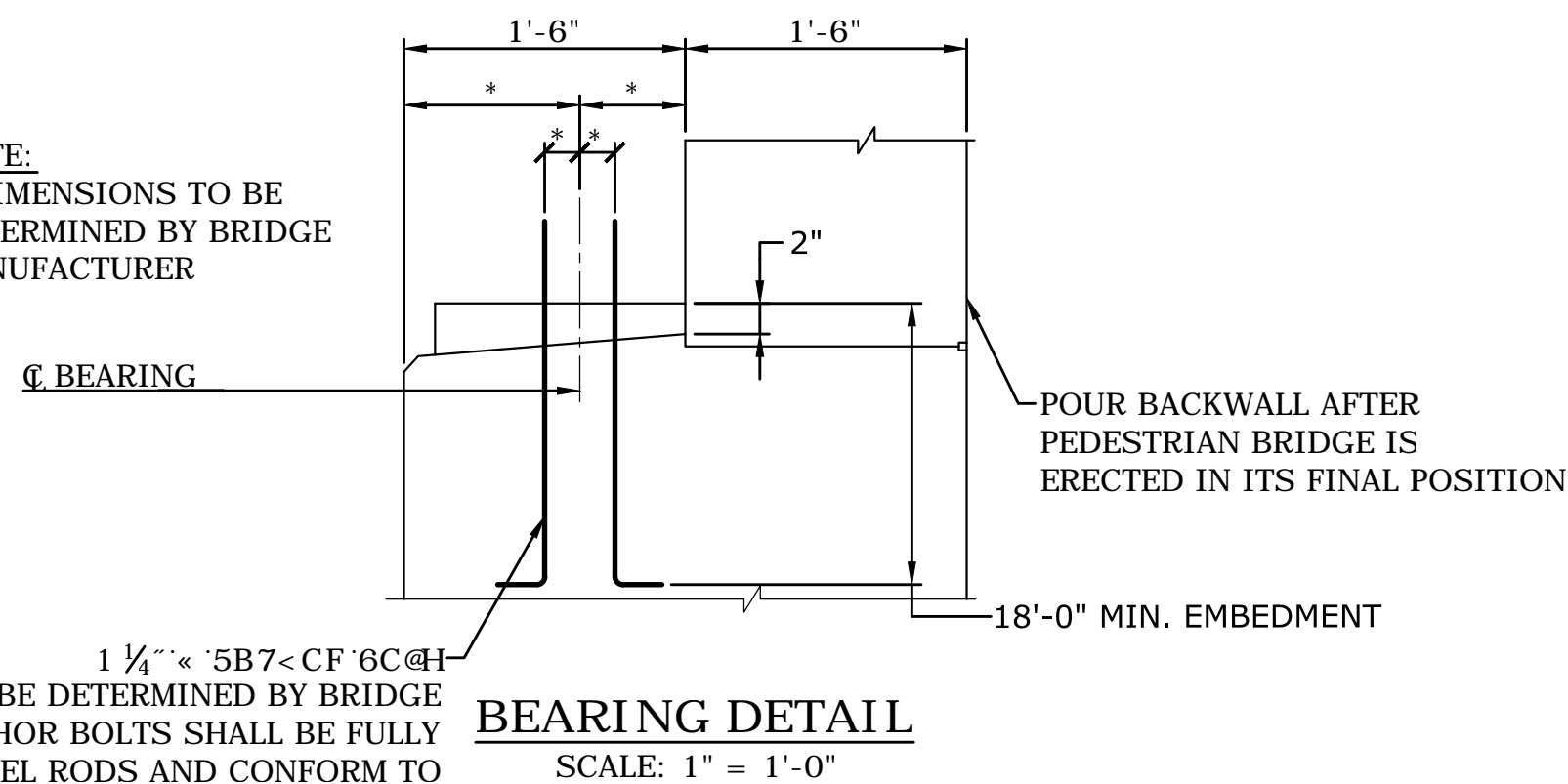


NOTE:

1. BRIDGE SEAT ELEVATIONS SHALL BE VERIFIED BY BRIDGE MANUFACTURER PRIOR TO CONSTRUCTING THE ABUTMENTS.
2. STRUCTURE EXCAVATION - EARTH (COMPLETE) AND PERVIOUS STRUCTURE BACKFILL SHALL BE PAID IN ACCORDANCE WITH FORM 818.



NOTE:
* DIMENSIONS TO BE
DETERMINED BY BRIDGE
MANUFACTURER



GENERAL NOTES

1. SPECIFICATIONS: CONNECTICUT DEPARTMENT OF TRANSPORTATION FORM 818 (2020), AND SPECIAL PROVISIONS.
2. DESIGN SPECIFICATIONS: AASHTO LRFD DESIGN SPECIFICATIONS, 8TH EDITION, 2017, AS SUPPLEMENTED BY THE CONNECTICUT DEPARTMENT OF TRANSPORTATION BRIDGE DESIGN MANUAL (2003) WITH INTERIM REVISIONS UP TO AND INCLUDING 2011.
3. MATERIAL STRENGTHS:

CONCRETE:
CLASS PCC 04460 $f'_c = 4,000$ PSI

THE CONCRETE STRENGTH USED IN DESIGN (f'_c) OF THE CONCRETE COMPONENTS IS NOTED ABOVE. THE COMPRESSIVE STRENGTH OF THE CONCRETE IN THE CONSTRUCTED COMPONENTS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 6.01 - CONCRETE FOR STRUCTURES AND M.03 - PORTLAND CEMENT CONCRETE.

REINFORCEMENT:
ASTM A615 GRADE 60 $f_y = 60,000$ PSI
4. LIVE LOAD: 90 PSF PEDESTRIAN LOADING OR 6,000 LB VEHICULAR LOADING WHICHEVER GOVERNS (SEE SPECIAL PROVISION)
5. DEAD LOAD: ALL PEDESTRIAN BRIDGE COMPONENTS
6. FUTURE PAVING ALLOWANCE: NONE
7. EXISTING DIMENSIONS: DIMENSIONS AND ELEVATIONS OF THE EXISTING STRUCTURE SHOWN ON THESE PLANS ARE FOR GENERAL REFERENCE ONLY AND ARE NOT GUARANTEED. THE CONTRACTOR SHALL TAKE ALL FIELD MEASUREMENTS NECESSARY TO ASSURE PROPER FIT OF THE FINISH WORK AND SHALL ASSUME FULL RESPONSIBILITY FOR THEIR ACCURACY. WHEN SHOP DRAWINGS BASED ON FIELD MEASUREMENTS ARE SUBMITTED FOR APPROVAL, THE FIELD MEASUREMENTS SHALL ALSO BE SUBMITTED FOR REFERENCE BY THE REVIEWER.

CONCRETE NOTES


1. REMAIN-IN-PLACE FORMS: THE USE OF REMAIN-IN-PLACE FORMS ON THIS STRUCTURE IS NOT ALLOWED.
2. THE FOLLOWING PAY ITEMS AND CONCRETE CLASSES ARE REQUIRED FOR CAST-IN-PLACE BRIDGE COMPONENTS:

ITEM	BRIDGE COMPONENTS	PCC CLASS
ABUTMENT AND WALL CONCRETE	ABUTMENT STEM, BACKWALL, CHEEKWALLS	PCC04460

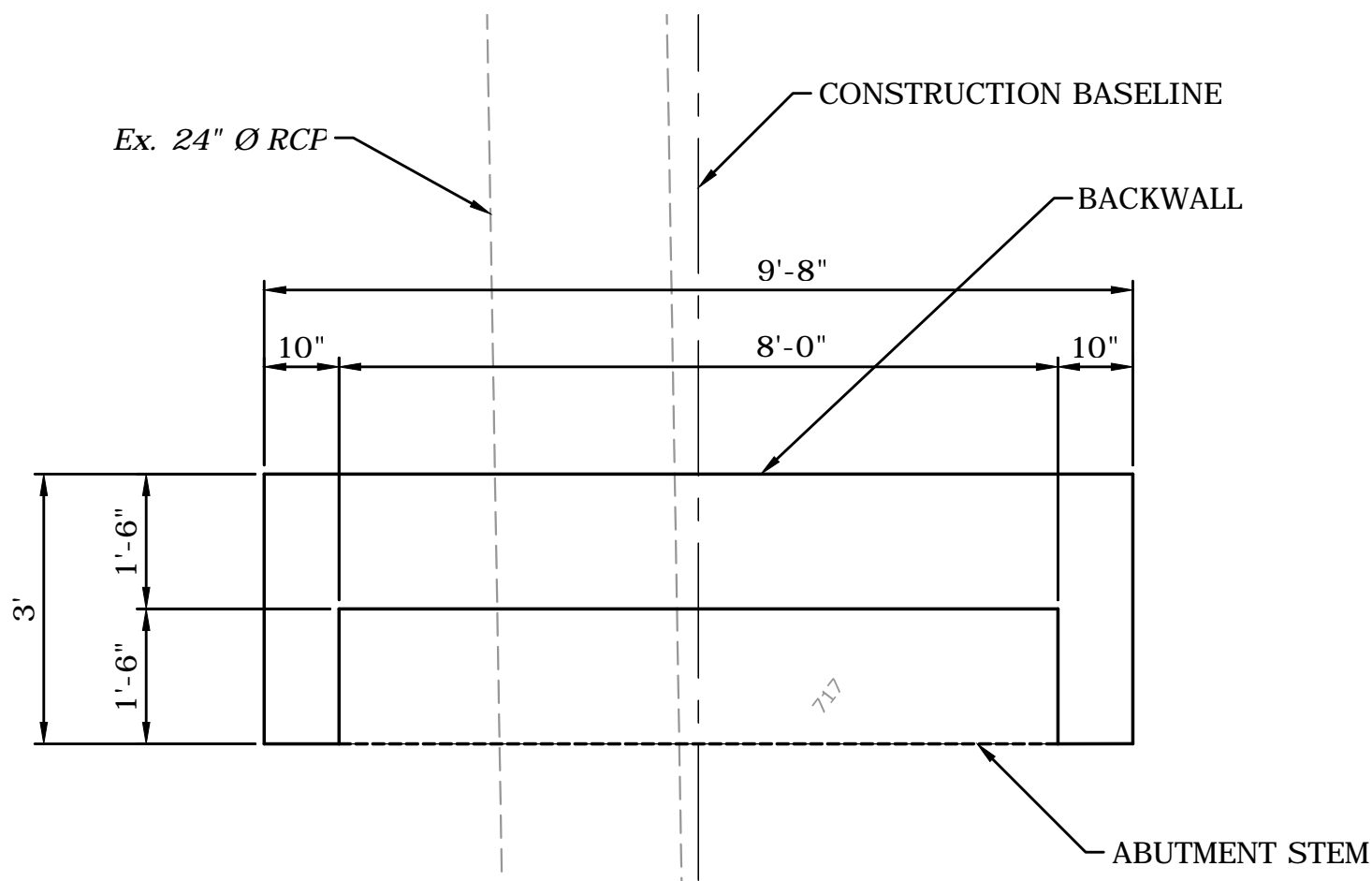
3. EXPPOSED EDGES: EXPOSED EDGES OF CONCRETE SHALL BE BEVELED 1"x1" UNLESS DIMENSIONED OTHERWISE.
4. CONCRETE COVER: ALL REINFORCEMENT SHALL HAVE TWO INCHES COVER UNLESS DIMENSIONED OTHERWISE.
5. REINFORCEMENT: ALL REINFORCEMENT SHALL BE GALVANIZED AFTER FABRICATION UNLESS NOTED OTHERWISE. ALL REINFORCEMENT SHALL CONFORM TO THE REQUIREMENTS OF ASTM A767, CLASS 1, INCLUDING SUPPLEMENTAL REQUIREMENTS. THE COST OF FURNISHING AND PLACING THIS REINFORCEMENT SHALL BE INCLUDED IN THE ITEM "PEDESTRIAN BRIDGE" SUPERSTRUCTURE (SITE NO. 1)
6. CONSTRUCTION JOINTS: CONSTRUCTION JOINTS, OTHER THAN THOSE SHOWN ON THE PLANS, WILL NOT BE PERMITTED WITHOUT THE PRIOR APPROVAL OF THE ENGINEER.

PEDESTRIAN BRIDGE SUPERSTRUCTURE NOTES

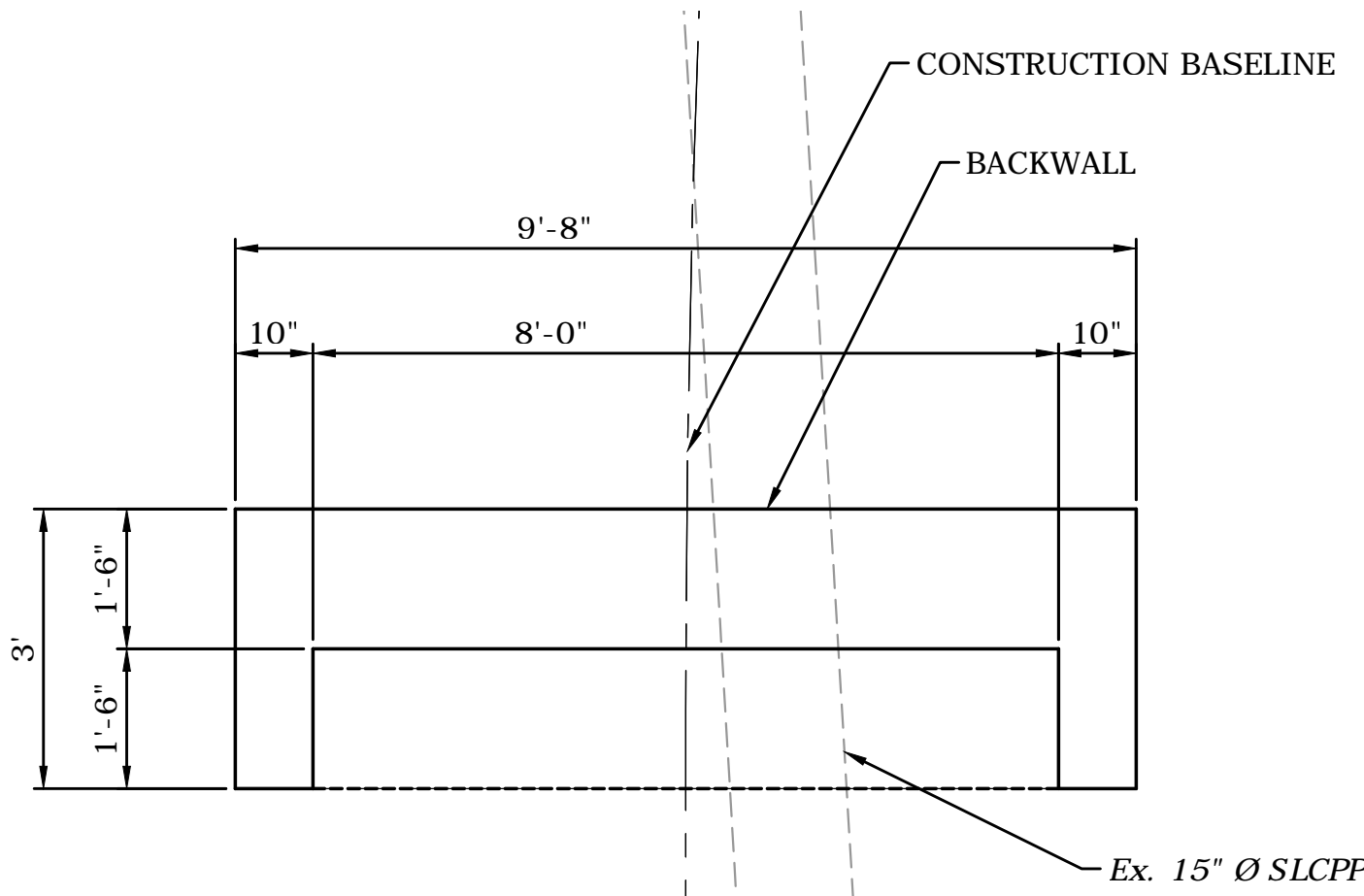
1. PEDESTRIAN BRIDGE SUPERSTRUCTURE SHALL BE DESIGNED, FABRICATED, AND INSTALLED BY THE CONTRACTOR IN ACCORDANCE WITH TECHNICAL SPECIFICATIONS. SHOP DRAWINGS, DESIGN CALCULATIONS, AND ERECTION PLAN MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL PRIOR TO ORDERING MATERIAL OR CONSTRUCTING BRIDGE.
2. BRIDGE SEAT ELEVATIONS AND ANCHOR BOLT LOCATIONS SHALL BE DETERMINED BY THE BRIDGE MANUFACTURER. CONSTRUCTION OF THE ABUTMENTS SHALL NOT COMMENCE UNTIL SHOP DRAWINGS HAVE BEEN REVIEWED AND APPROVED BY THE ENGINEER.
3. ANCHOR BOLTS SHALL BE FULLY THREADED STAINLESS STEEL RODS AND CONFORM TO A193, CLASS 2, GRADE 8 (UNS DESIGNATION S 30400 (304)). THE NUTS SHALL BE PREVAILING-TORQUE REUSABLE-TYPE (WITH NYLON INSERT) LOCK NUTS AND CONFORM TO A194, GRADE 8, STRAIN HARDENED (UNS DESIGNATION S 030400 (304)). WASHERS SHALL BE $\frac{1}{16}$ " THICK STAINLESS STEEL AND CONFORM TO ASTM A276, TYPE 304, ANNEALED. ANCHOR BOLTS SHALL BE PAID FOR UNDER ITEM "PEDESTRIAN BRIDGE SUPERSTRUCTURE (SITE NO. 1)".
4. BEARINGS SHALL BE NEOPRENE ELASTOMERIC BEARING PADS DESIGNED IN ACCORDANCE WITH AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS (5TH EDITION). BEARING PADS SHALL BE DESIGNED AND PAID FOR UNDER ITEM "PEDESTRIAN BRIDGE SUPERSTRUCTURE (SITE NO. 1)".
5. BRIDGE SHALL BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
6. ALL MEMBERS OF VERTICAL TRUSSES (TOP AND BOTTOM CHORDS, VERTICAL AND DIAGONALS) AND LATERAL BRACING WITH CALCULATED TENSILE STRESSES SHALL BE DESIGNATED FRACTURE CRITICAL MEMBERS.
7. PREFABRICATED PEDESTRIAN BRIDGE SHALL BE PAINTED IN ACCORDANCE WITH THE TECHNICAL SPECIFICATIONS. BRIDGE MEMBERS SHALL BE FABRICATED FROM HIGH STRENGTH, LOW ALLOY STEEL CONFORMING TO THE REQUIREMENTS OF ASTM A M270, GRADE 50 AND IN ACCORDANCE WITH THE TECHNICAL SPECIFICATIONS. PLATE AND STRUCTURAL SHAPES SHALL BE $F_y = 50,000$ PSI.
8. PAINT COLOR SHALL BE COORDINATED WITH THE TOWN PRIOR TO FABRICATION.
9. $\frac{1}{4}$ " MINIMUM STEEL THICKNESS REQUIRED ON ALL STRUCTURAL MEMBERS.
10. WELDING DETAILS, PROCEDURES AND TESTING METHODS SHALL CONFORM TO THE ANSI/AWS D1.1 - STRUCTURAL WELDING CODE, LATEST EDITION.
11. PROVIDE VERTICAL STEEL PICKETS, SUCH THAT THE MAXIMUM CLEAR OPENING IS 4". PROVIDE CLOSURE ANGLES AT TOP AND BOTTOM.
12. BRIDGE DECKING SHALL CONSIST OF IPE HARDWOOD.

<div>PEDESTRIAN BRIDGE - DETAILS</div> <div>CT S.R. 44 (MAIN ST.) SIDEWALK IMPROVEMENTS</div> <div>CT S.R. 44 (MAIN ST.) SALISBURY, CONNECTICUT</div>			<div>SLR</div> <div>99 BEALY DRIVE CHESHIRE, CT 06610 TEL: 203.261.0600 SLRCONSULTING.COM</div>				
DESIGNED	DRAWN	AAC	DESCRIPTION	DATE	BY		
AS SHOWN							
SCALE							
DATE							
PROJECT NO.							
DWG NO.							
06							
SHEET NO.							

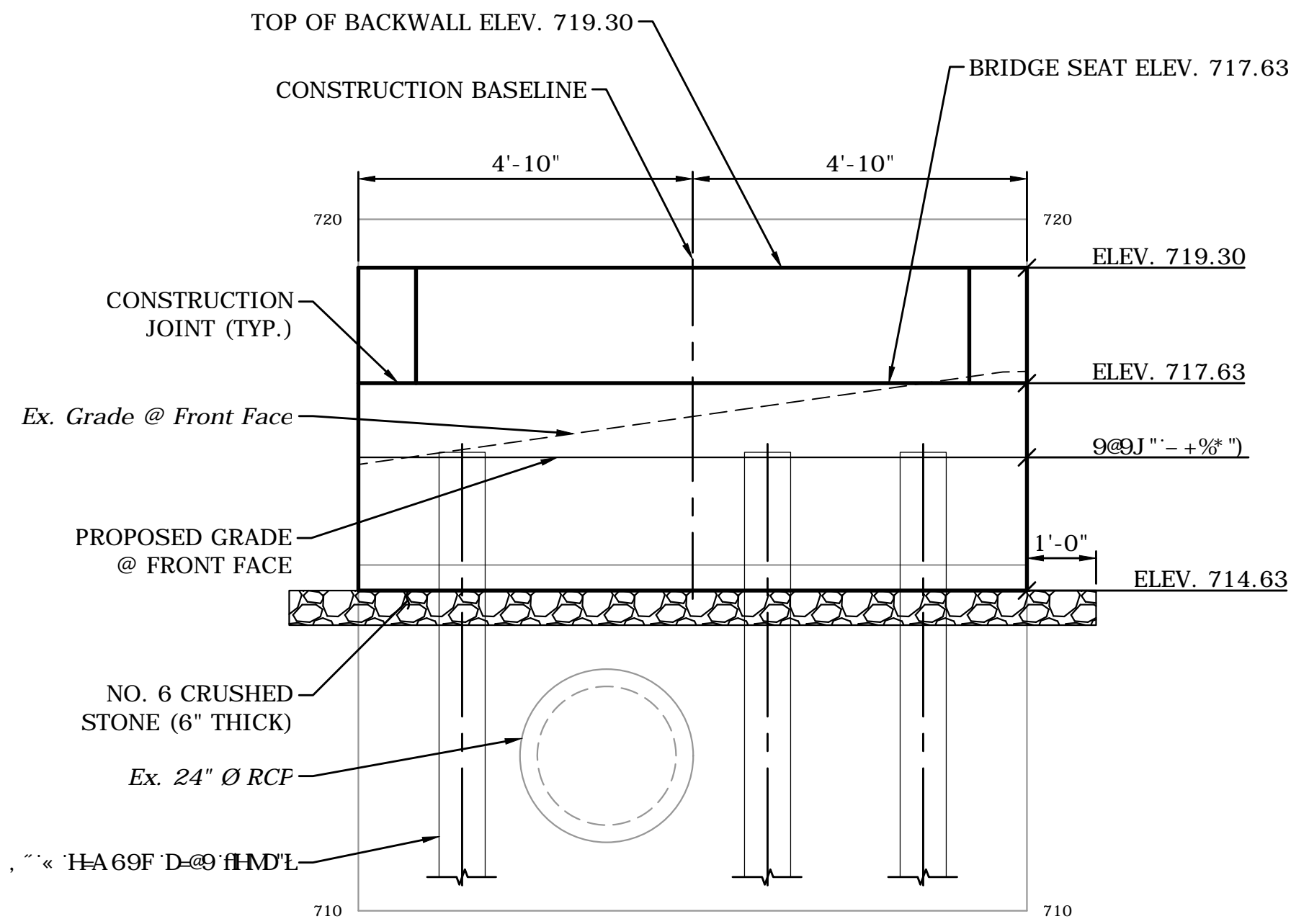
PROJECT: PEDESTRIAN BRIDGE - ABUTMENT LAYOUT
DRAWN BY: J. L. LARSEN, P.E.
CHECKED BY: J. L. LARSEN, P.E.
DATE: 03/25/2022
SCALE: 1" = 2'-0"



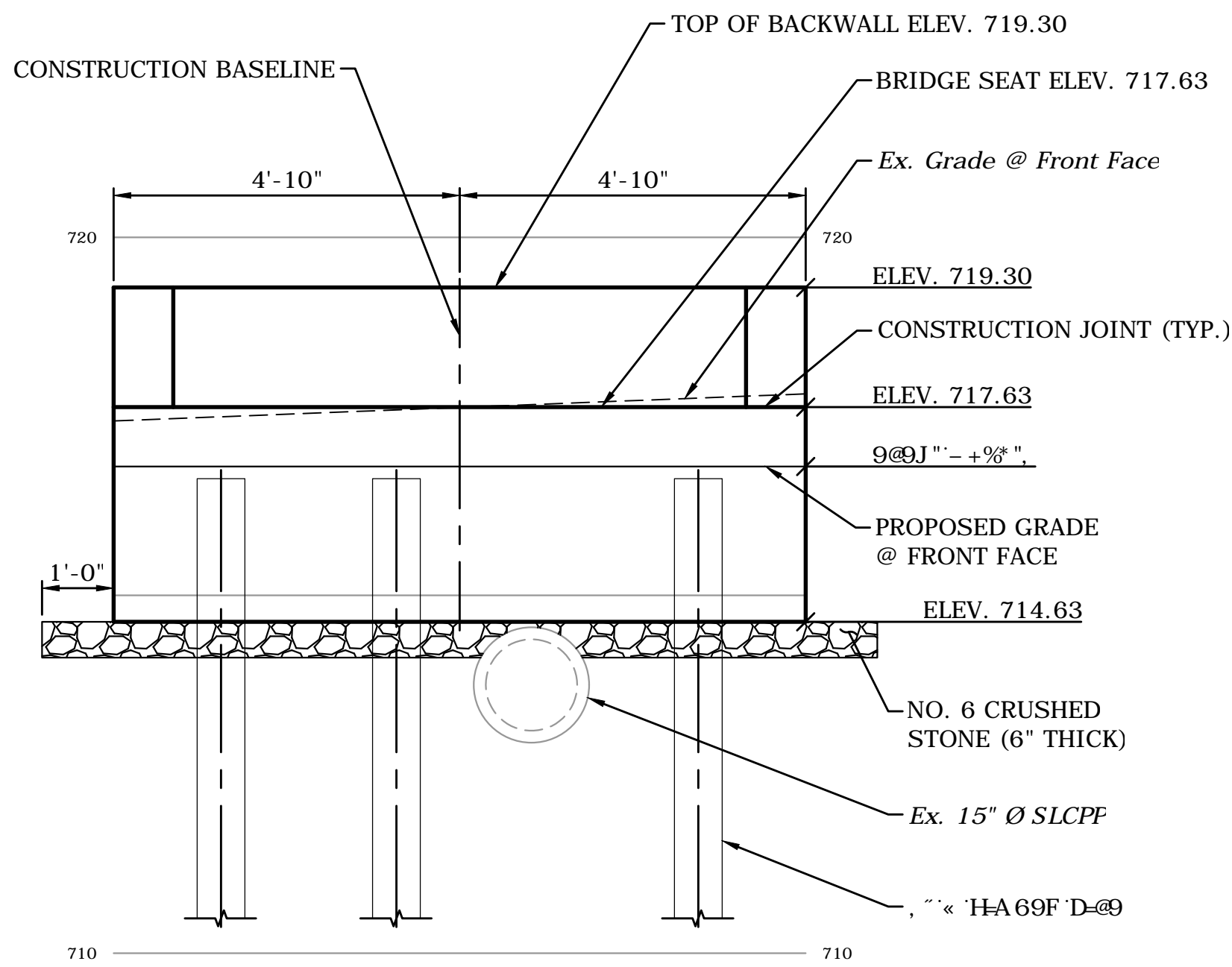
ABUTMENT NO. 1 PLAN
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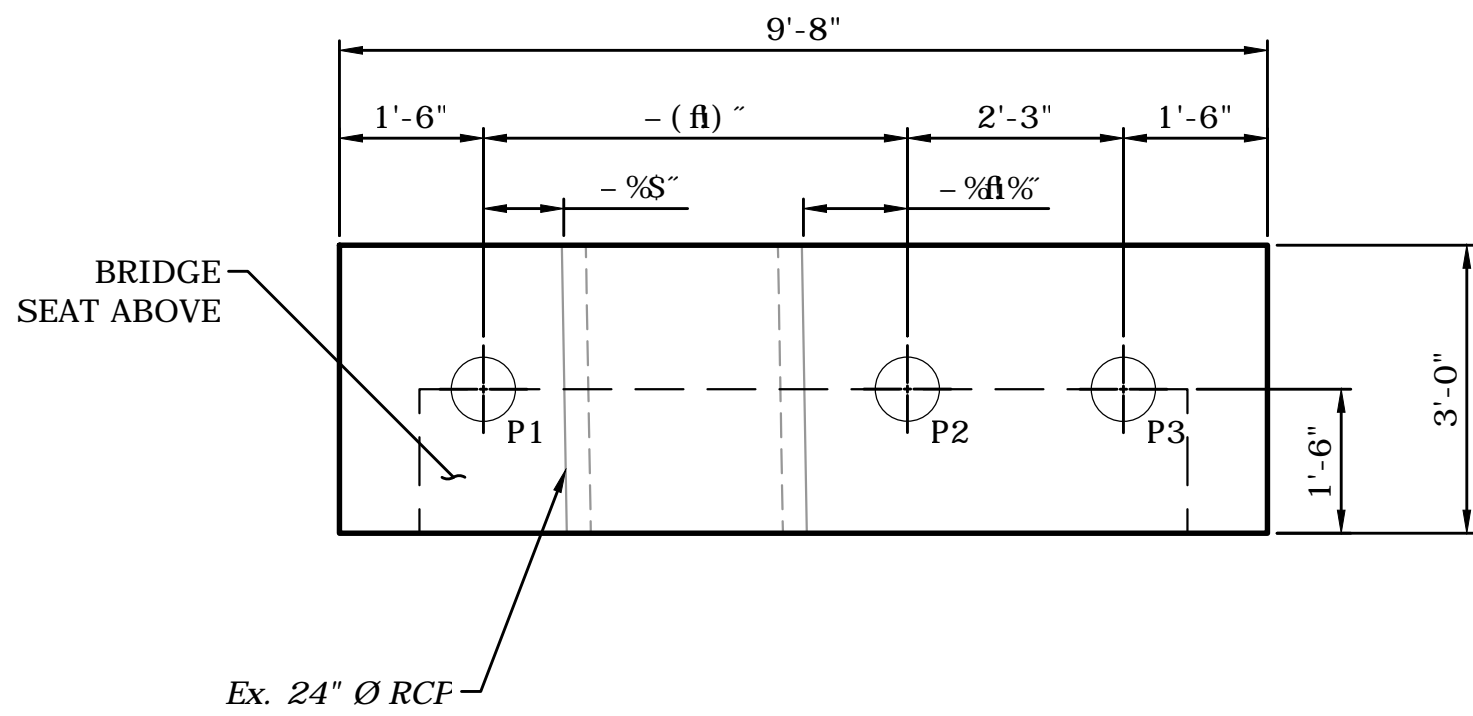
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SCALE: 1" = 2'-0"



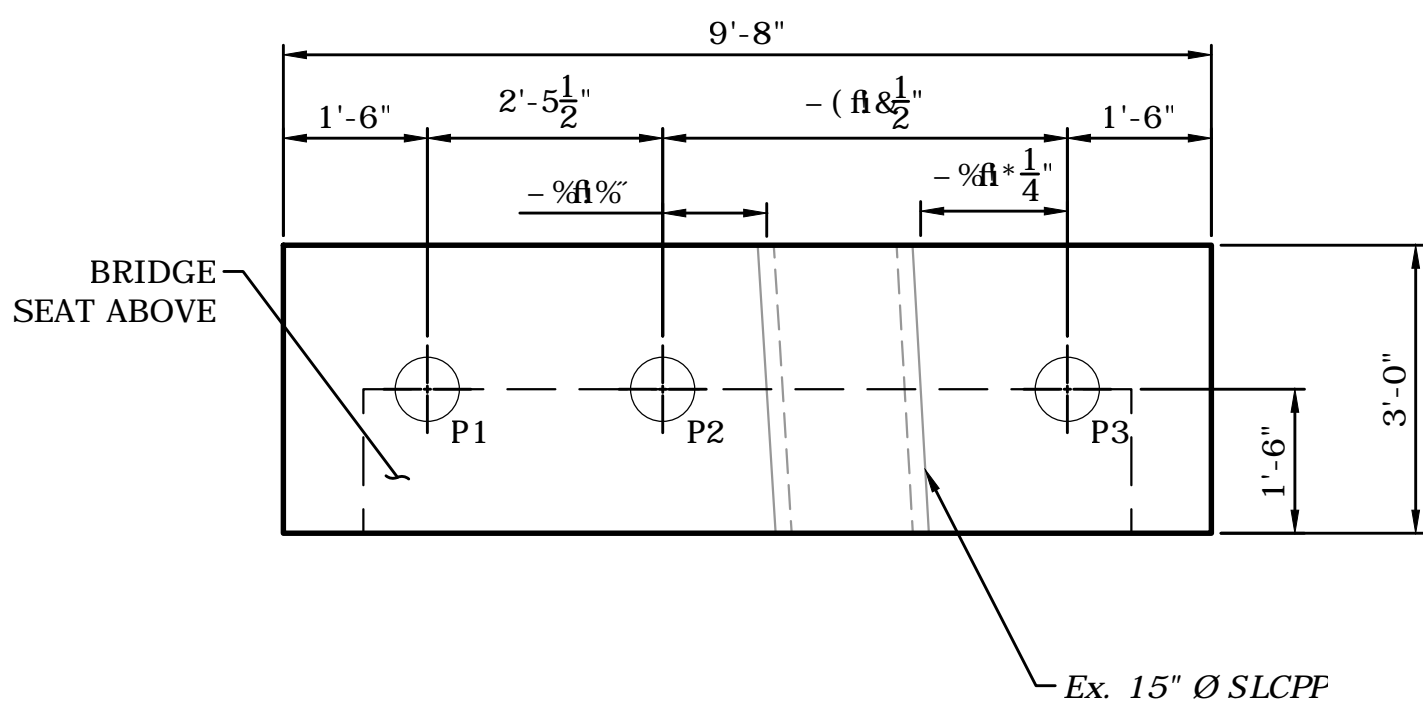
ABUTMENT NO. 1 ELEVATION
SCALE: 1" = 2'-0"



ABUTMENT NO. 2 ELEVATION
SCALE: 1" = 2'-0"



ABUTMENT NO. 1 PILE LAYOUT PLAN
SCALE: 1" = 2'-0"



ABUTMENT NO. 2 PILE LAYOUT PLAN
SCALE: 1" = 2'-0"

PILE NOTES

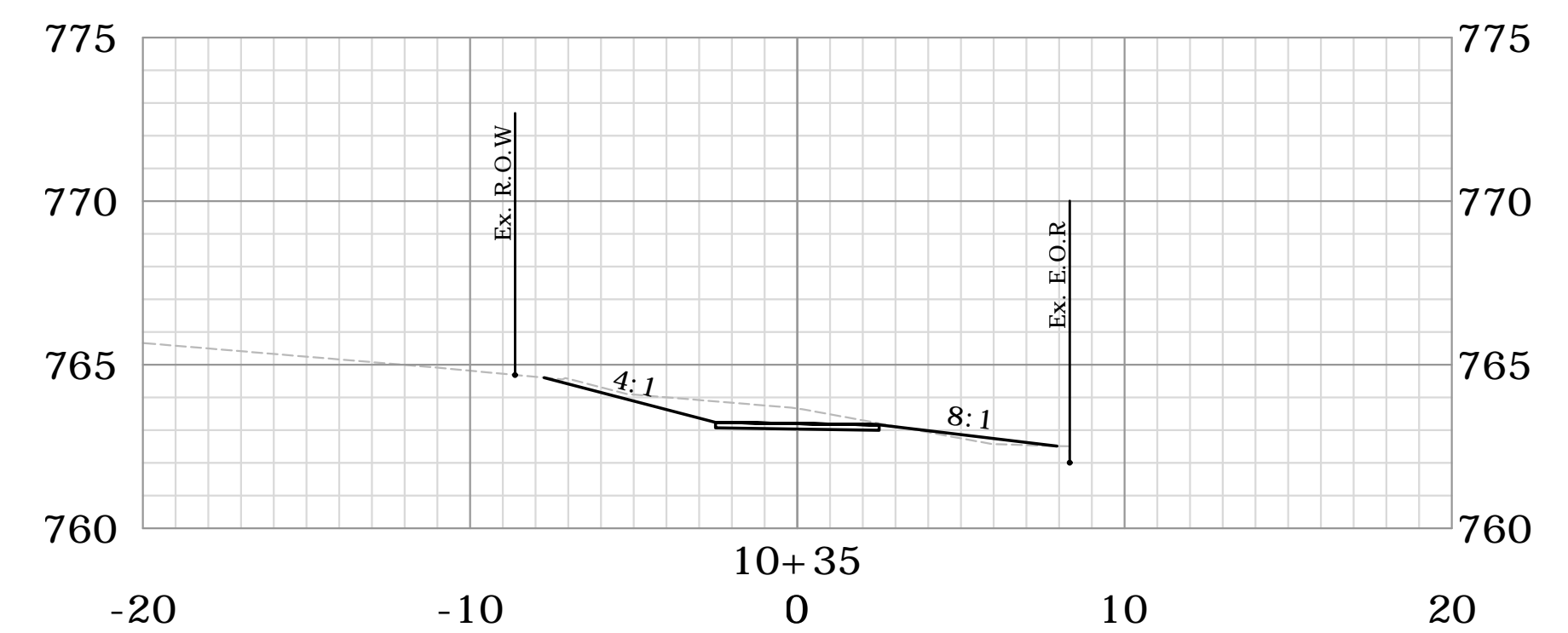
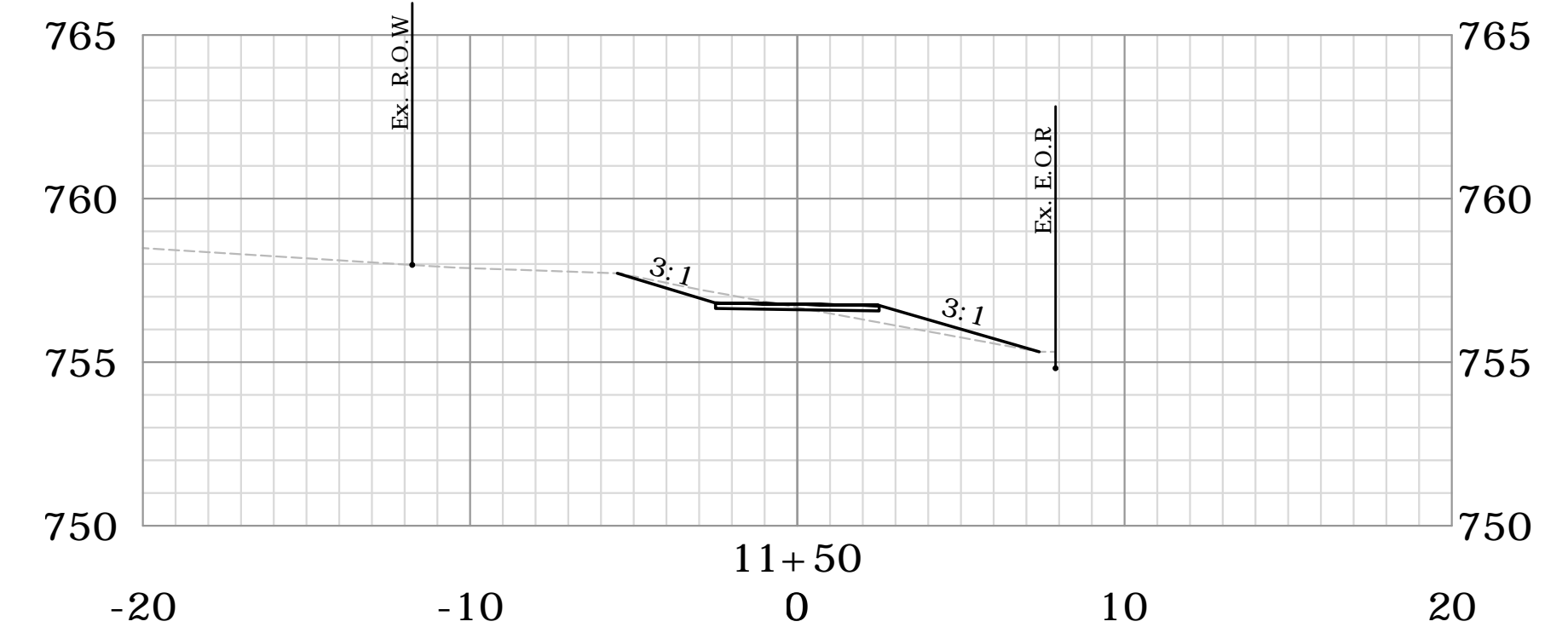
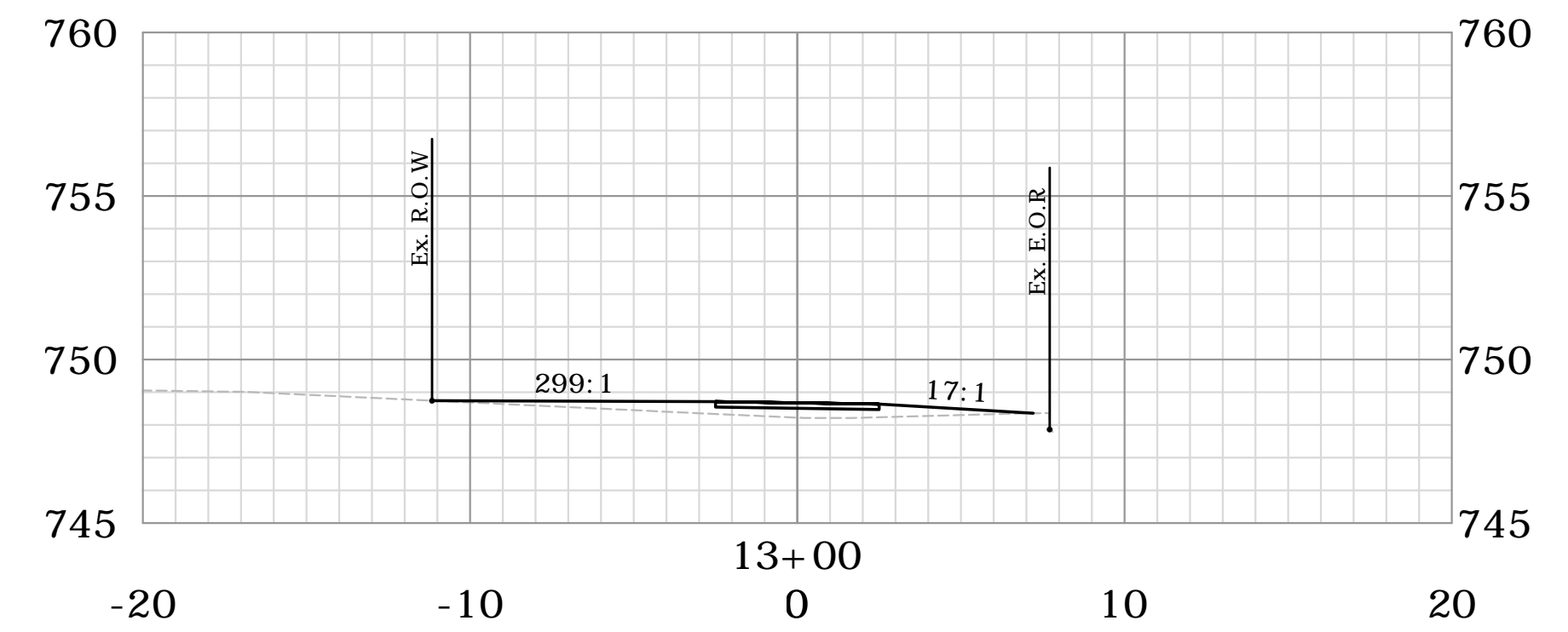
- ALL PILES SHALL BE SET VERTICAL.
- ESTIMATE OF PILES REQUIRED:
ABUTMENT NO. 1: 15 PILES
ABUTMENT NO. 2: 15 PILES
- THE PILES SHALL BE FROM SOUTHERN PINE OR DOUGLAS FIRM CONFORMING TO ASTM D25 AND AWPA STANDARDS FOR CLASS 1 OR B PILES. THE PILES SHALL HAVE PRESSURE TREATED PRESERVATIVE ACCEPTABLE FOR AWPA USE CATEGORY UC4C. PILES SHALL BE DRIVEN WITH A HAMMER ENERGY OF 9,300 TO 15,000 FT-LBS. BASED ON THE RELATIVELY LOW CAPACITY, THE CAPACITY IN THE FIELD CAN BE EVALUATED WITH THE ENGINEERING NEWS RECORD FORMULA. THE PILES SHALL BE DRIVEN NO MORE THAN 12 BLOWS PER 11N TO AVOID BROOMING OF THE PILES.

ULTIMATE PILE CAPACITY

ABUTMENT NO. 1	13.1 TONS
ABUTMENT NO. 2	13.1 TONS

DESCRIPTION	DATE	BY

SEP DESIGNED	WRS DRAWN	SEP CHECKED
AS SHOWN		
MARCH 25, 2022		
13039.00006		
STR-03		

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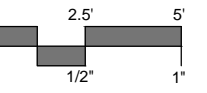
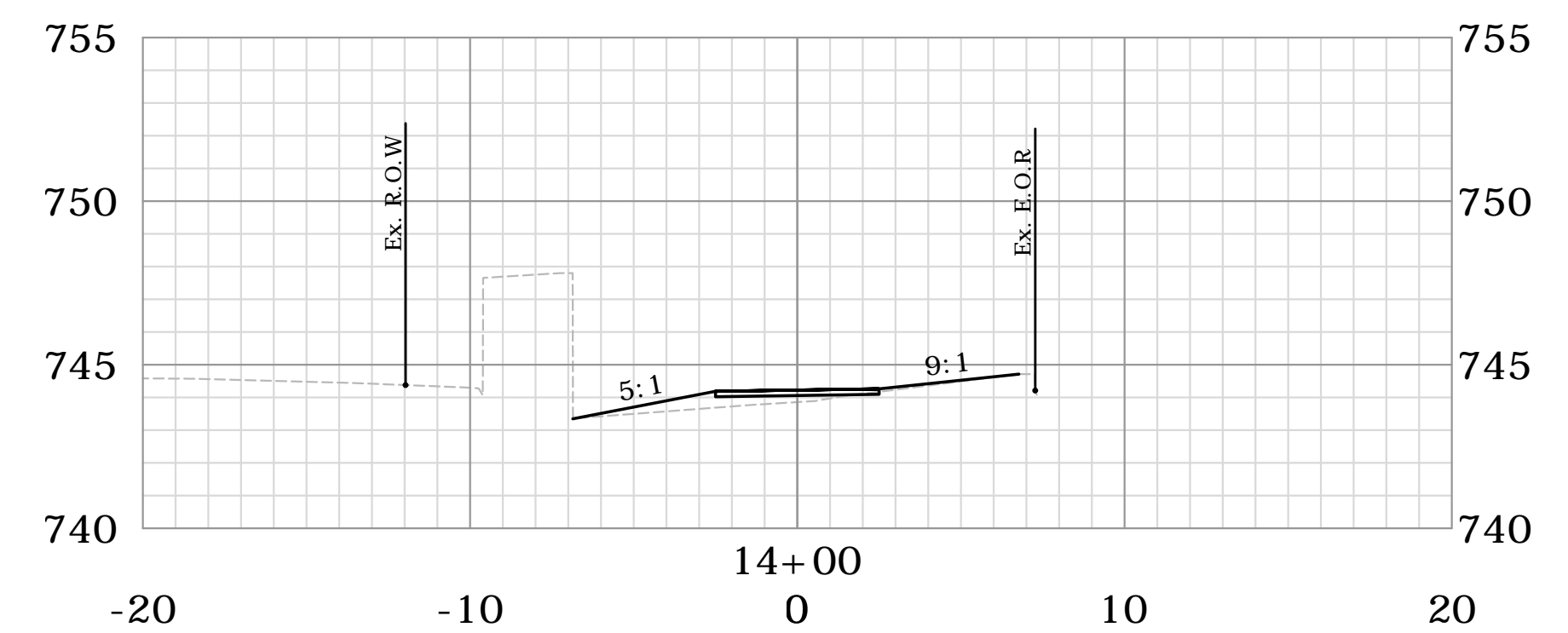
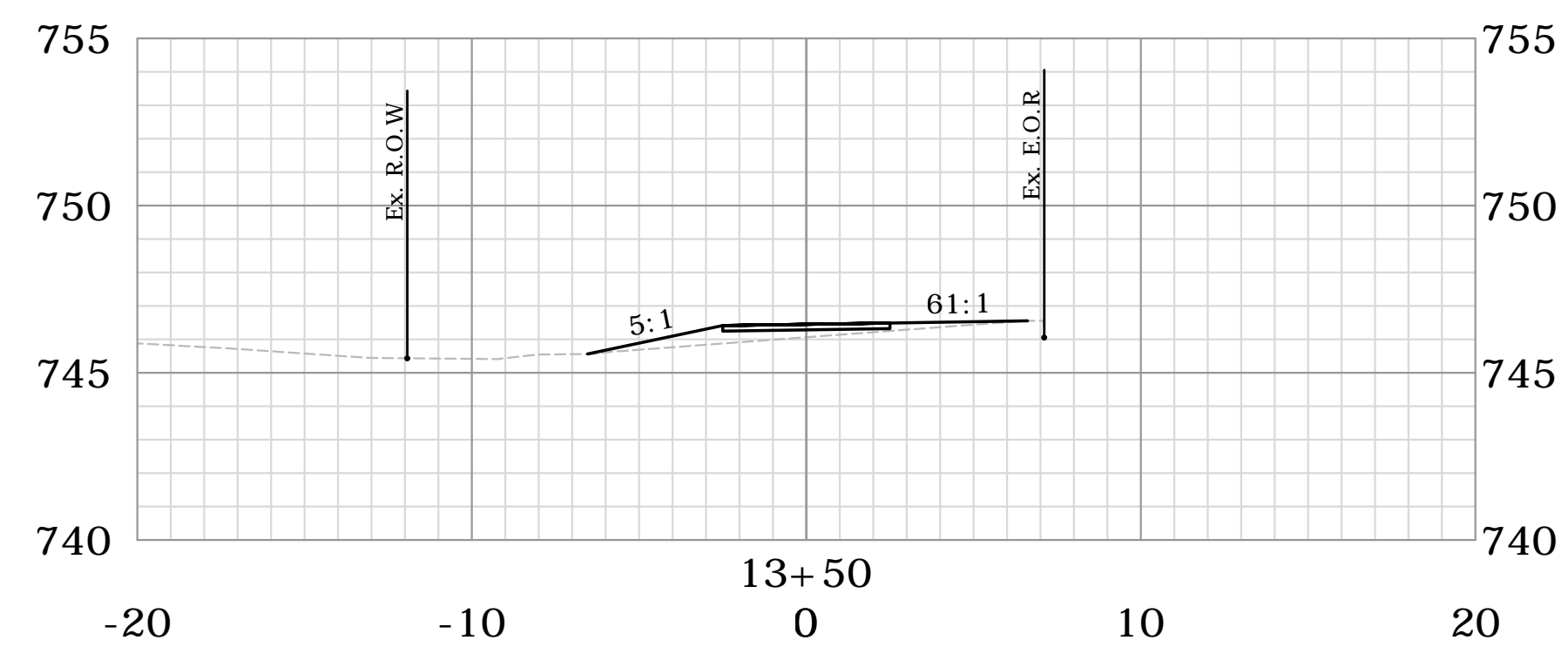
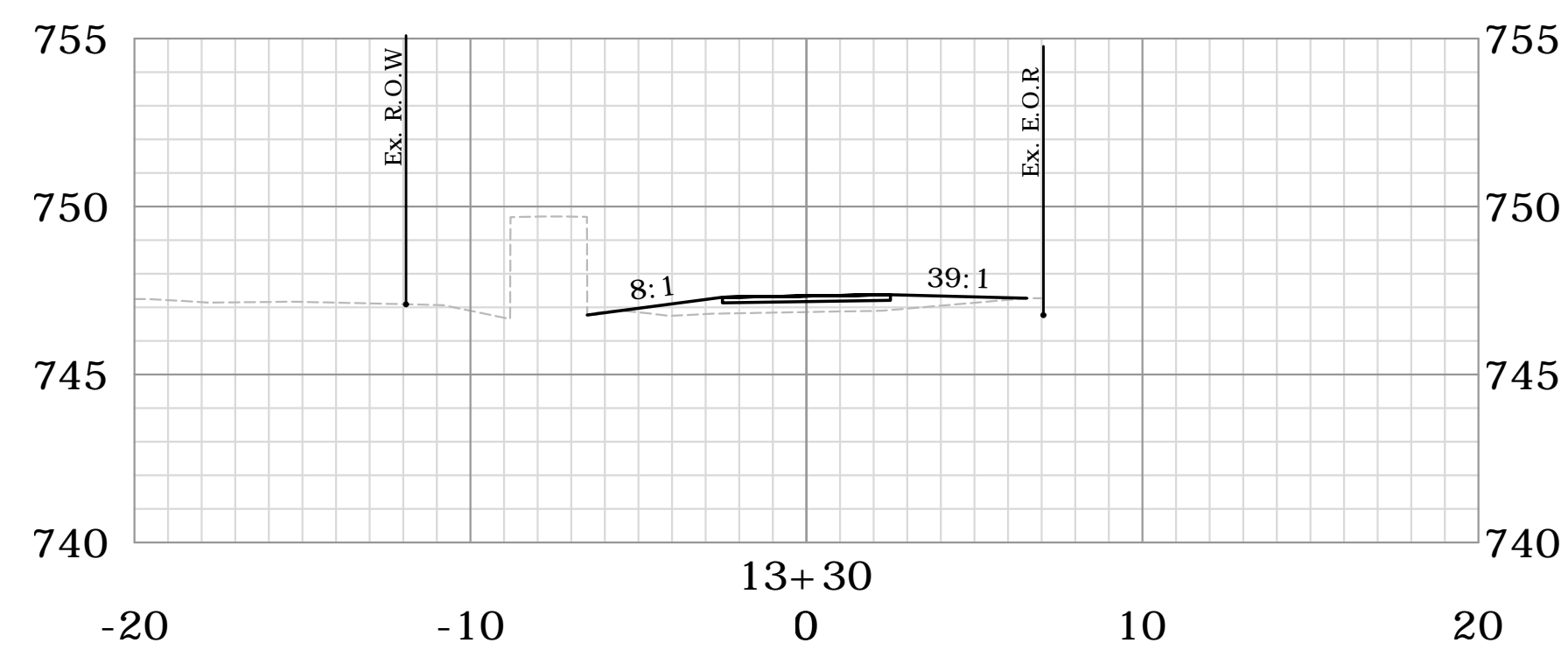
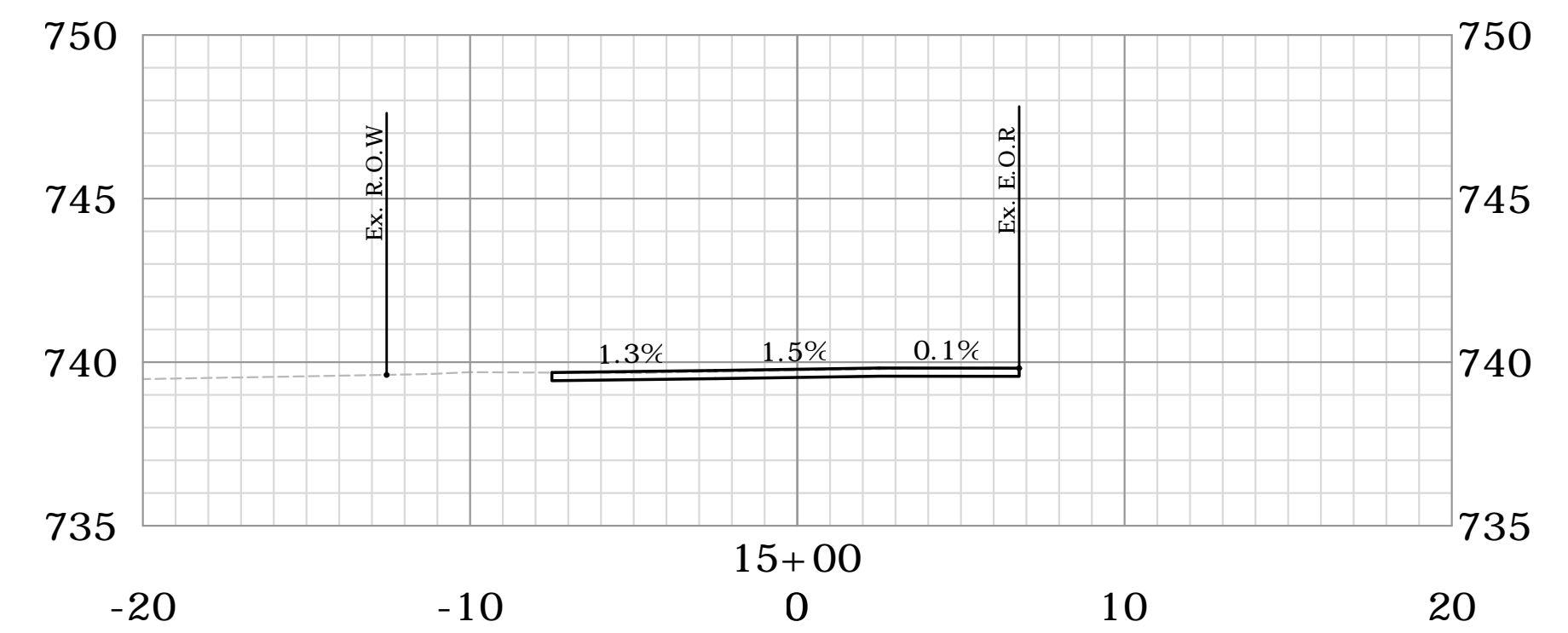
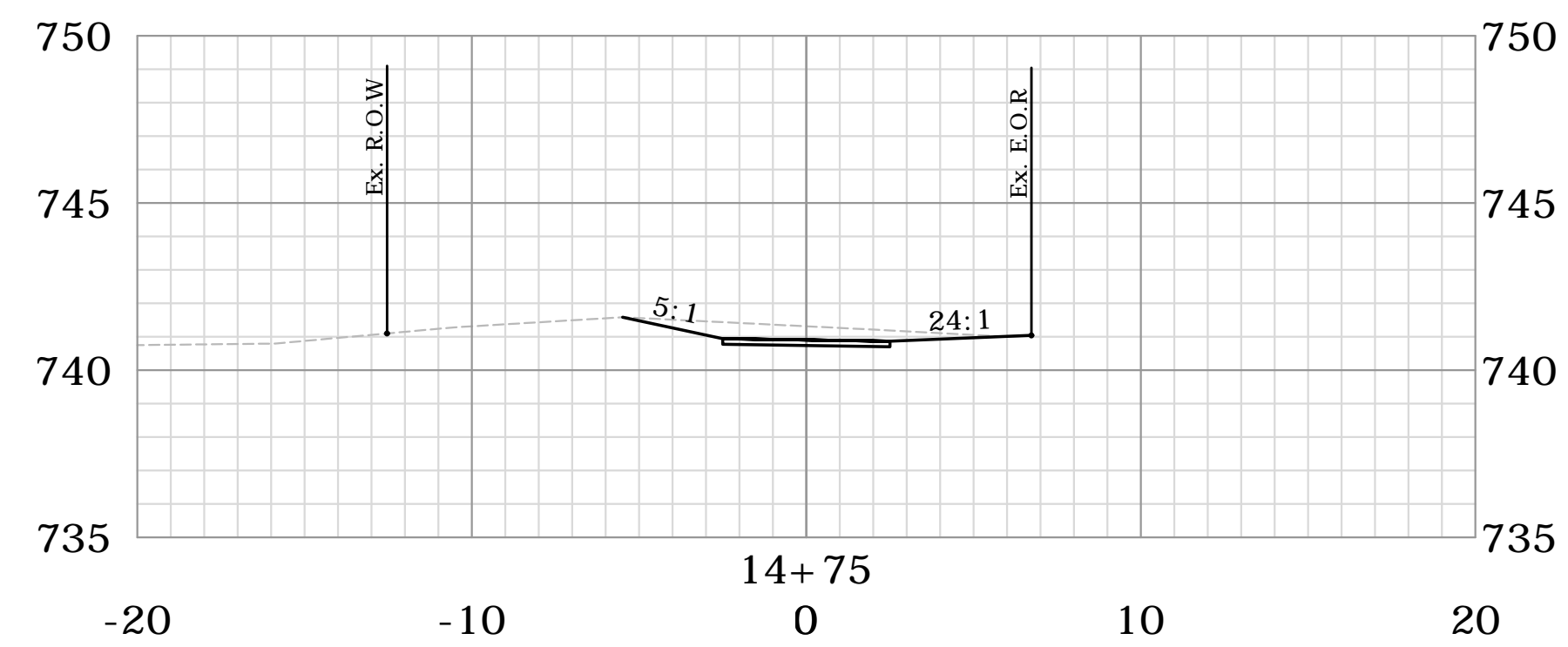
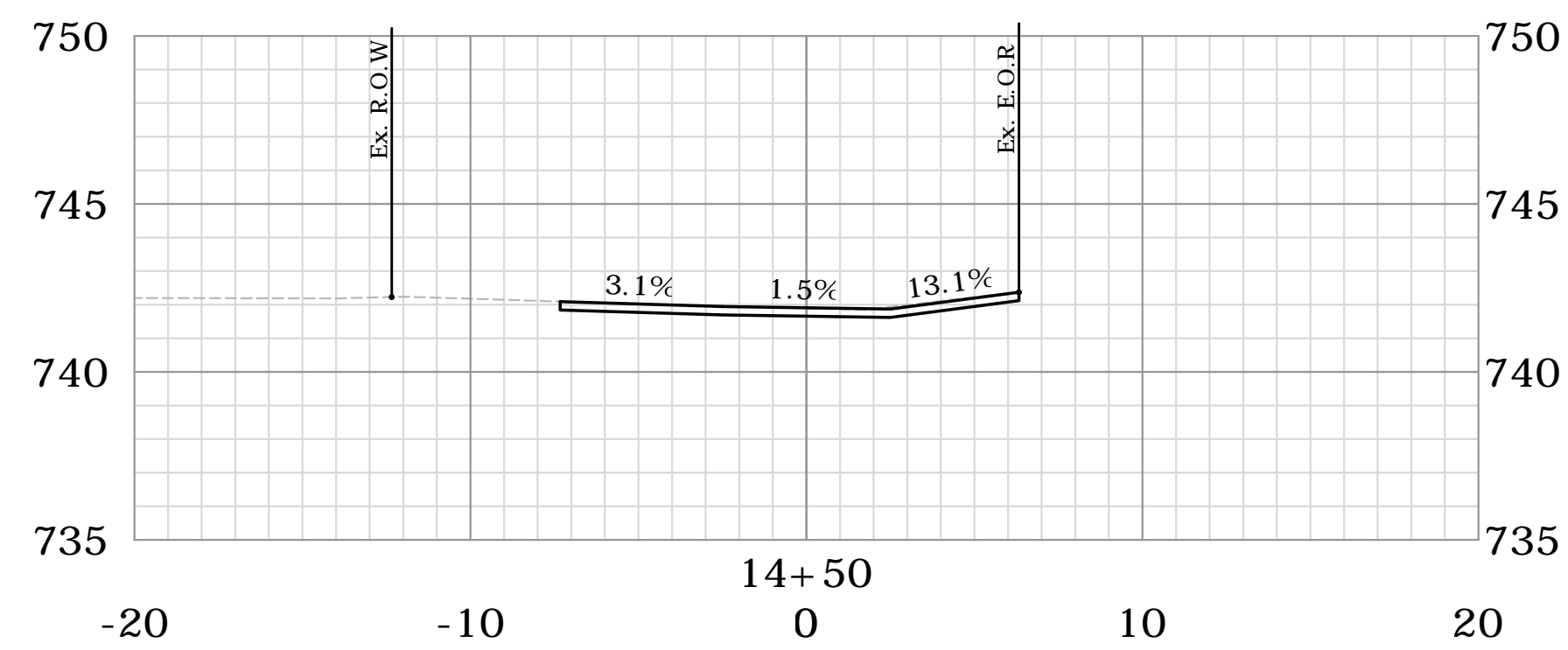
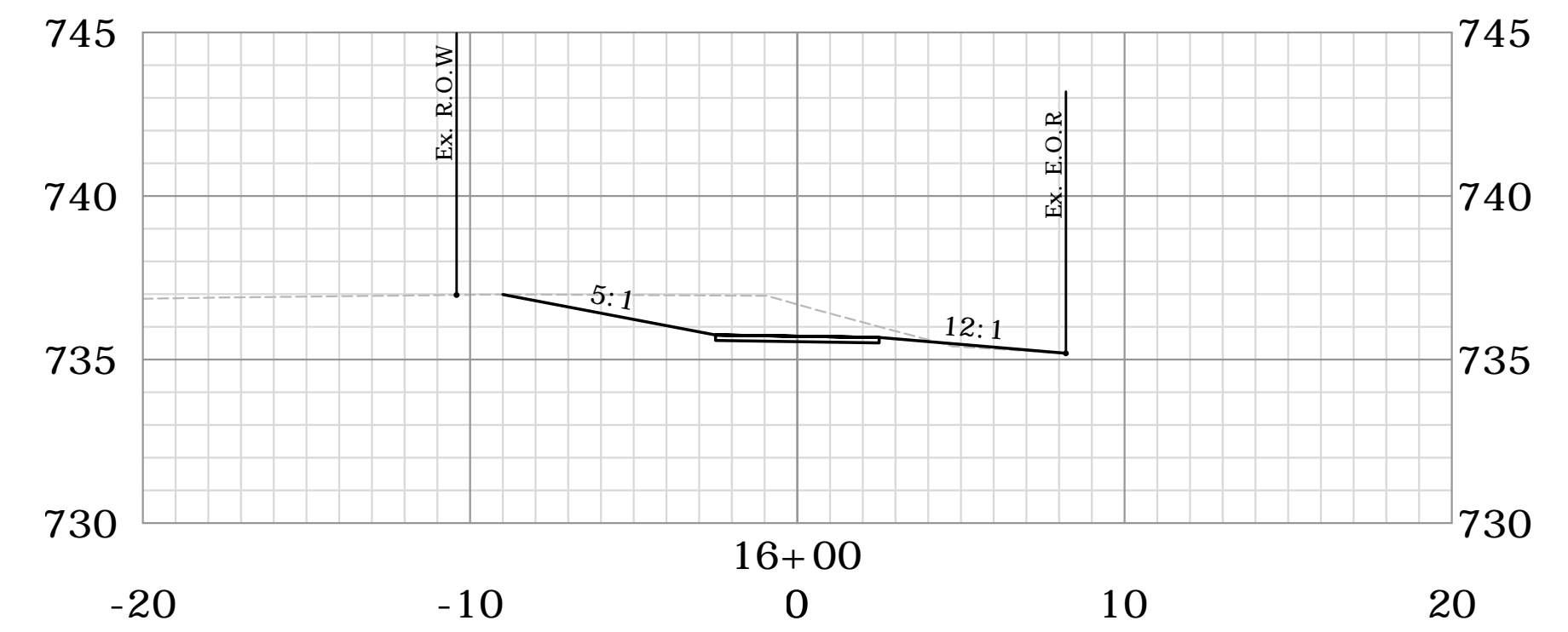
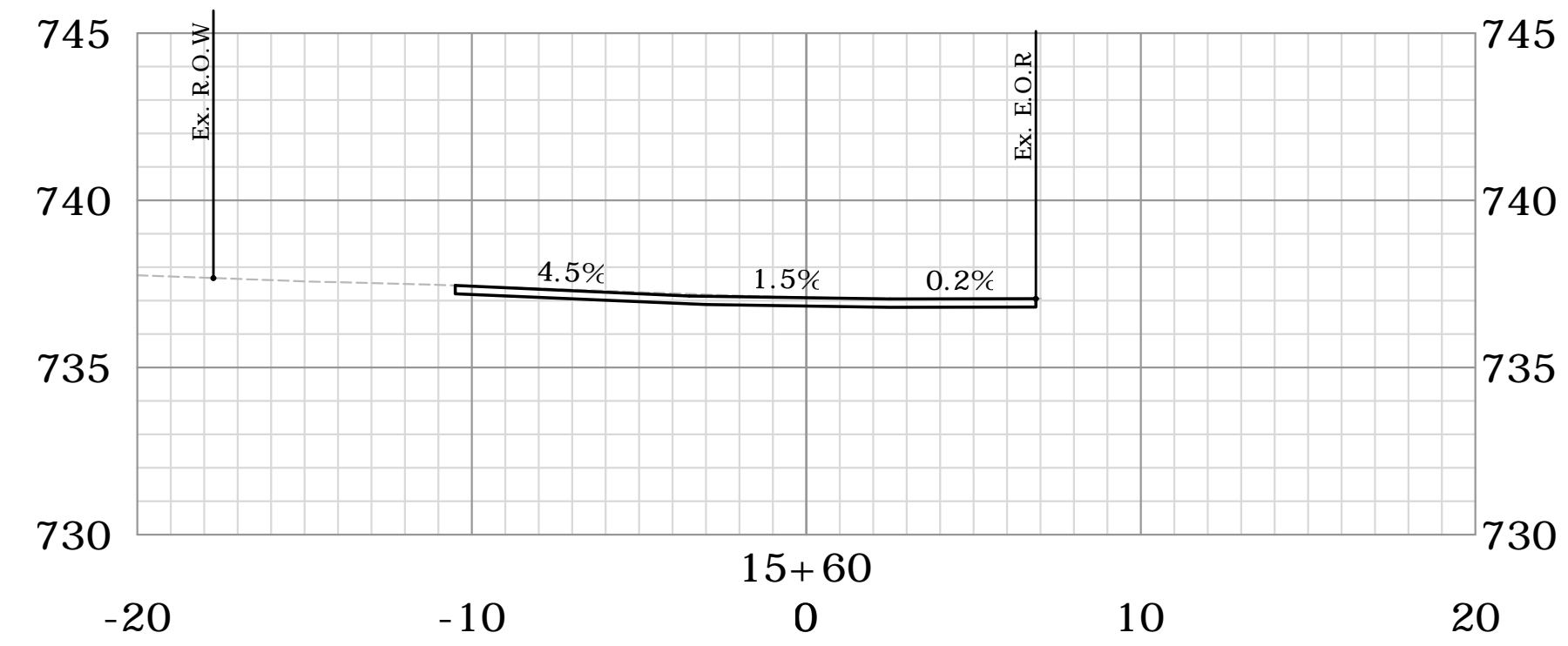
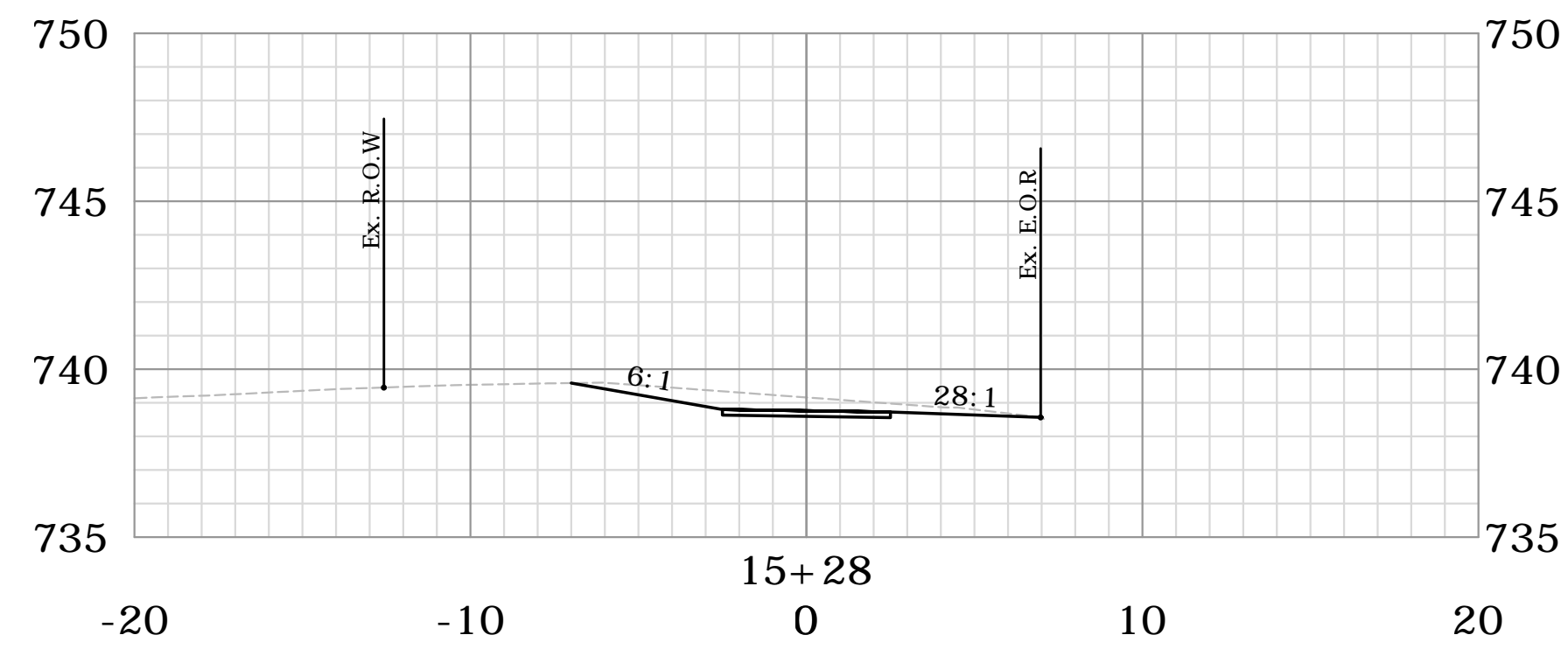
SIDEWALK CROSS SECTIONS

CT S.R. 44 (MAIN ST.) SIDEWALK IMPROVEMENTS

CT S.R. 44 (MAIN ST.)
SALISBURY, CONNECTICUT

MSM DESIGNED	MSM DRAWN	AAC CHECKED
1"=5'		
MARCH 25, 2022		
13039.00006		
XSC-01		

16

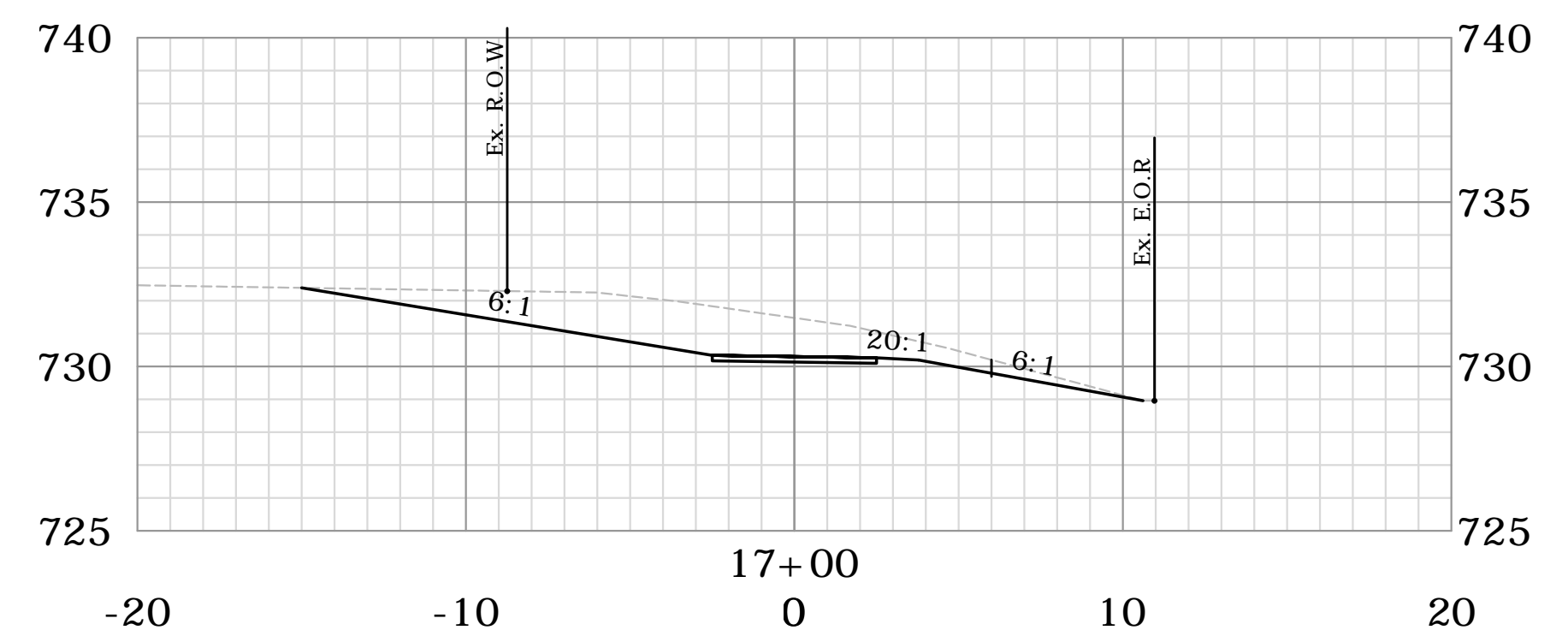
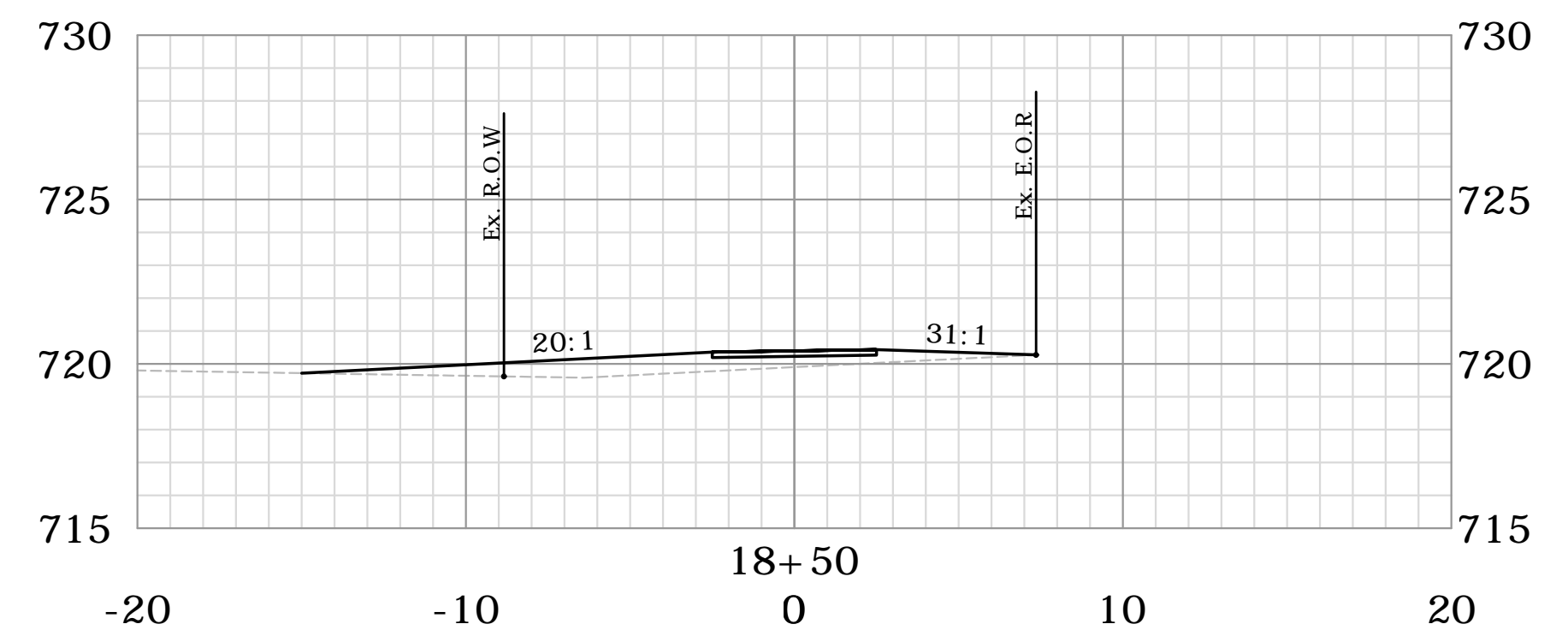
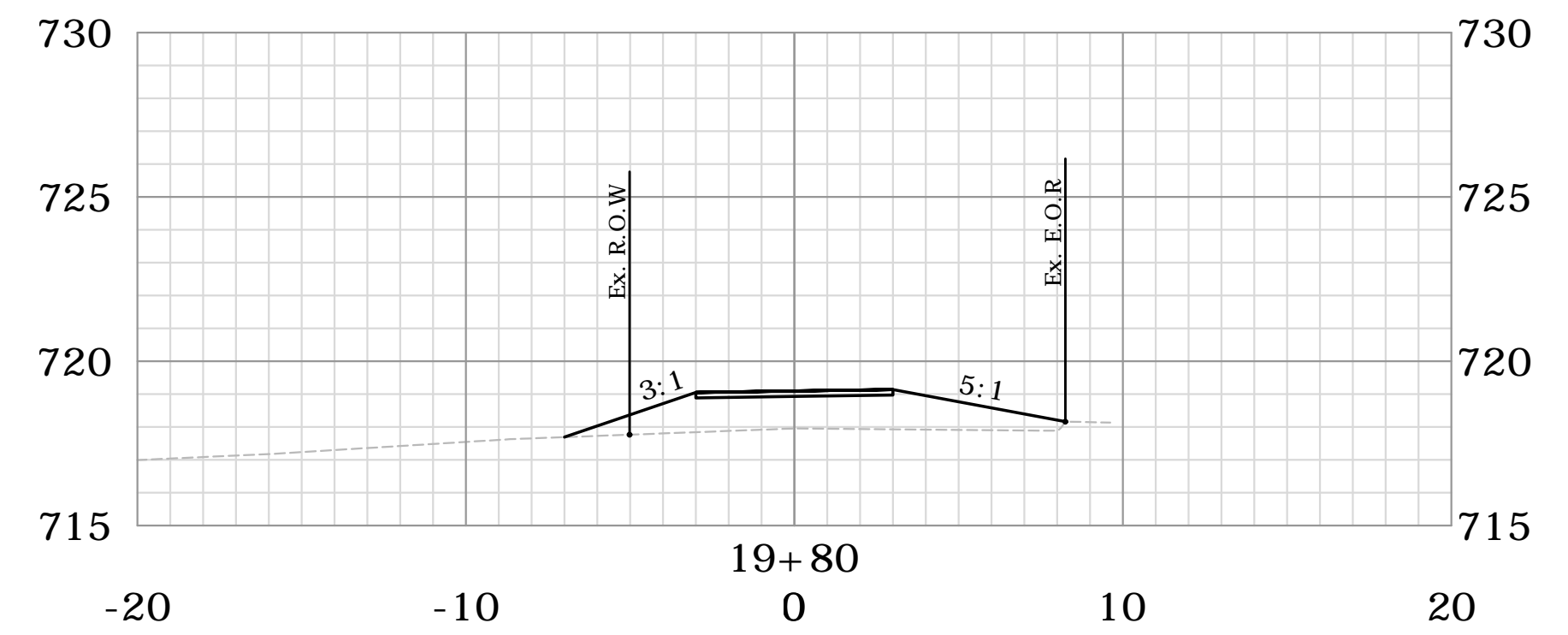


CT S.R. 44 (MAIN ST.) SIDEWALK IMPROVEMENTS

CT S.R. 44 (MAIN ST.)
SALISBURY, CONNECTICUT

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1"=5'		
MARCH 25, 2022		
13039.00006		
XSC-02		

17

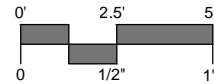
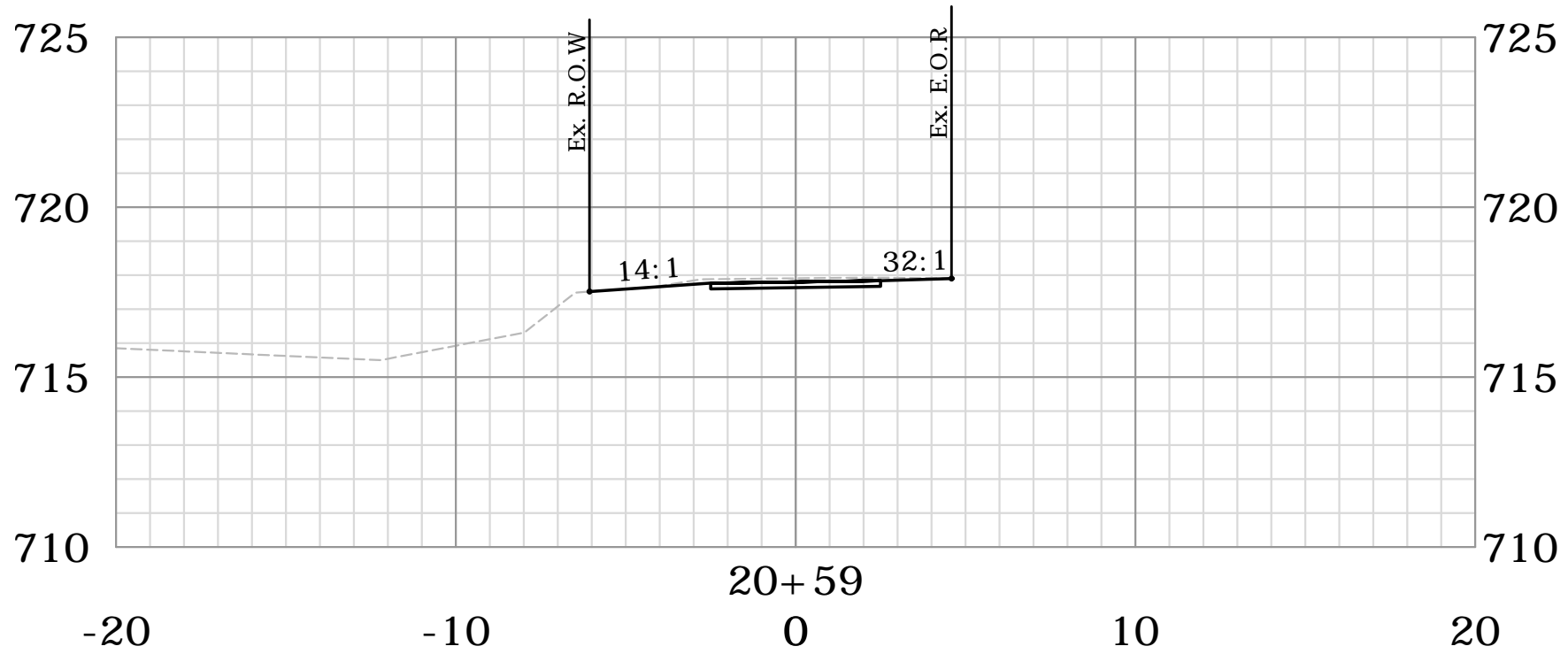
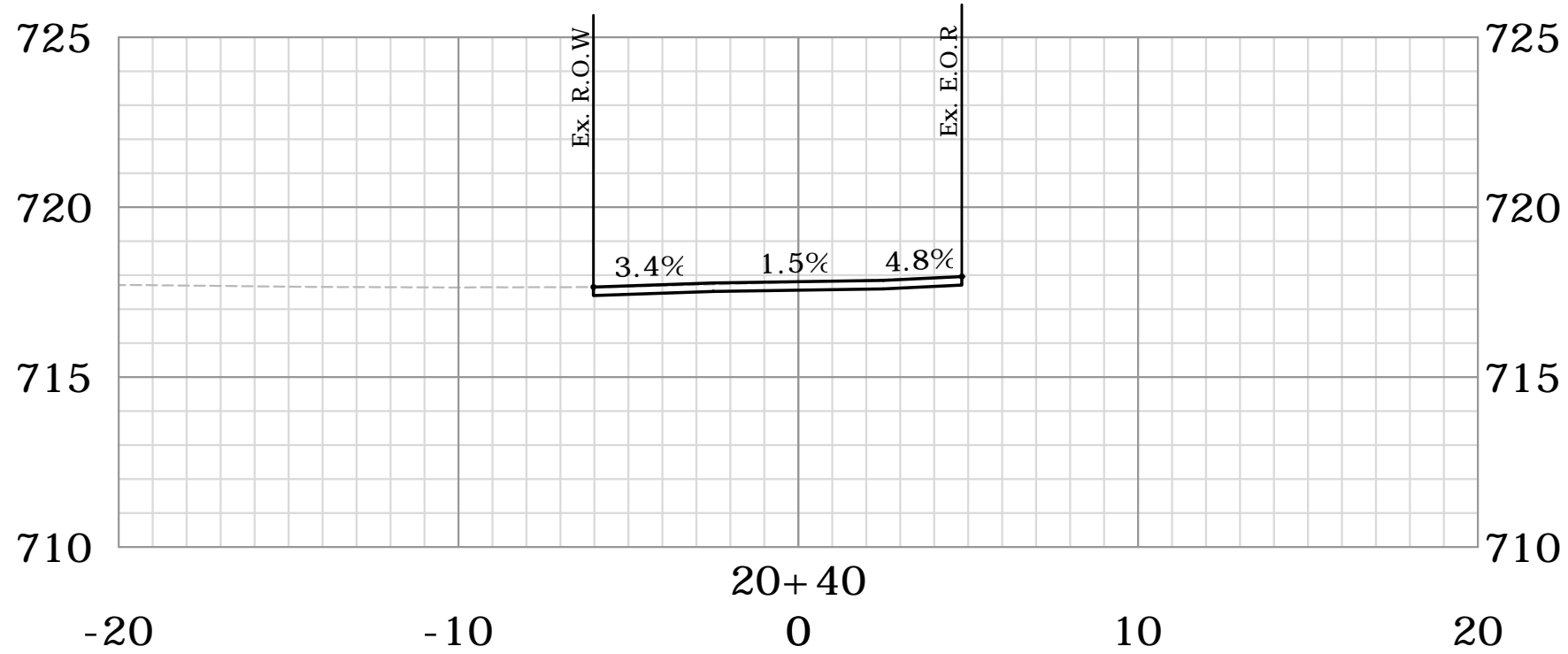
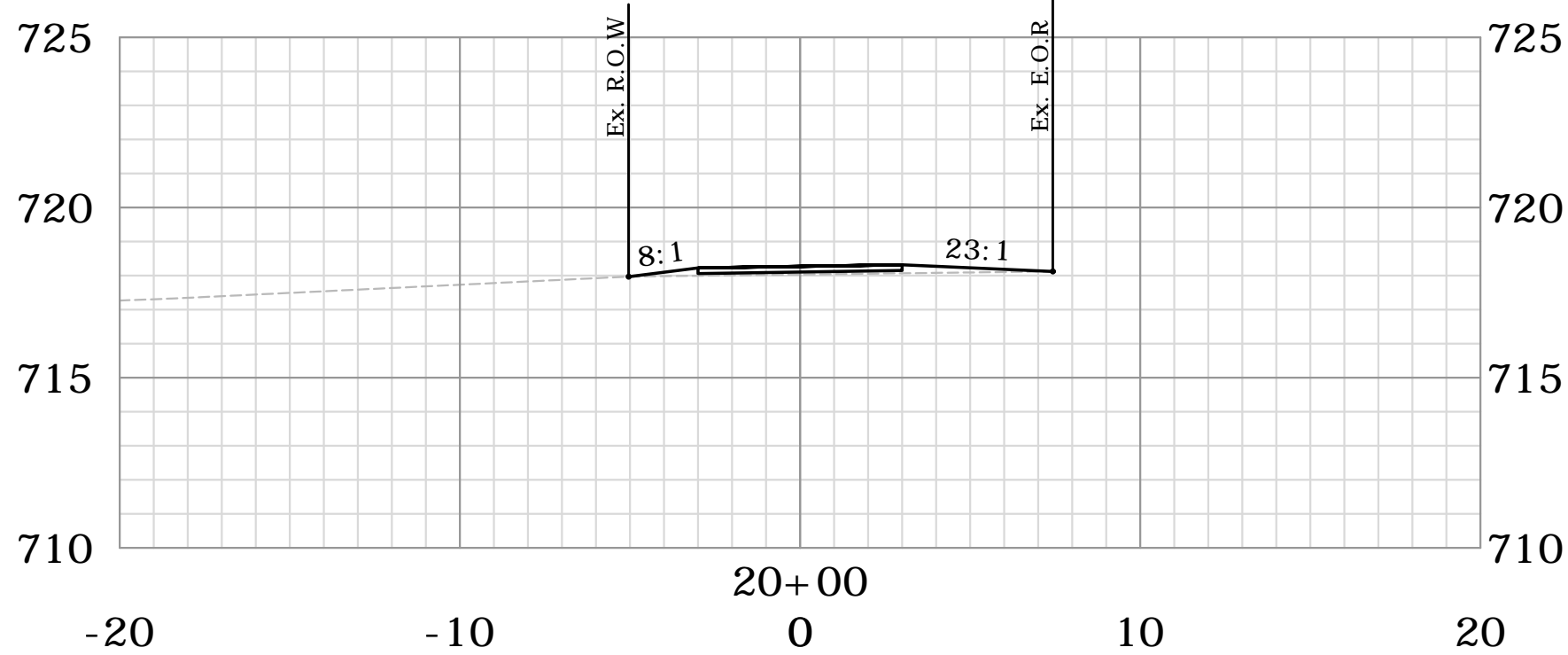
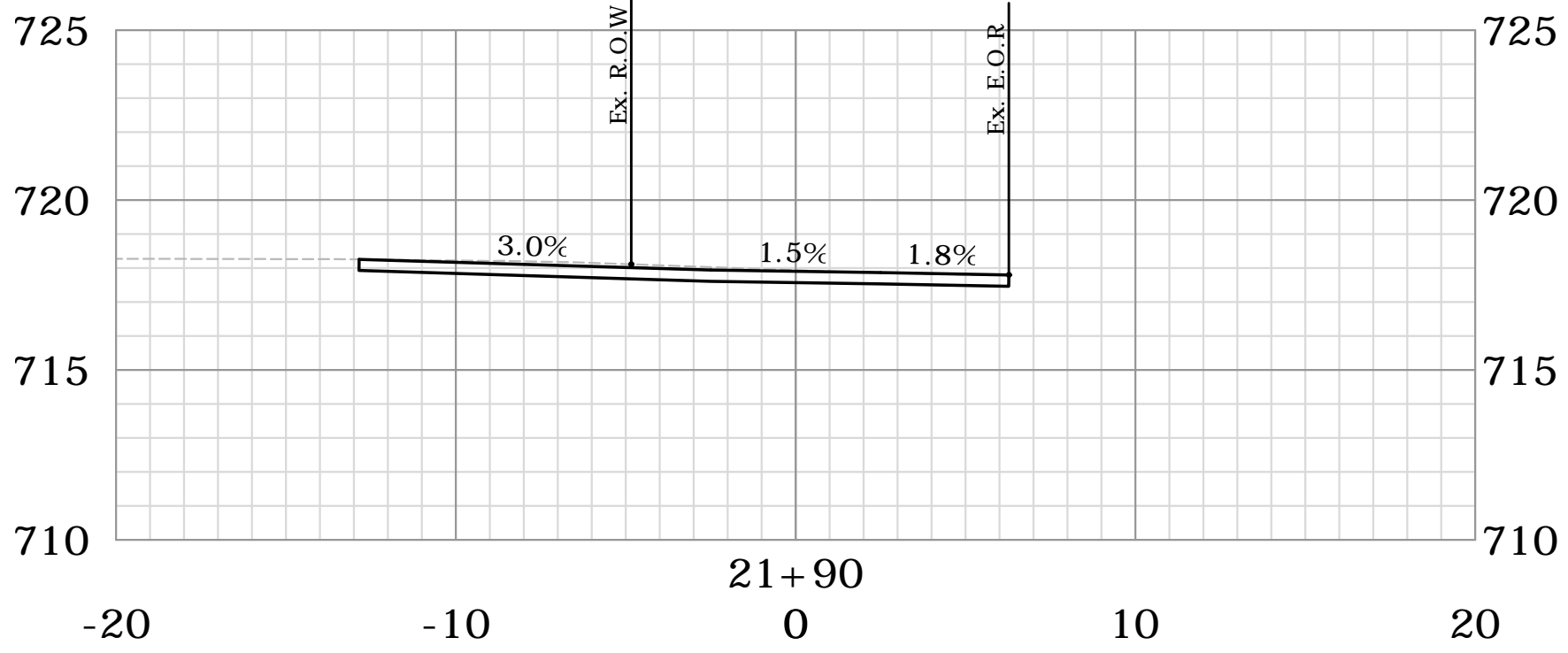
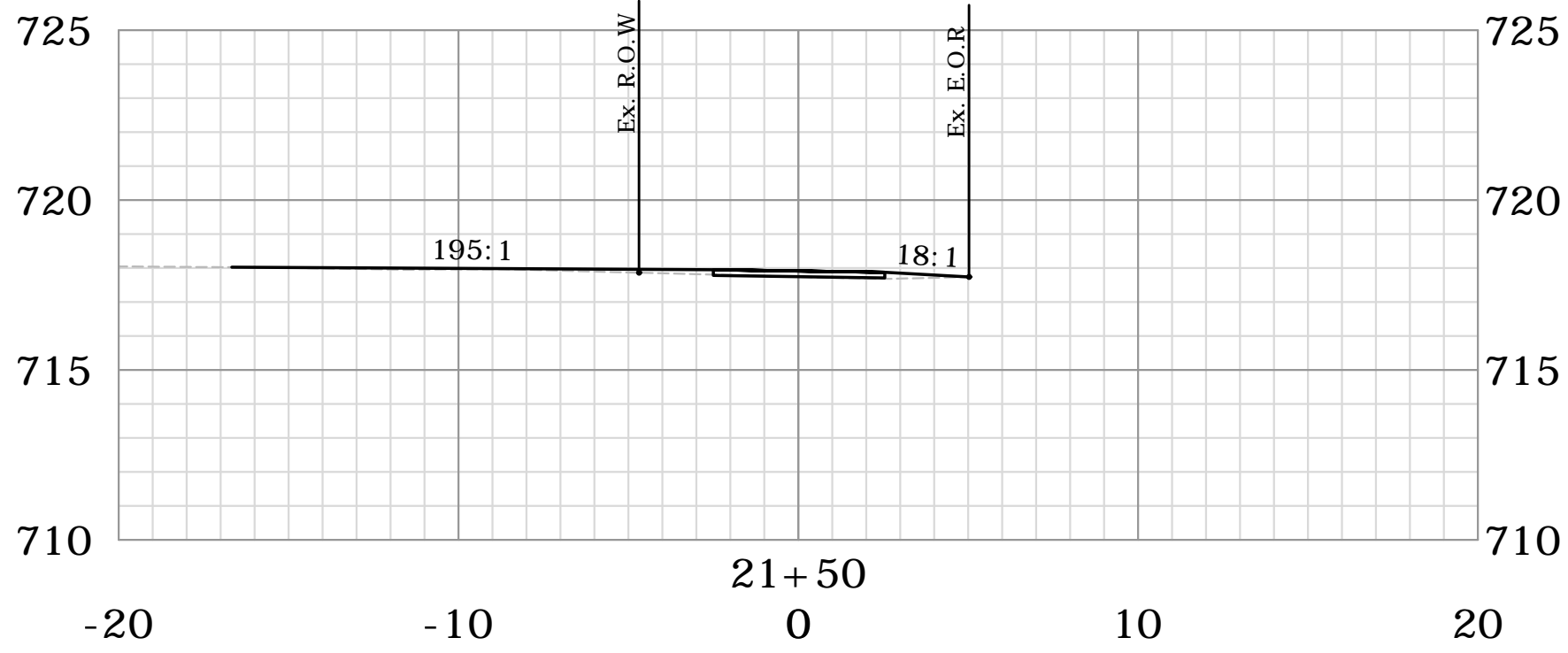
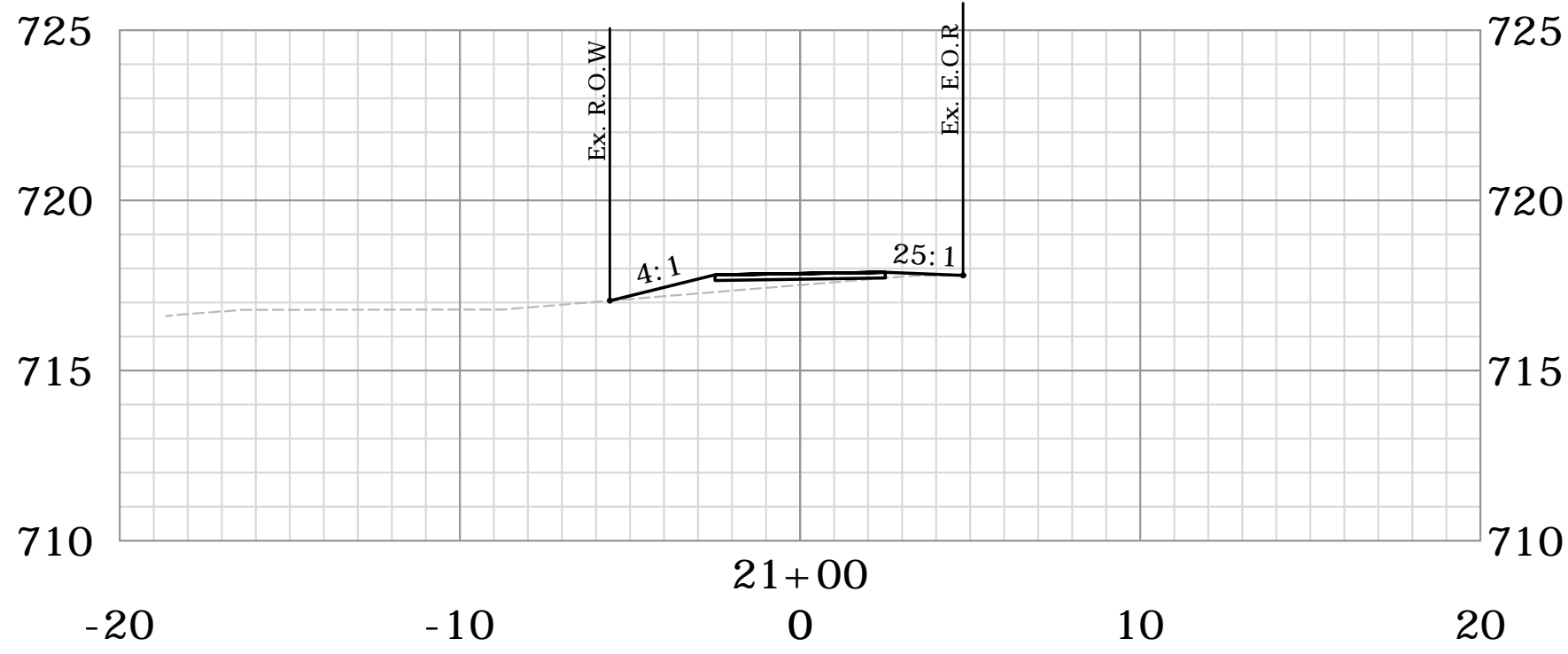
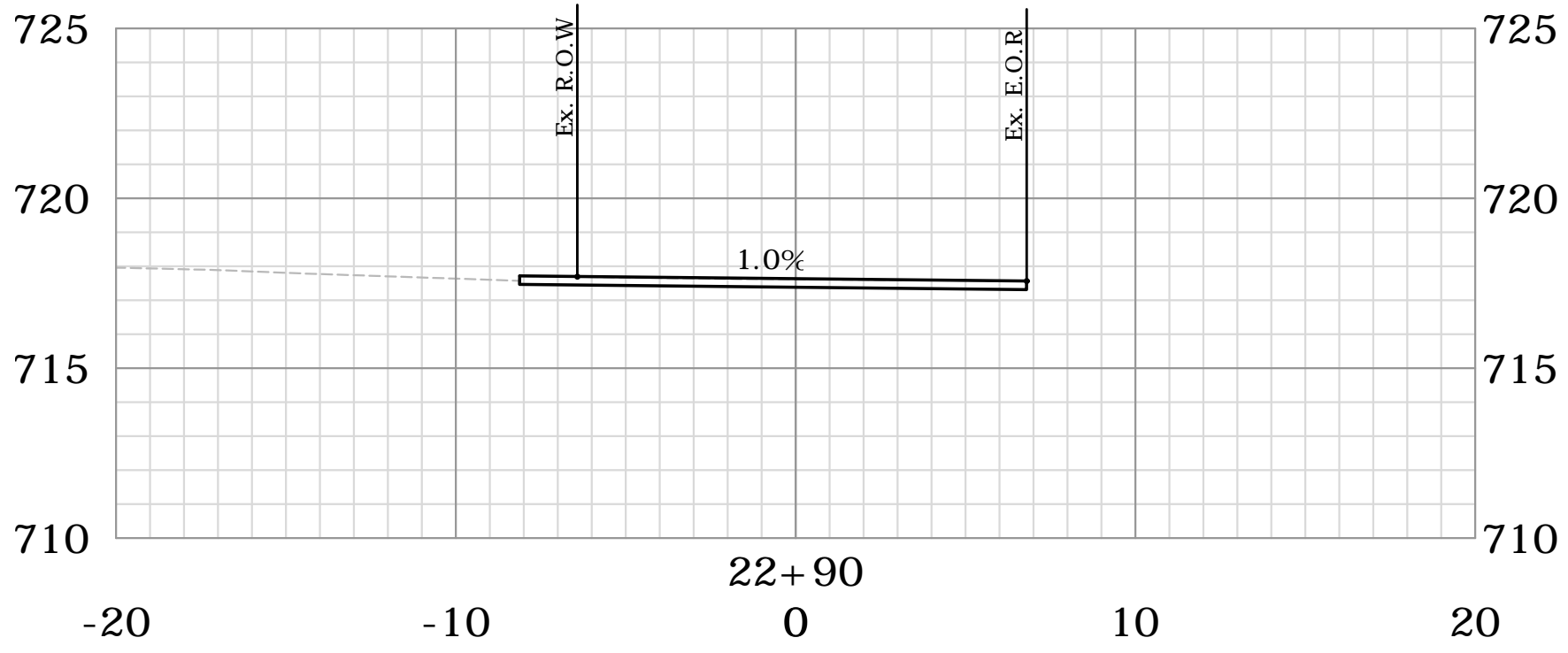
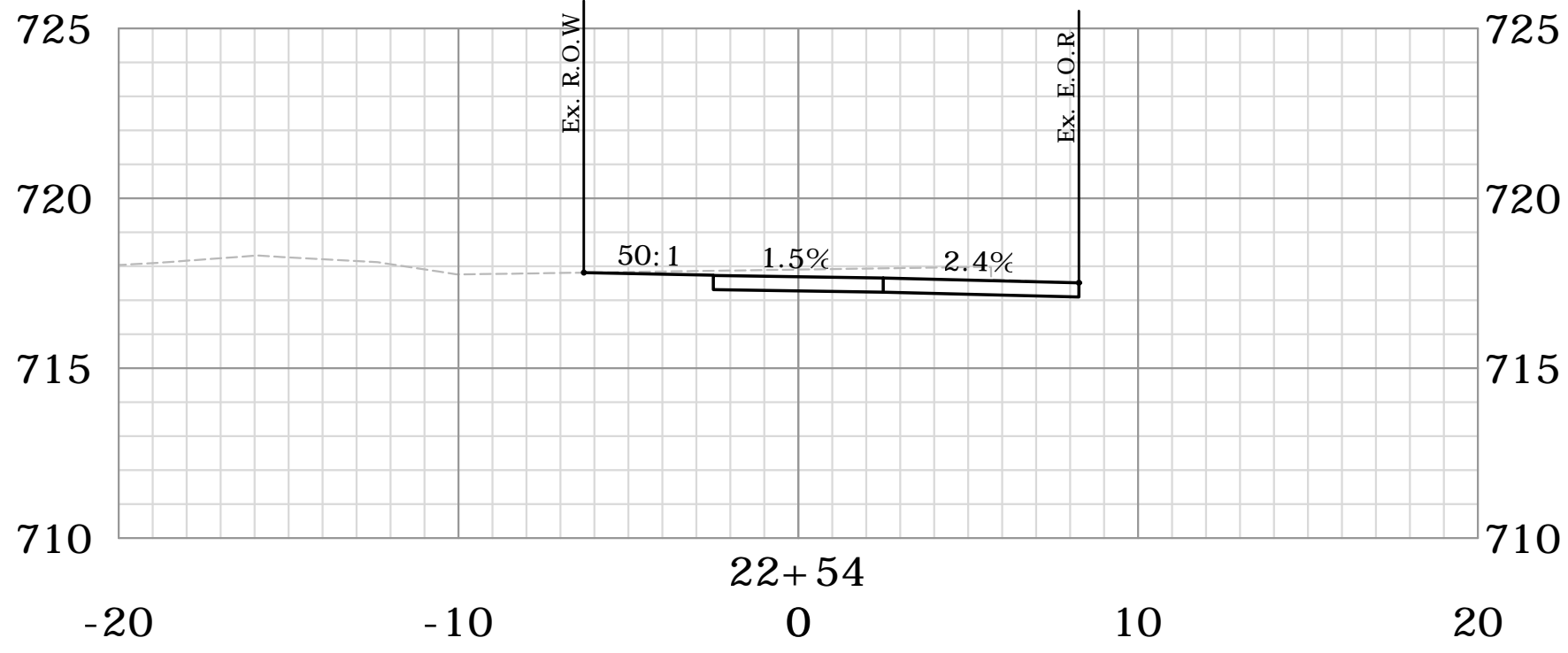
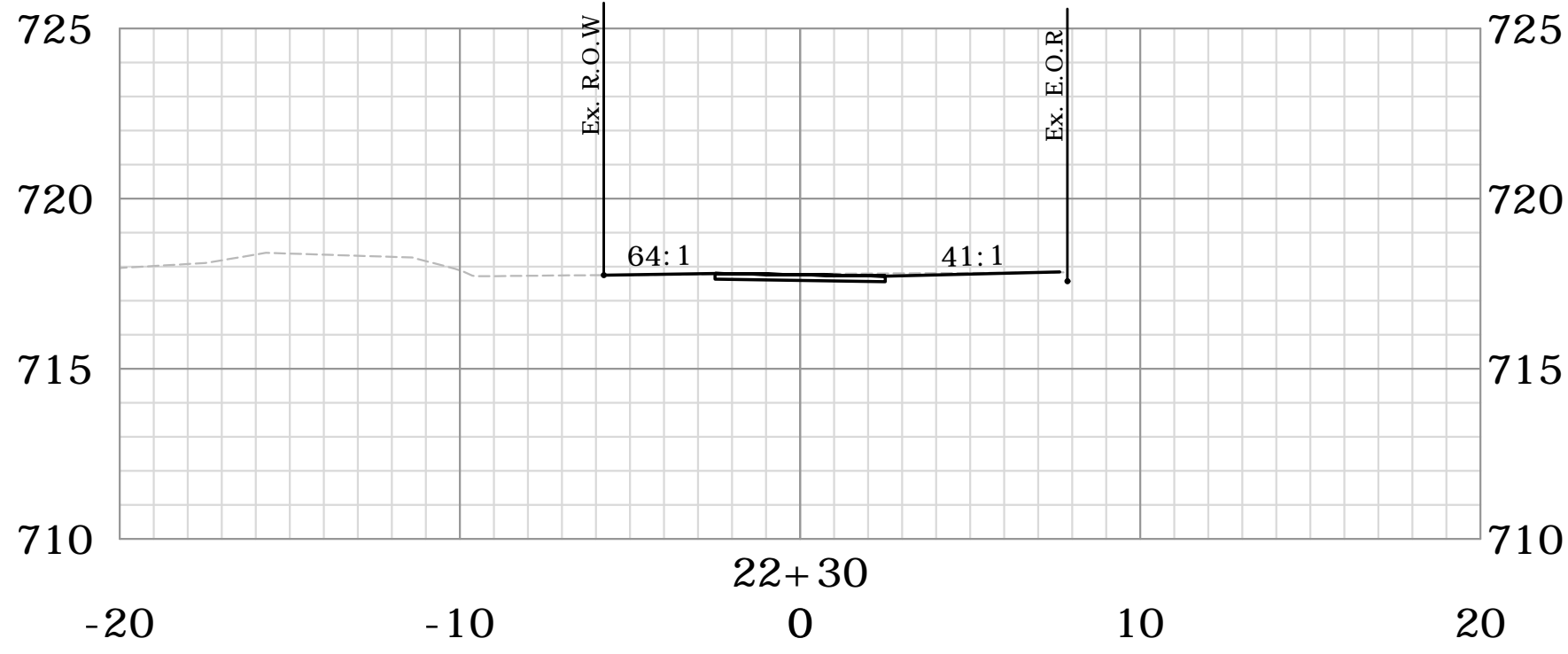
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CT S.R. 44 (MAIN ST.)
SALISBURY, CONNECTICUT

MSM SIGNED	MSM DRAWN	AAC CHECKED
1"=5'		
DATE MARCH 25, 2022		
PROJECT NO. 13039.00006		
DRAWING NO. XSC-03		

06

MSM
DATE
PROJECT NO.
DRAWING NO.



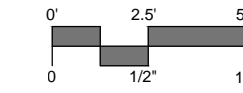
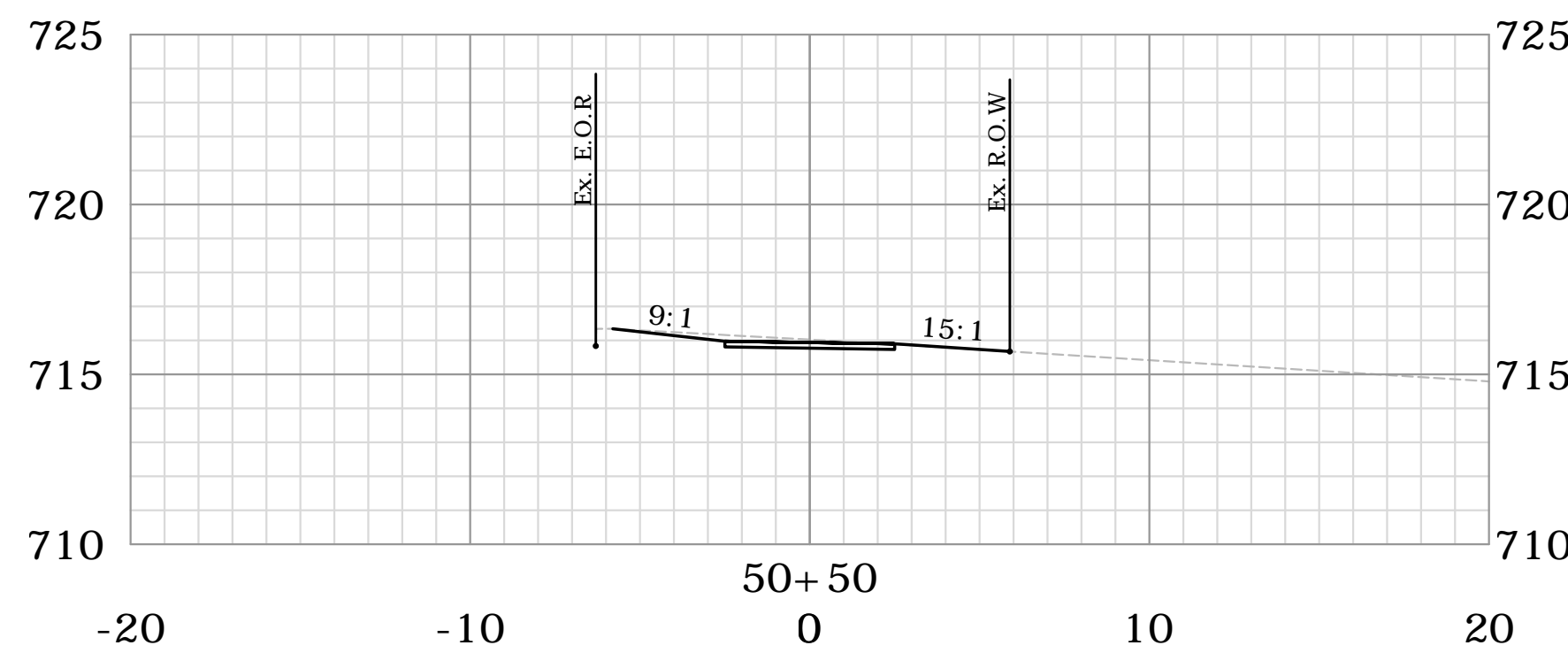
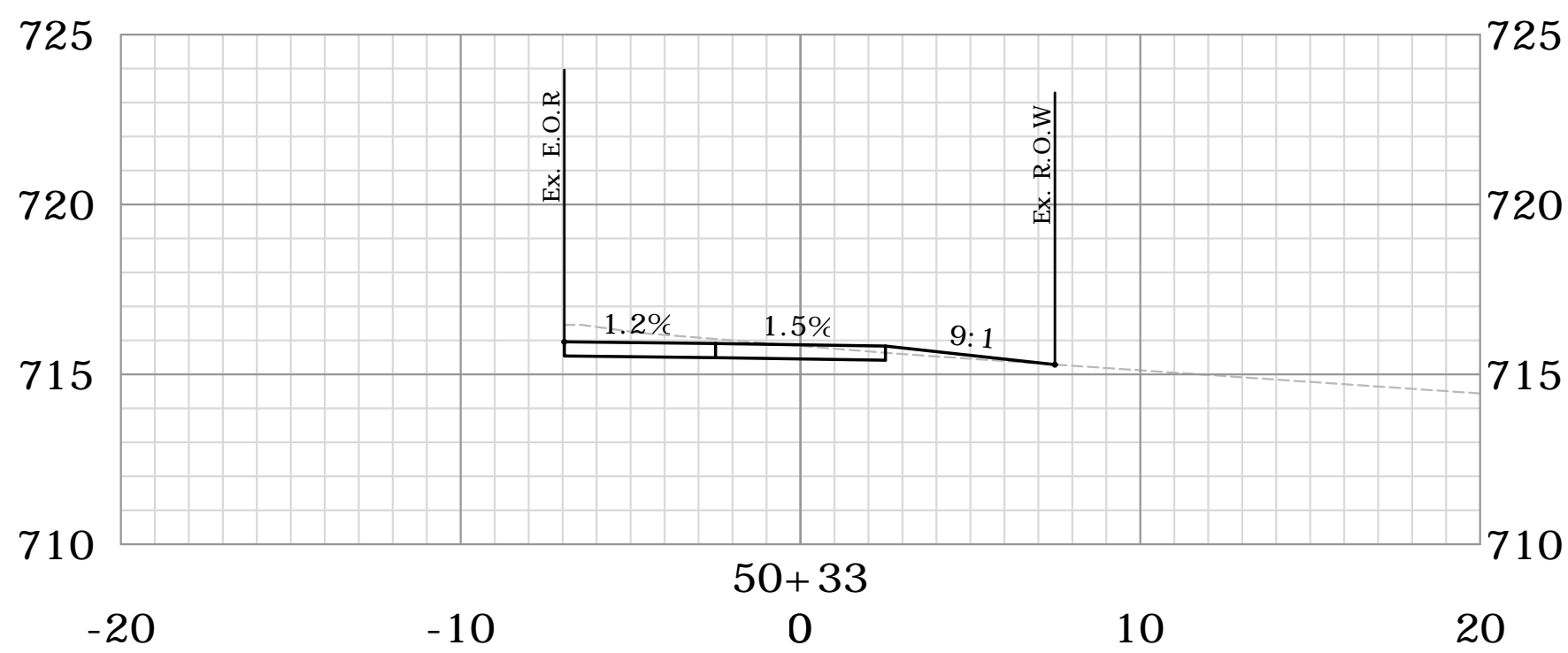
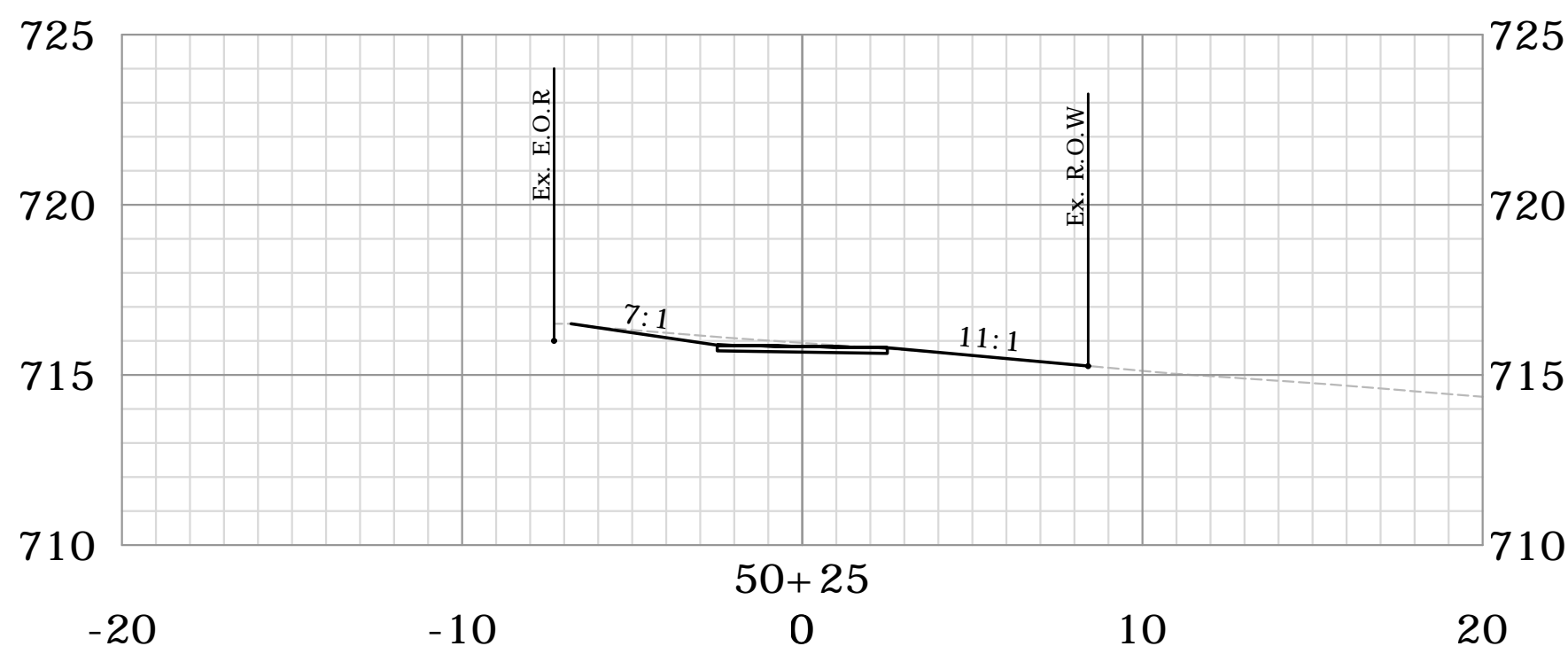
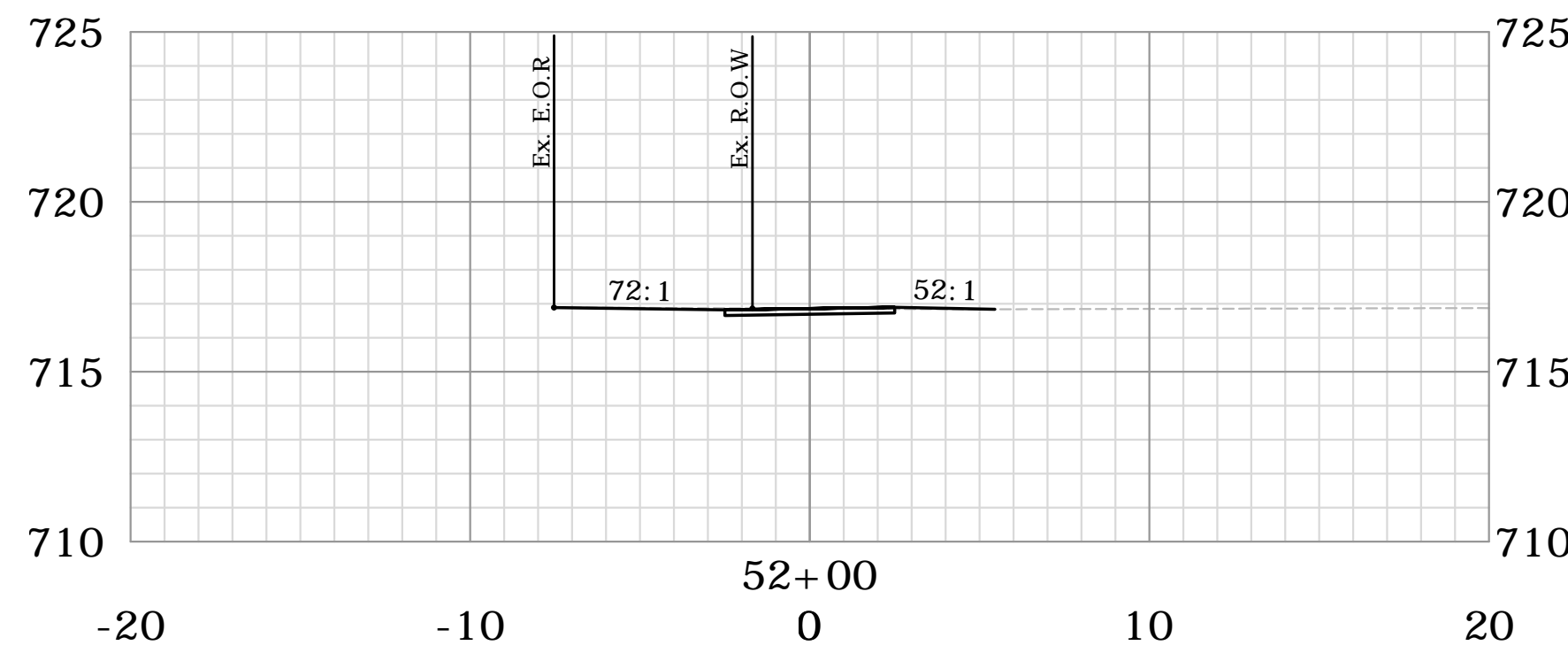
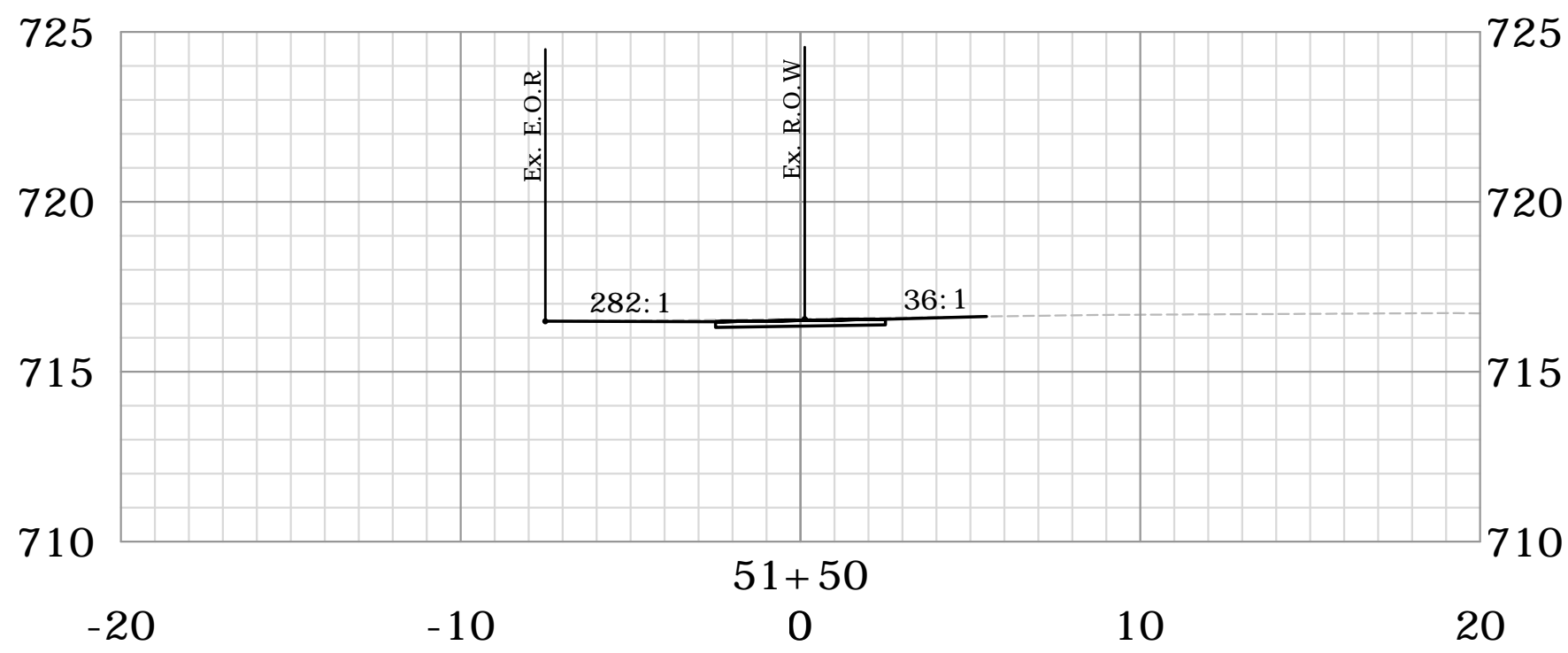
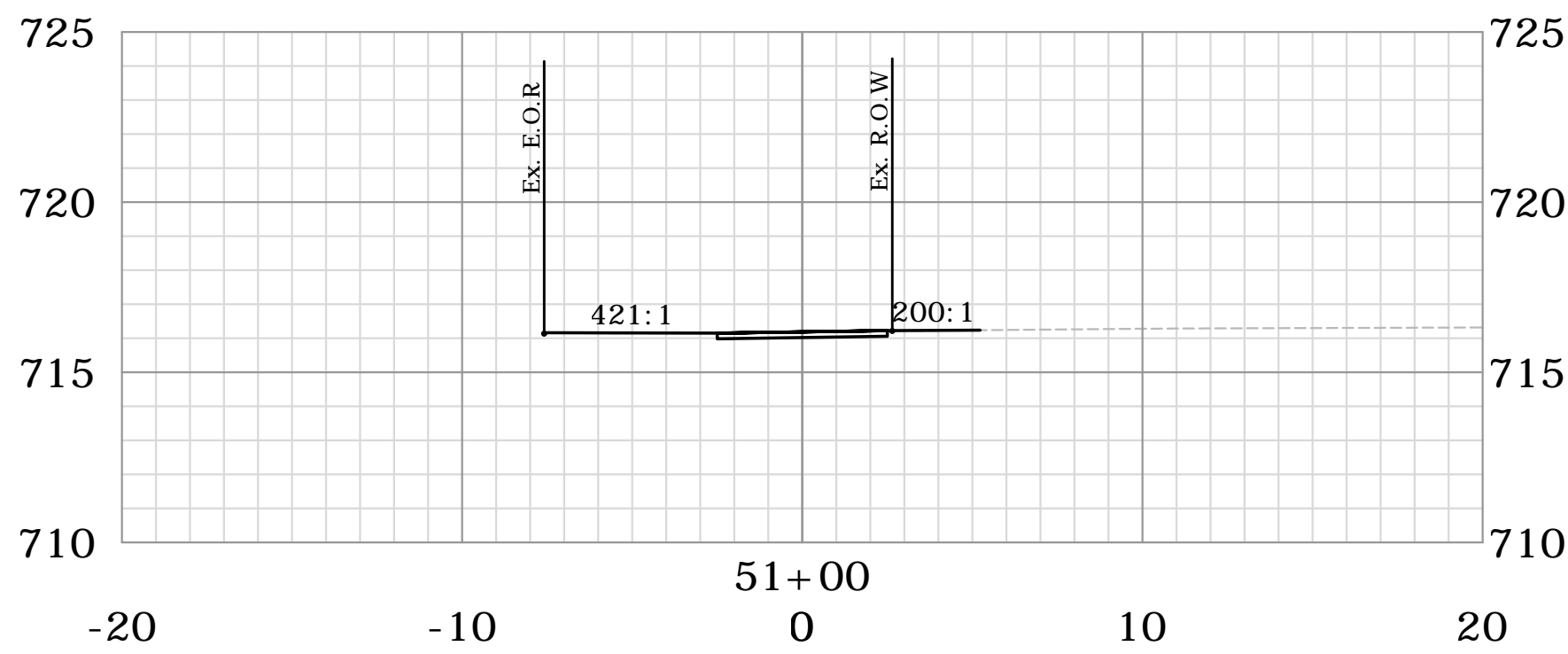
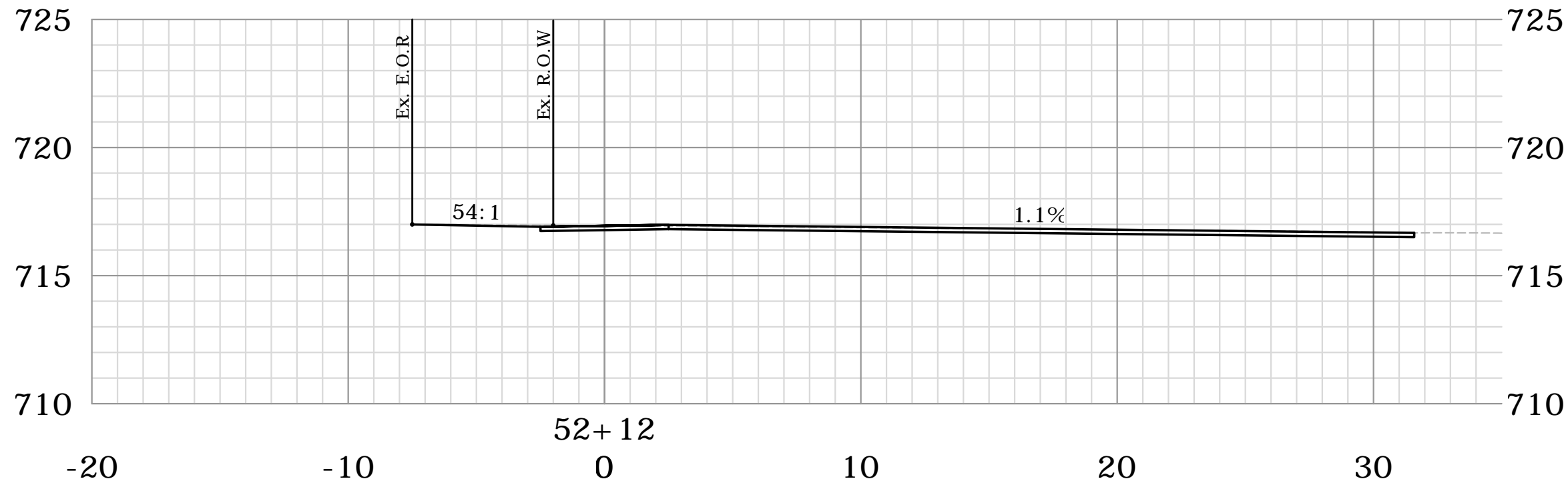
DESCRIPTION	DATE	BY

SIDEWALK CROSS SECTIONS
CT S.R. 44 (MAIN ST.) SIDEWALK IMPROVEMENTS
CT S.R. 44 (MAIN ST.)
SALISBURY, CONNECTICUT

MSM DESIGNED	MSM DRAWN	AAC CHECKED
SCALE 1"=5'		
DATE MARCH 25, 2022		
PROJECT NO. 13039.00006		
DRAWING NO. XSC-04		

07

SLR CONSULTING, INC. 600 WEST 10TH AVENUE, SUITE 200
DENVER, COLORADO 80202-3177
TEL: 303.733.8800 FAX: 303.733.8801
WWW.SLRCONSULTING.COM

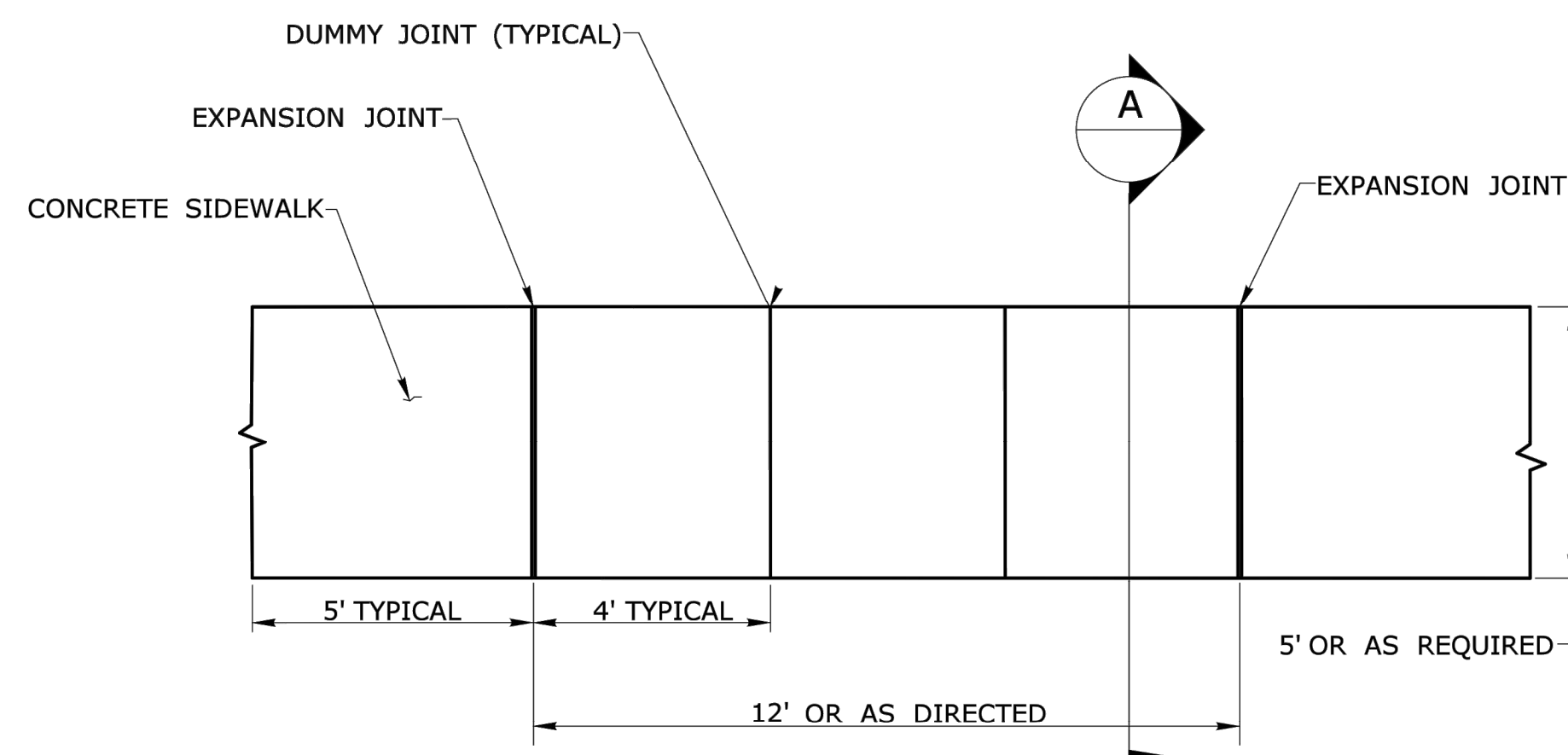


DESCRIPTION	DATE	BY

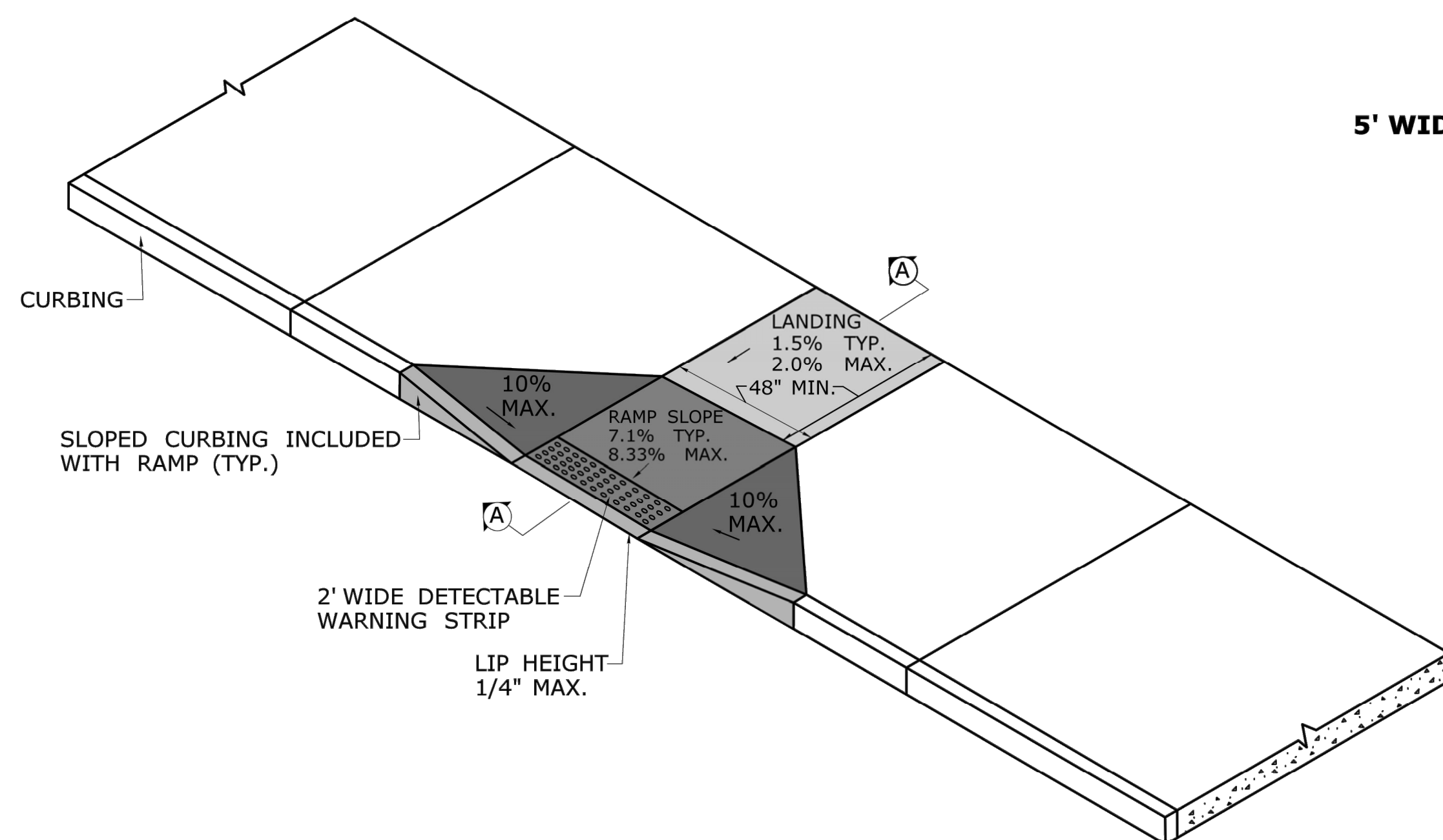
SIDEWALK CROSS SECTIONS
CT S.R. 44 (MAIN ST.) SIDEWALK IMPROVEMENTS
CT S.R. 44 (MAIN ST.)
SALISBURY, CONNECTICUT

MSM DESIGNED	MSM DRAWN	AAC CHECKED
SCALE 1"=5'		
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DRAWING NO. XSC-05		

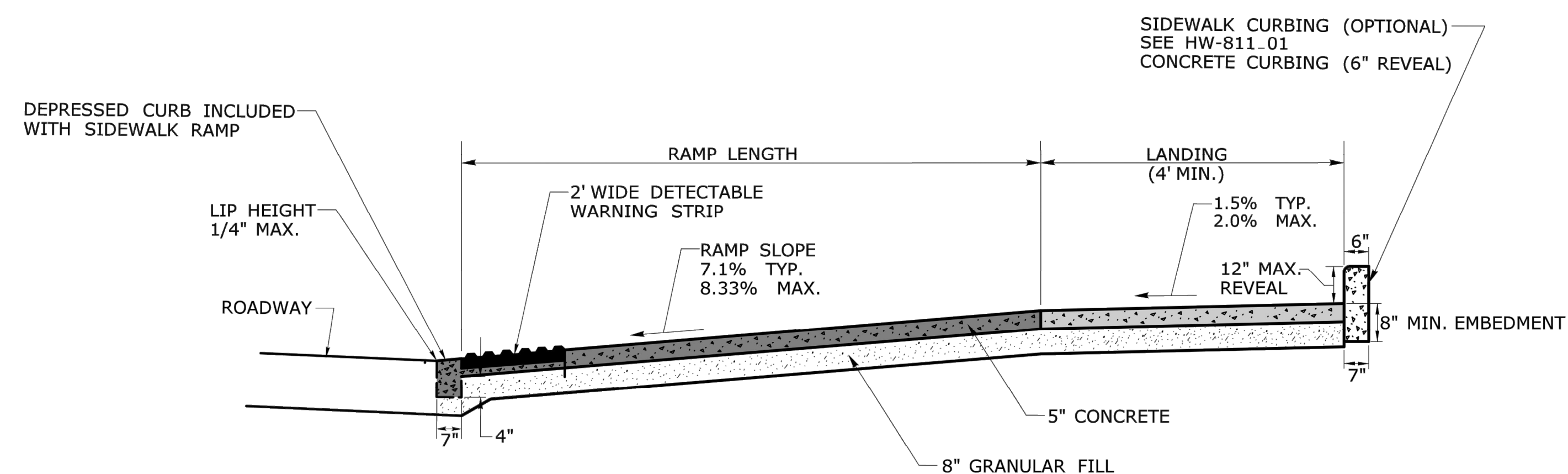
20



5' WIDE SIDEWALK PLAN



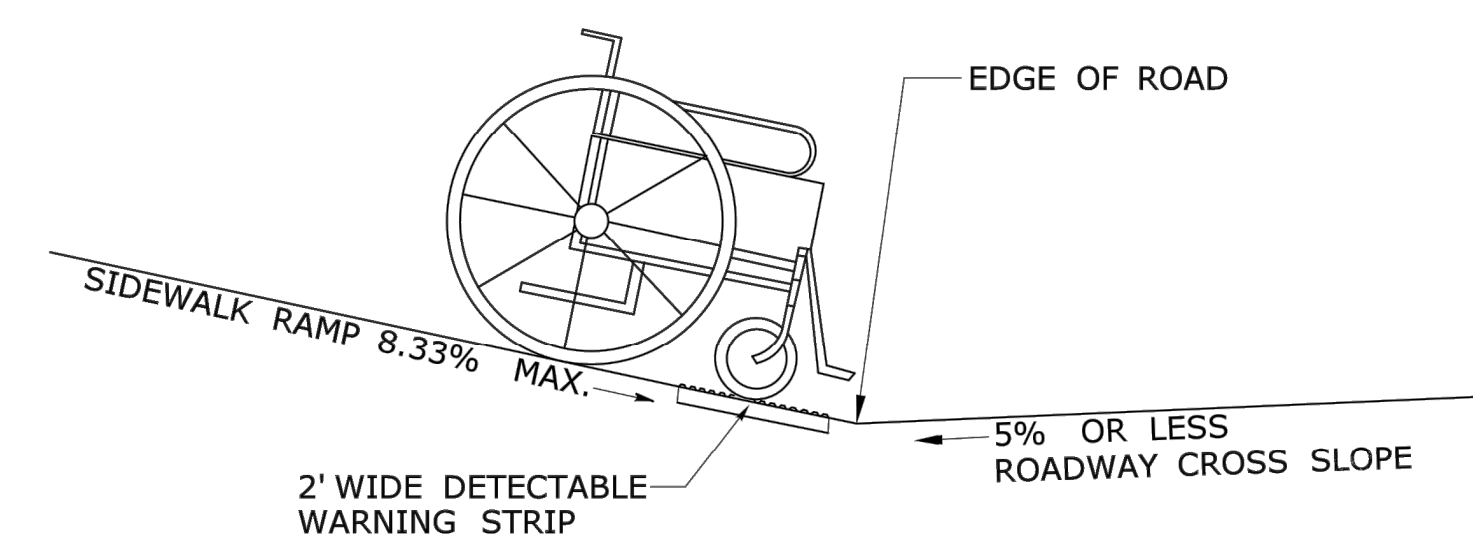
PERPENDICULAR SIDEWALK RAMP



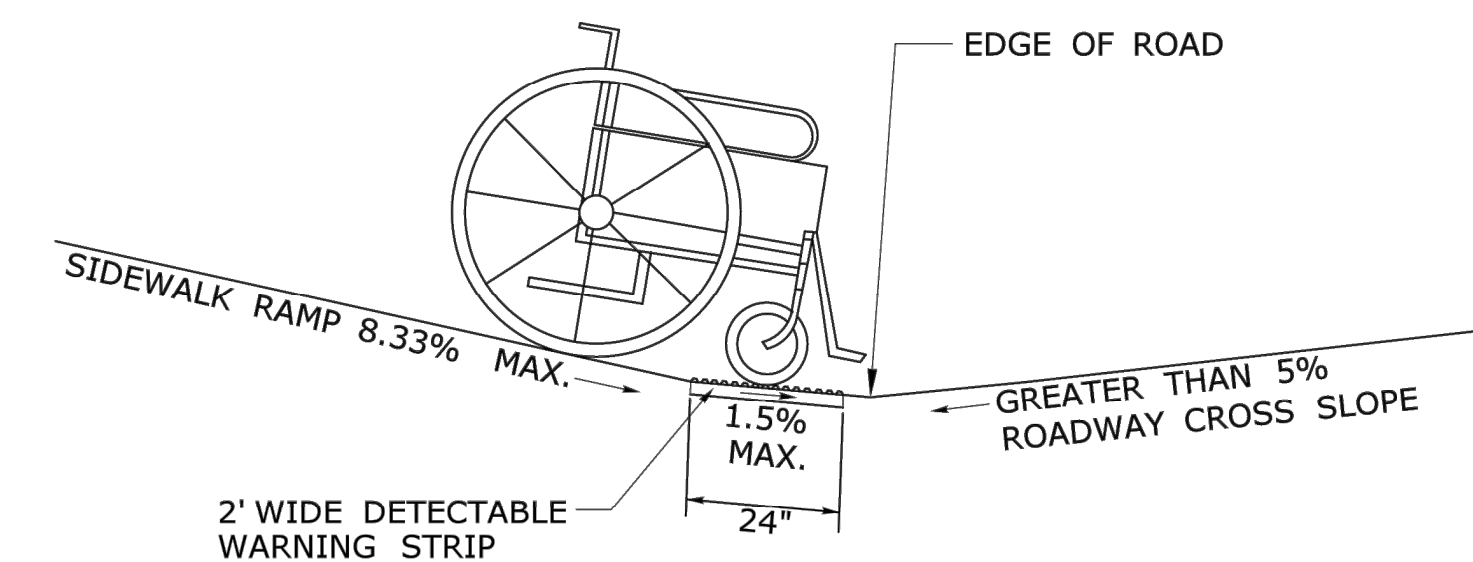
SECTION AA

- GENERAL NOTES:**

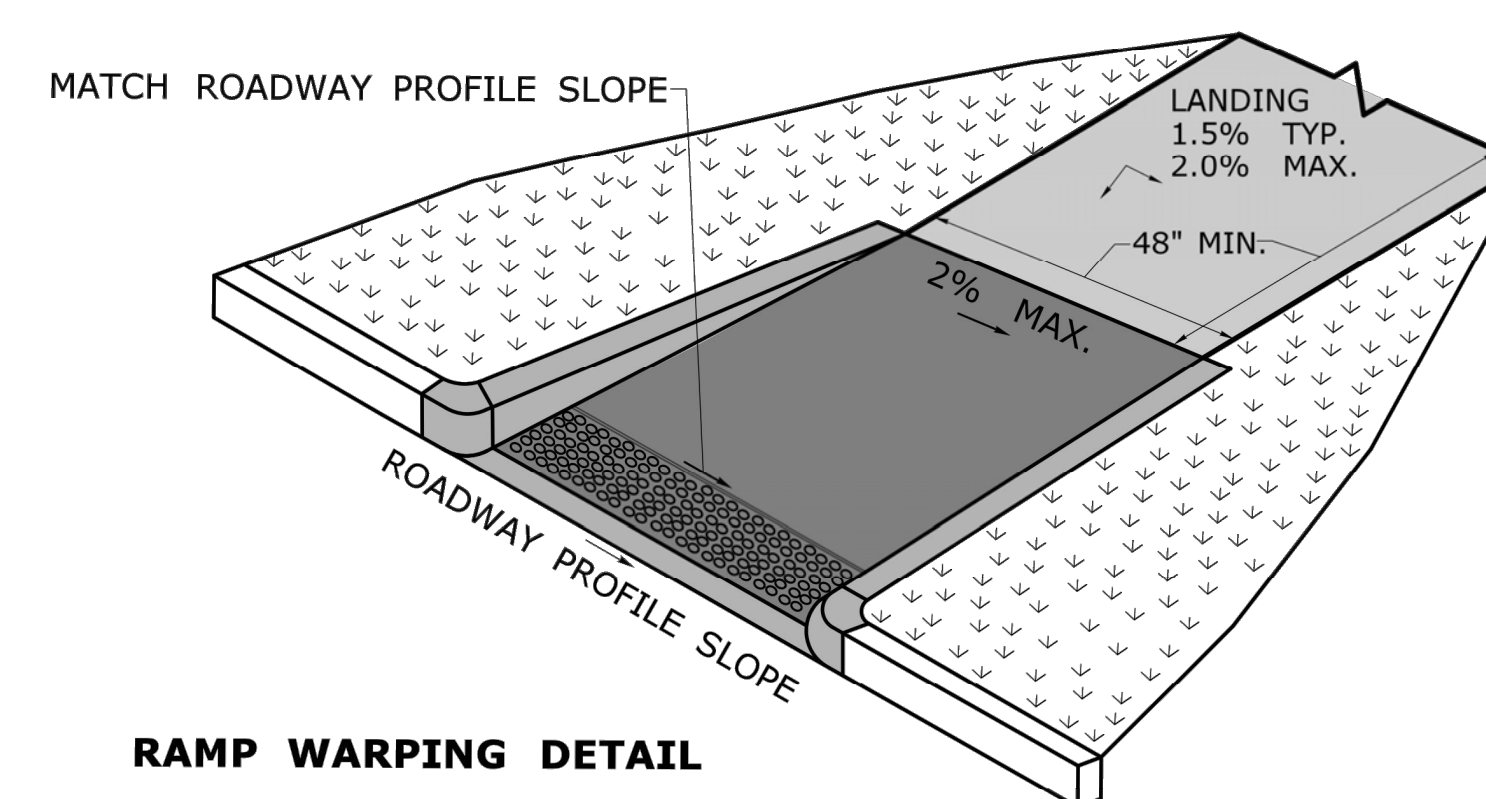
1. SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRAVERSE TO THE SL
2. VERTICAL SURFACE DISCONTINUITIES AT JOINTS SHALL NOT EXCEED $\frac{1}{4}$ INCH
3. REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO EXPANSION OR CONTRACTION JOINT.
4. THE RUNNING SLOPE OF THE CURB RAMP SHALL BE 8.3 PERCENT MAXIMUM BUT SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET.



**SIDEWALK RAMP GRADE AT
ROADWAY CROSS SLOPE OF 5% OR LESS**



**SIDEWALK RAMP GRADE AT
ROADWAY CROSS SLOPE OF GREATER THAN 5%**



RAMP WARPING DETAIL

1. TRANSITION SIDEWALK RAMP TO MATCH ROADWAY PROFILE AS GRADUALLY AS POSSIBLE. DO NOT EXCEED 3 % PER FOOT CROSS SLOPE RATE OF CHANGE WHEN TRANSITIONING TO ROADWAY PROFILE.
2. COMPLETE TRANSITION TO ROADWAY PROFILE BEHIND DETECTABLE WARNING SURFACE.

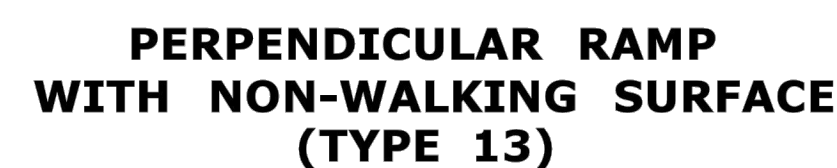
[illegible]

MISCELLANEOUS DETAILS

CT S.R. 44 (MAIN ST.) SIDEWALK IMPROVEMENTS

CT S.R. 44 (MAIN ST.)
SALISBURY, CONNECTICUT

MSM	MSM	AA
DESIGNED	DRAWN	CHECKED
NTS		
SCALE		
MARCH 25, 2022		
DATE		
13039.00006		
PROJECT NO.		
MDS-01		
DWG NO.		



-
- The diagram illustrates a cross-section of a trench repair. The trench is defined by a width labeled "TRENCH WIDTH (TW)". The trench is filled with "PROCESSED AGGREGATE BASE MATERIAL COMPACTED IN 4\" LIFTS". Above the aggregate base is a layer of "EXISTING BITUMINOUS CONCRETE PAVEMENT". The trench width is specified as "TRENCH WIDTH GREATER THAN 12\"", with 12" dimensions shown on either side of the aggregate base. The depth of the trench is indicated as "20\" (30\" IN ROCK CUT)". A note on the left states: "MATCH EXISTING PAVEMENT—THICKNESS USING HMA S0.5 TRAFFIC LEVEL 2 IN MULTIPLE 2\" - 3\" LIFTS FINAL LIFT SHALL BE 2\" MINIMUM COMPACTED PAVEMENT THICKNESS SHALL BE 6\"". A note on the right states: "SAW CUT EDGE, TACK COAT, AND SEAL JOINT AFTER PAVING (TYP.)".

PERMANENT PAVEMENT FOR TRENCH THROUGH BITUMINOUS CONCRETE



DEEP ROOT BARRIER
NOT TO SCALE




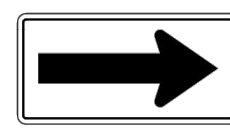

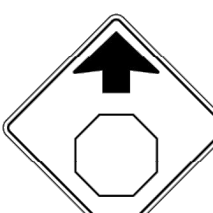









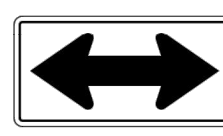

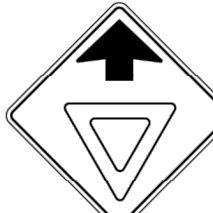
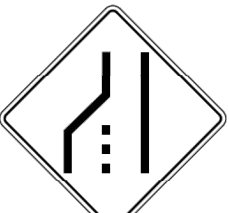
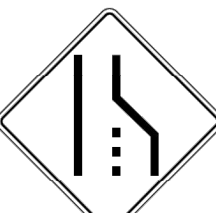


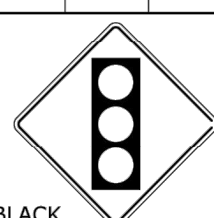
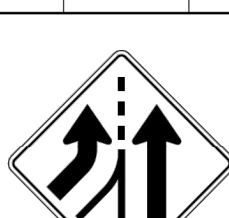
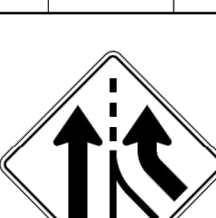
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MISCELLANEOUS DETAILS

CT S.R. 44 (MAIN ST.) SIDEWALK IMPROVEMENTS

CT S.R. 44 (MAIN ST.)
SALISBURY, CONNECTICUT



MSM DESIGNED	MSM DRAWN	AA CHECKED
NTS		
SCALE		
MARCH 25, 2022		
DATE		
13039.00006		
PROJECT NO.		
MDS-02		
DWG NO.		

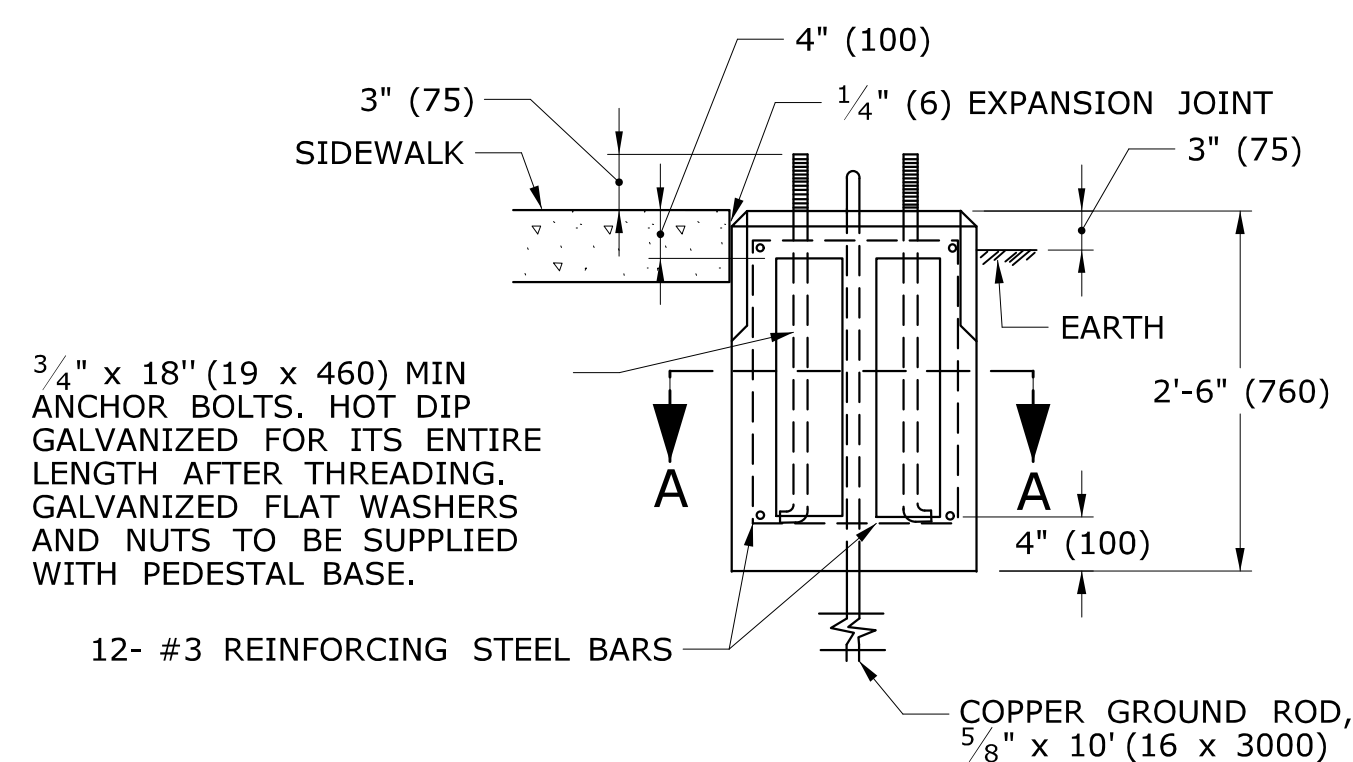
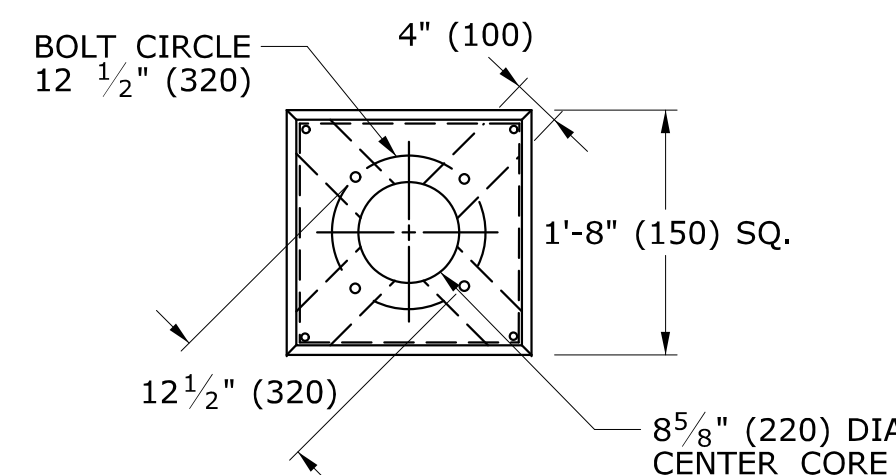
S - SERIES						W1 - SERIES										W2 - SERIES					W3 - SERIES					W4 - SERIES					W5 - SERIES					W6 - SERIES					W7 - SERIES					W8 - SERIES																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																						
<div>S1-1</div> <div></div> <div>LEGEND - BLACK BACKGROUND - FLUORESCENT YELLOW GREEN</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr><tr><td>6.75</td><td>36</td><td>41-2112</td><td>1</td><td>.080</td></tr><tr><td>12.00</td><td>48</td><td>41-2113</td><td>2</td><td>.100</td></tr></table>						AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	6.75	36	41-2112	1	.080	12.00	48	41-2113	2	.100	<div>W1-1L</div> <div></div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr><tr><td>6.25</td><td>30</td><td>41-4006</td><td>1</td><td>.080</td></tr><tr><td>9.00</td><td>36</td><td>41-4031</td><td>1</td><td>.080</td></tr></table>					AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	6.25	30	41-4006	1	.080	9.00	36	41-4031	1	.080	<div>W1-1R</div> <div></div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr><tr><td>6.25</td><td>30</td><td>41-4005</td><td>1</td><td>.080</td></tr><tr><td>9.00</td><td>36</td><td>41-4160</td><td>1</td><td>.080</td></tr></table>					AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	6.25	30	41-4005	1	.080	9.00	36	41-4160	1	.080	<div>W1-6</div> <div></div> <div>LEGEND - BLACK BACKGROUND - FLUORESCENT YELLOW (LEFT OR RIGHT)</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr><tr><td>8.00</td><td>48X24</td><td>41-4223</td><td>2</td><td>.100</td></tr><tr><td>12.50</td><td>60X30</td><td>41-4262</td><td>2</td><td>.100</td></tr></table>					AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	8.00	48X24	41-4223	2	.100	12.50	60X30	41-4262	2	.100	<div>W2-1</div> <div></div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr><tr><td>6.25</td><td>30</td><td>41-4226</td><td>1</td><td>.080</td></tr><tr><td>9.00</td><td>36</td><td>41-4236</td><td>1</td><td>.080</td></tr></table>					AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	6.25	30	41-4226	1	.080	9.00	36	41-4236	1	.080	<div>W3-1</div> <div></div> <div>OCTAGON - RED</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr><tr><td>6.25</td><td>30</td><td>41-0501</td><td>1</td><td>.080</td></tr><tr><td>16.00</td><td>48</td><td>41-0503</td><td>2</td><td>.100</td></tr></table>					AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	6.25	30	41-0501	1	.080	16.00	48	41-0503	2	.100	<div>W4-1</div> <div></div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr><tr><td>9.00</td><td>36</td><td>41-4448</td><td>1</td><td>.080</td></tr><tr><td>16.00</td><td>48</td><td>41-4452</td><td>2</td><td>.100</td></tr></table>					AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	9.00	36	41-4448	1	.080	16.00	48	41-4452	2	.100	<div>W4-1</div> <div></div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr><tr><td>9.00</td><td>36</td><td>41-4449</td><td>1</td><td>.080</td></tr><tr><td>16.00</td><td>48</td><td>41-4459</td><td>2</td><td>.100</td></tr></table>					AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	9.00	36	41-4449	1	.080	16.00	48	41-4459	2	.100	<div>W5-1</div> <div></div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr><tr><td>9.00</td><td>36</td><td>41-4402</td><td>1</td><td>.080</td></tr><tr><td>16.00</td><td>48</td><td>41-4404</td><td>2</td><td>.100</td></tr></table>					AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	9.00	36	41-4402	1	.080	16.00	48	41-4404	2	.100	<div>W6-1</div> <div></div> <div></div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr><tr><td>9.00</td><td>36</td><td>41-4335</td><td>1</td><td>.080</td></tr><tr><td>16.00</td><td>48</td><td>41-4330</td><td>2</td><td>.100</td></tr></table>					AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	9.00	36	41-4335	1	.080	16.00	48	41-4330	2	.100	<div>W7-1</div> <div></div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr><tr><td>6.25</td><td>30</td><td>41-4506</td><td>1</td><td>.080</td></tr><tr><td>9.00</td><td>36</td><td>41-4530</td><td>1</td><td>.080</td></tr><tr><td>16.00</td><td>48</td><td>41-4508</td><td>2</td><td>.100</td></tr></table>					AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	6.25	30	41-4506	1	.080	9.00	36	41-4530	1	.080	16.00	48	41-4508	2	.100	<div>W8-5</div> <div></div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr><tr><td>6.25</td><td>30</td><td>41-4519</td><td>1</td><td>.080</td></tr><tr><td>9.00</td><td>36</td><td>41-4520</td><td>1</td><td>.080</td></tr><tr><td>16.00</td><td>48</td><td>41-4521</td><td>2</td><td>.100</td></tr></table>					AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	6.25	30	41-4519	1	.080	9.00	36	41-4520	1	.080	16.00	48	41-4521	2	.100																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																										
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											<div>W1-8</div> <div></div> <div>LEGEND - BLACK BACKGROUND - FLUORESCENT YELLOW (LEFT OR RIGHT)</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr><tr><td>3.00</td><td>18X24</td><td>41-3951</td><td>1</td><td>.080</td></tr><tr><td>7.50</td><td>30X36</td><td>41-4211</td><td>1</td><td>.080</td></tr><tr><td>12.00</td><td>36X48</td><td>41-4260</td><td>2</td><td>.080</td></tr></table>					AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	3.00	18X24	41-3951	1	.080	7.50	30X36	41-4211	1	.080	12.00	36X48	41-4260	2	.080											<div>W3-3</div> <div></div> <div>SYMBOL & LEGEND - BLACK BACKGROUND - YELLOW TOP CIRCLE - RED BOTTOM CIRCLE - GREEN</div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr><tr><td>6.25</td><td>30</td><td>41-0835</td><td>1</td><td>.080</td></tr><tr><td>16.00</td><td>48</td><td>41-0848</td><td>2</td><td>.100</td></tr></table>					AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	6.25	30	41-0835	1	.080	16.00	48	41-0848	2	.100	<div>W4-3</div> <div></div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr><tr><td>9.00</td><td>36</td><td>41-4468</td><td>1</td><td>.080</td></tr><tr><td>16.00</td><td>48</td><td>41-4435</td><td>2</td><td>.100</td></tr></table>					AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	9.00	36	41-4468	1	.080	16.00	48	41-4435	2	.100	<div>W4-3</div> <div></div> <table><tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th><th>ALUM. THK.</th></tr><tr><td>9.00</td><td>36</td><td>41-4470</td><td>1</td><td>.080</td></tr><tr><td>16.00</td><td>48</td><td>41-4434</td><td>2</td><td>.100</td></tr></table>					AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	ALUM. THK.	9.00	36	41-4470	1	.080	16.00	48	41-4434	2	.100																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																											
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ONLY STANDARD SHEETS MARKED WITH AN "✓" ARE IN THIS PROJECT #

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STANDARD SHEETS SHALL BE USED WITH STANDARD SPECIFICATIONS

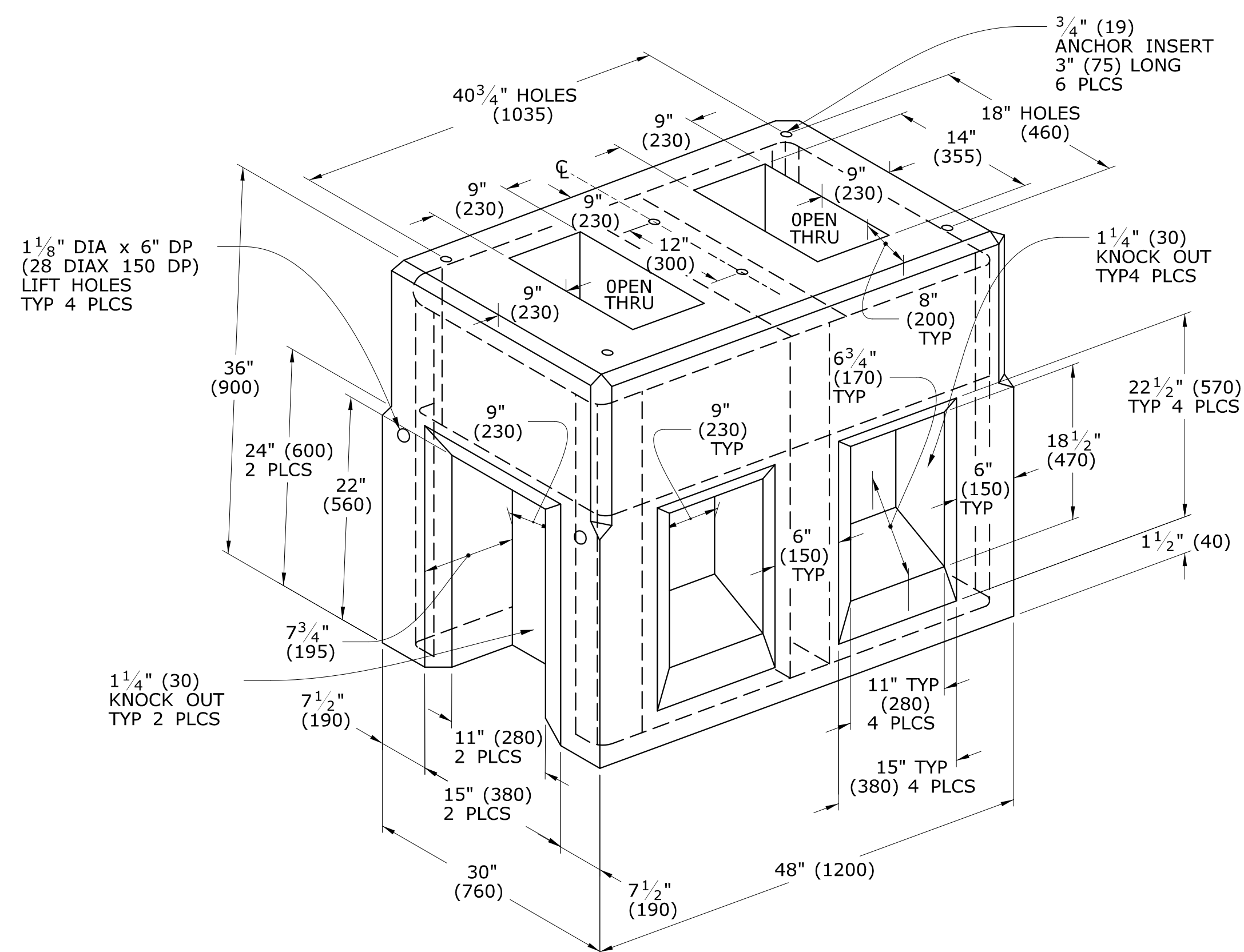
				THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		<div><div><div>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</div></div></div>		SUBMITTED BY: _____NAME/DATE/TIME: _____		CTDOT STANDARD SHEET OFFICE OF ENGINEERING		STANDARD SHEET TITLE:		STANDARD SHEET NO.:	
				NOT TO SCALE		Filename: CTDOT_TRAFFIC_STD_DGN.DGN Model: TR-01-STD_INDEX		TRAFFIC STANDARD SHEET INDEX				TR-STD_INDEX			
4	2-2017	REMOVED TR-1210_01 TO TR-1210_03. ADDED TR-1210_04 TO TR-1210_09													
3	4-2014	REMOVED TR-1111_02.													
2	1-2014	REMOVED TR-1103_01.													
1	4-2012	RENUMBERED TR-1107_02 TO TR-1114_01. REMOVED TR-1116_01.													
REV. DATE		REVISION DESCRIPTION				Plotted Date: 8/16/2018									



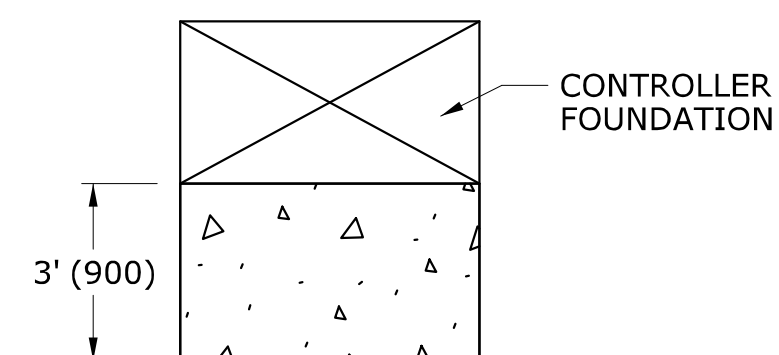
TRAFFIC CONTROL FOUNDATION
PEDESTAL - TYPE I - PRECAST

NOTES:

PLACE NO. 6 CRUSHED STONE IN CENTER OPENING AFTER
CONDUITS AND GROUND ROD HAVE BEEN INSTALLED.



TRAFFIC CONTROL FOUNDATION
CONTROLLER - TYPE IV - PRECAST

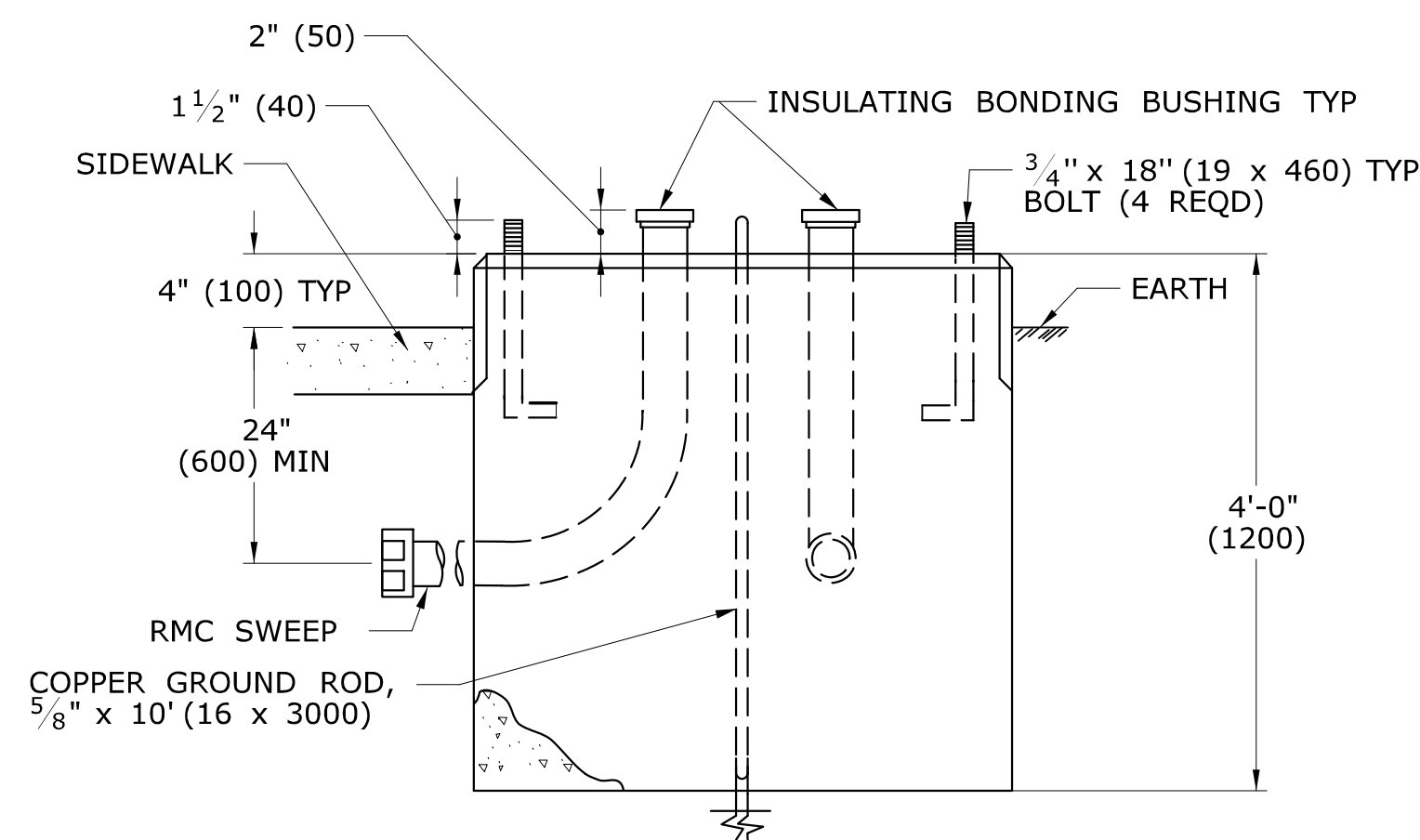
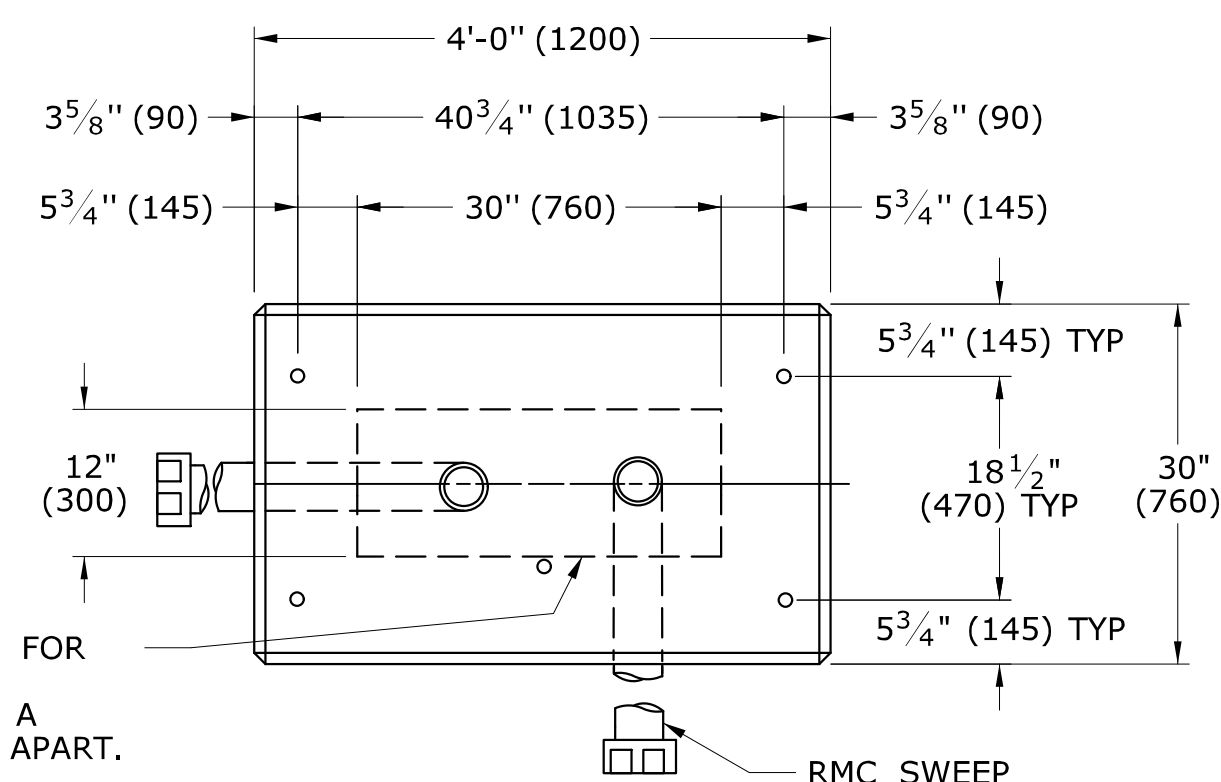


INSTALL PRECAST OR CAST IN PLACE CONCRETE SIDEWALK ON CABINET
DOOR SIDE OF CONTROLLER FOUNDATION.

PITCH SIDEWALK 1/4" PER FOOT (20 PER METER) AWAY FROM THE
CONTROLLER FOUNDATION.

REFER TO HIGHWAY STANDARD SHEET HW-921_01 FOR SIDEWALK CONSTRUCTION.

TYPICAL CONCRETE SIDEWALK AT CONTROLLER FOUNDATION











TRAFFIC CONTROL FOUNDATION
CONTROLLER - TYPE IV - CAST IN PLACE

NOTES:

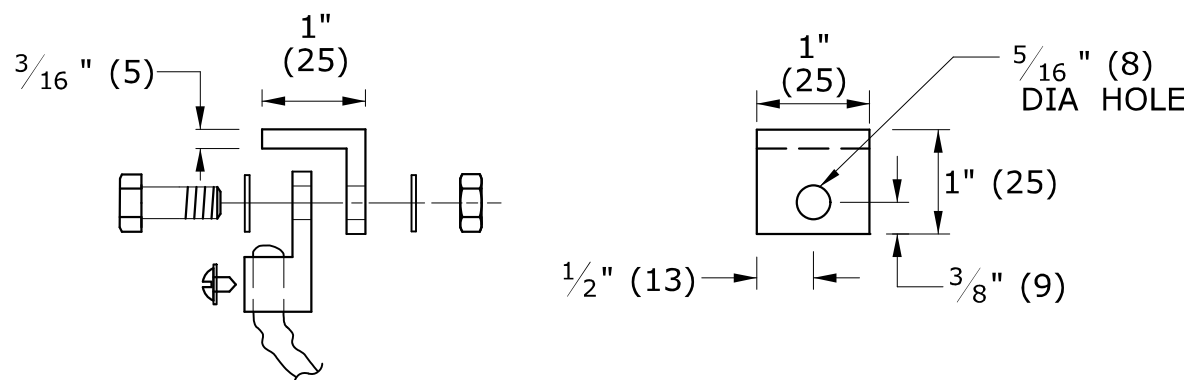
INSTALL FOUNDATION OF 6" (150) OF COMPACTED GRAVEL IN ACCORDANCE WITH SECTION 2.14.
LEVEL FOUNDATION WITH A PROJECTION OF 4" (100) ABOVE FINISHED GRADE.
INSTALL COPPER GROUND ROD: $5/8"$ x 10 (16 x 3000).
PLACE NO. 6 CRUSHED STONE IN THE CENTER OPENINGS AFTER THE CONDUITS AND GROUND ROD
HAVE BEEN INSTALLED. THE OPENINGS SHALL BE CAPPED WITH A 2" (50) GROUT LEVEL WITH THE
TOP OF THE FOUNDATION AND NEATLY FINISHED. THE GROUT SHALL CONFORM WITH THE
REQUIREMENTS OF ARTICLE M.3.01-12.
CONCRETE: CLASS "A" CONFORMING TO ARTICLE M.03.01.
#4 REBAR 2" (50) MIN COVER AROUND ALL OPENINGS, 3-#4 REBARS IN EACH CORNER.
CONDUITS SHALL NOT PROJECT MORE THAN 2" (50) ABOVE FOUNDATION.

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:

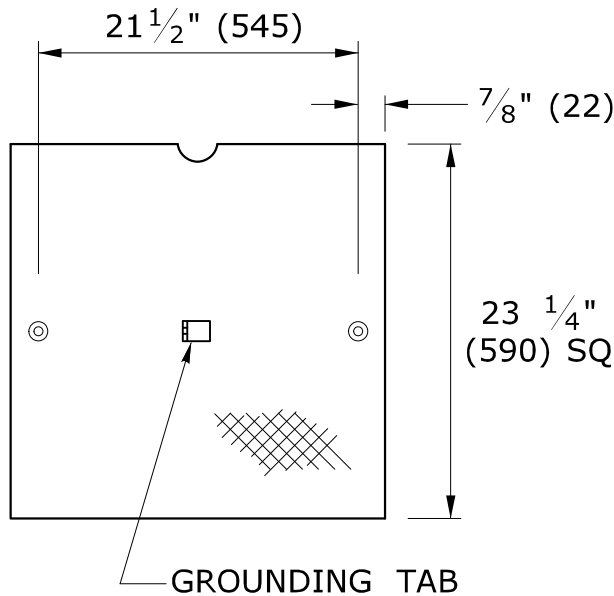
	PROPOSED CONTROLLER
	EXISTING CONTROLLER
	PROPOSED STEEL SPAN POLE
	EXISTING STEEL SPAN POLE

2	1-2014	REMOVED SPAN POLE FOUNDATION DETAILS, REVISED TYPICAL CONCRETE SIDEWALK AT CONTROLLER FOUNDATION.	THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. NOT TO SCALE	DIMENSIONS ARE IN ENGLISH ("') & METRIC UNITS (mm). METRIC DIMENSIONS ARE ROUNDED: - OVER 1" TO NEAREST 5 mm - UNDER 1" TO NEAREST 1 mm.	 STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION 	SUBMITTED BY: NAME/DATE/TIME:  Tracy L. Fogarty 2014.01.07 16:12:06-05'00'	CTDOT STANDARD SHEET TRAFFIC CONTROL FOUNDATIONS OFFICE OF ENGINEERING	STANDARD SHEET TITLE: TRAFFIC CONTROL FOUNDATIONS	STANDARD SHEET NO.: TR-1002.01
1	4-2012	MINOR REVISIONS.				APPROVED BY: NAME/DATE/TIME:  Charles S. Harlow 2014.01.08 09:02:54-05'00'			
REV.	DATE	REVISION DESCRIPTION				Plotted Date: 1/7/2014			

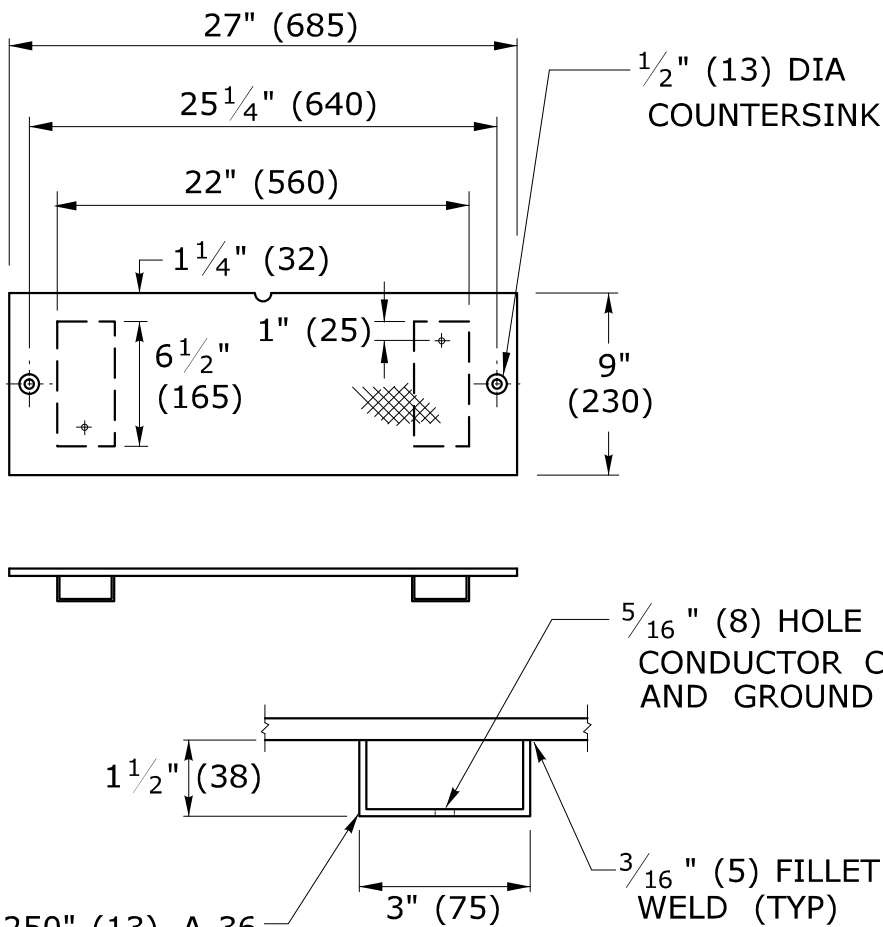
- COVER NOTES:
1. GROUNDING TAB WELDED TO BOTTOM CENTER OF COVER WITH $\frac{3}{16}$ " (5) WELD (3 SIDES).
 2. ATTACH 6' (2 m) LENGTH OF NO. 8 GROUND WIRE TO GROUNDING TAB WITH CONDUCTOR CONNECTOR, $\frac{1}{4}$ " - 20 X $\frac{3}{4}$ " (M6 X 20) LG SST HEX HEAD BOLT, AND SST FLAT WASHER. ATTACH FREE END OF GROUND WIRE TO CONDUIT BONDING BUSHING IN HANDHOLE.
 3. CONDUCTOR CONNECTOR: COPPER ALLOY BODY, BRASS SCREW, BRASS OR COPPER ALLOY PRESSURE PLATE.
 4. COVER SCREW INSERT: $\frac{3}{8}$ "-16 (9-16), $1\frac{1}{2}$ "L (37L), STAINLESS STEEL.
 5. COVER SCREW: $\frac{3}{8}$ "-16 (9-16), 1"L (25L), FLAT HEAD, SLOTTED, STAINLESS STEEL.



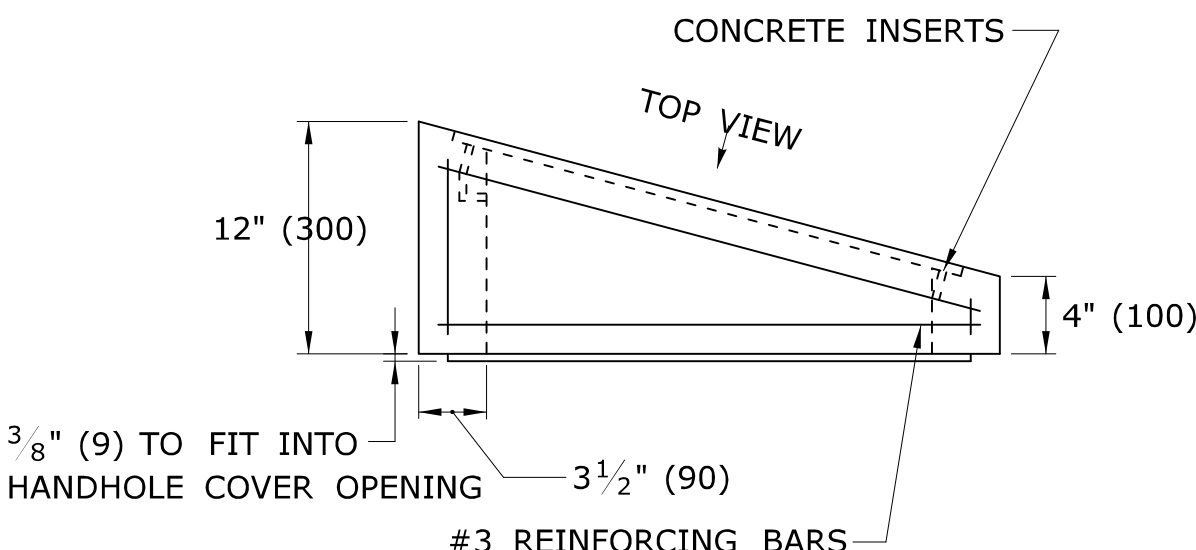
**STEEL GROUNDING TAB
w/ CONDUCTOR CONNECTOR**



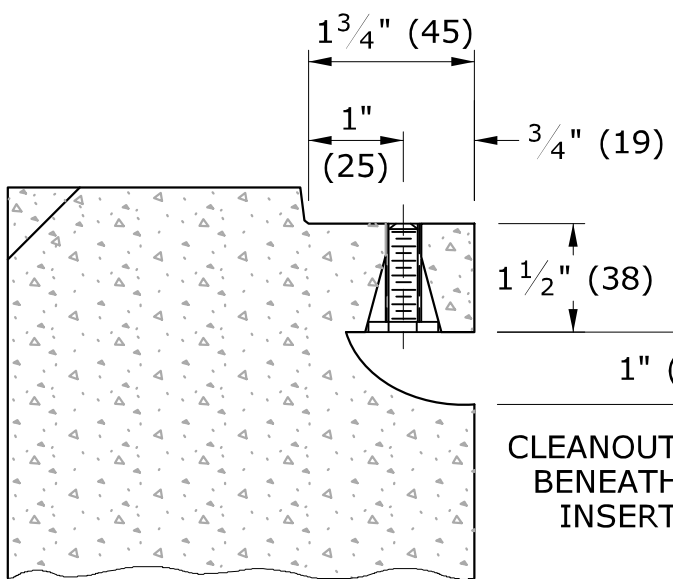
C-CHANNEL 0.250" (13), A-36



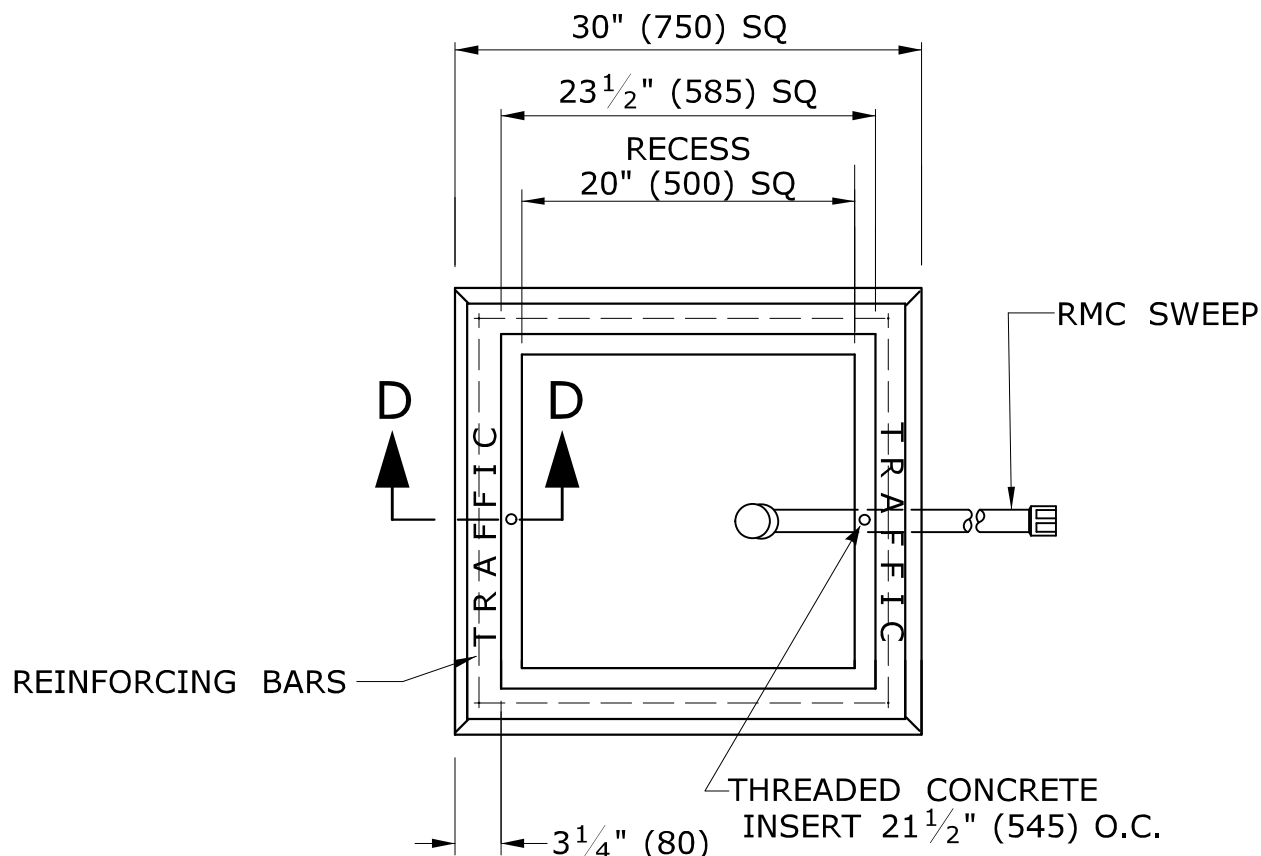
**NON SKID FLOOR PLATE
GALVANIZED STEEL, 3/8" (10)**



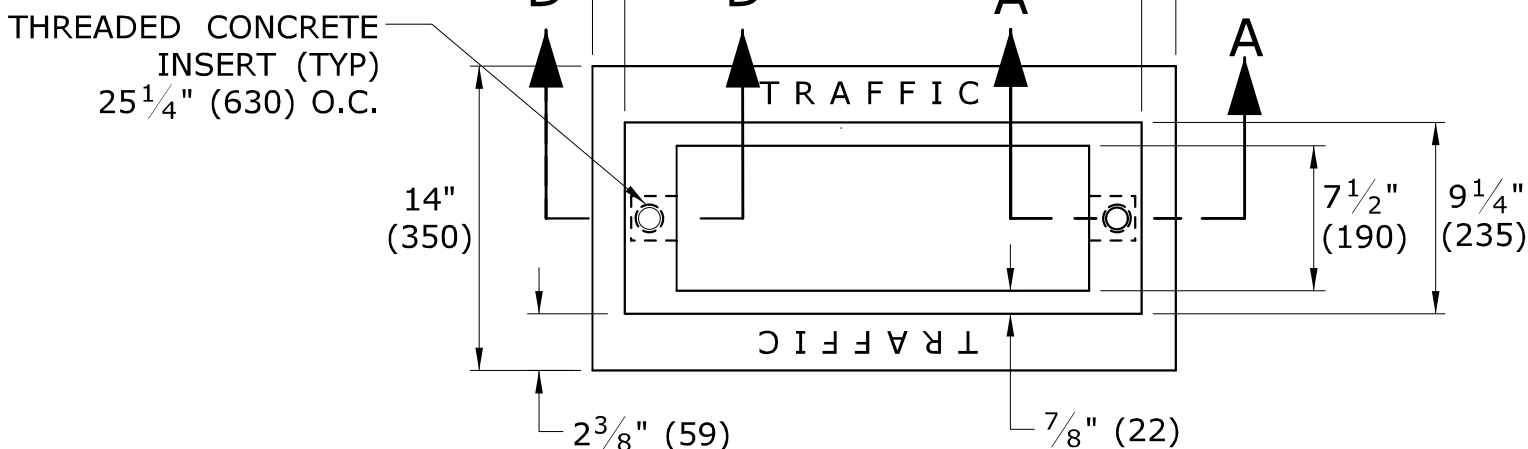
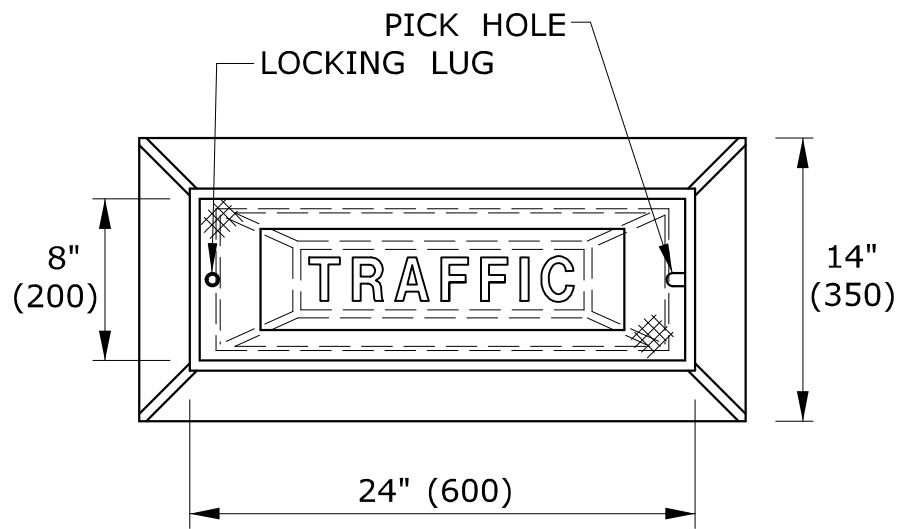
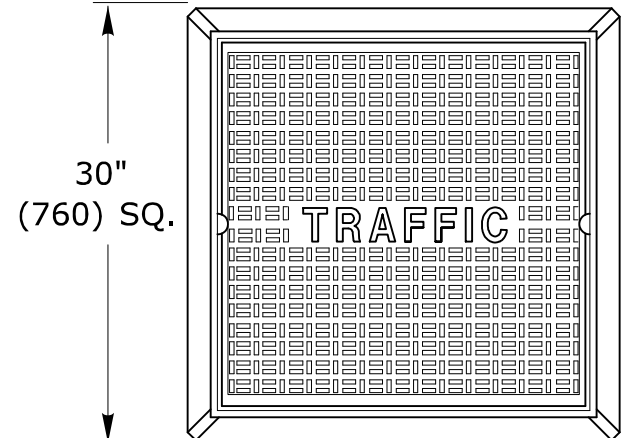
BANK ADAPTER



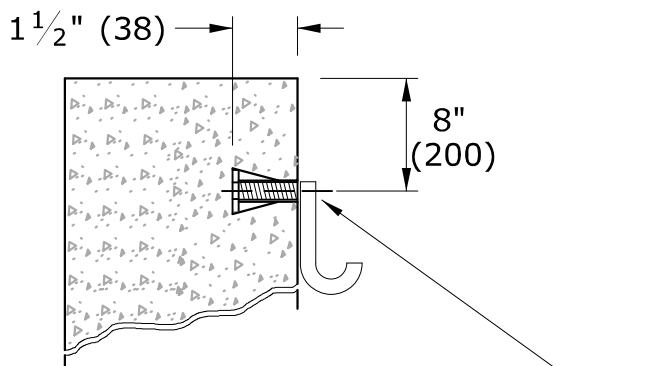
SECTION D-D



PLAN VIEW

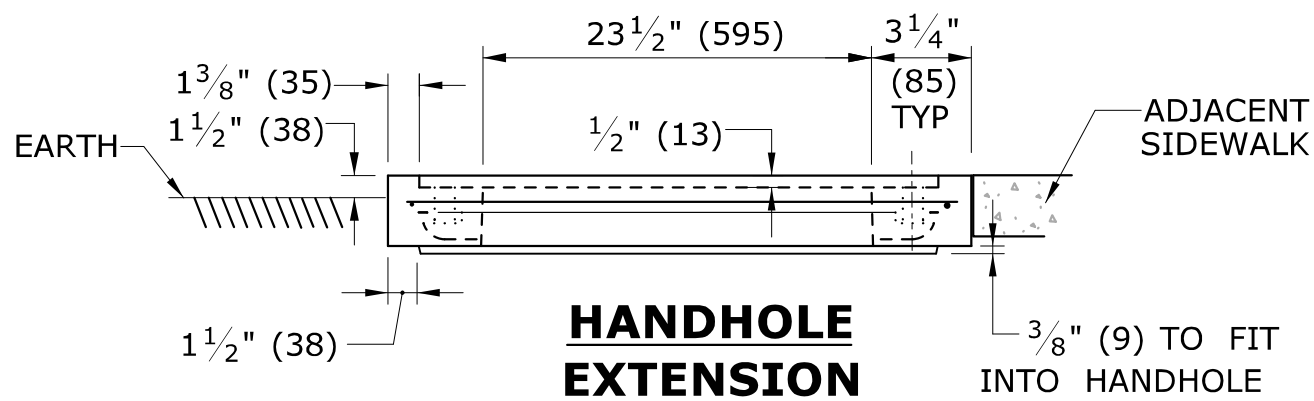


PLAN VIEW

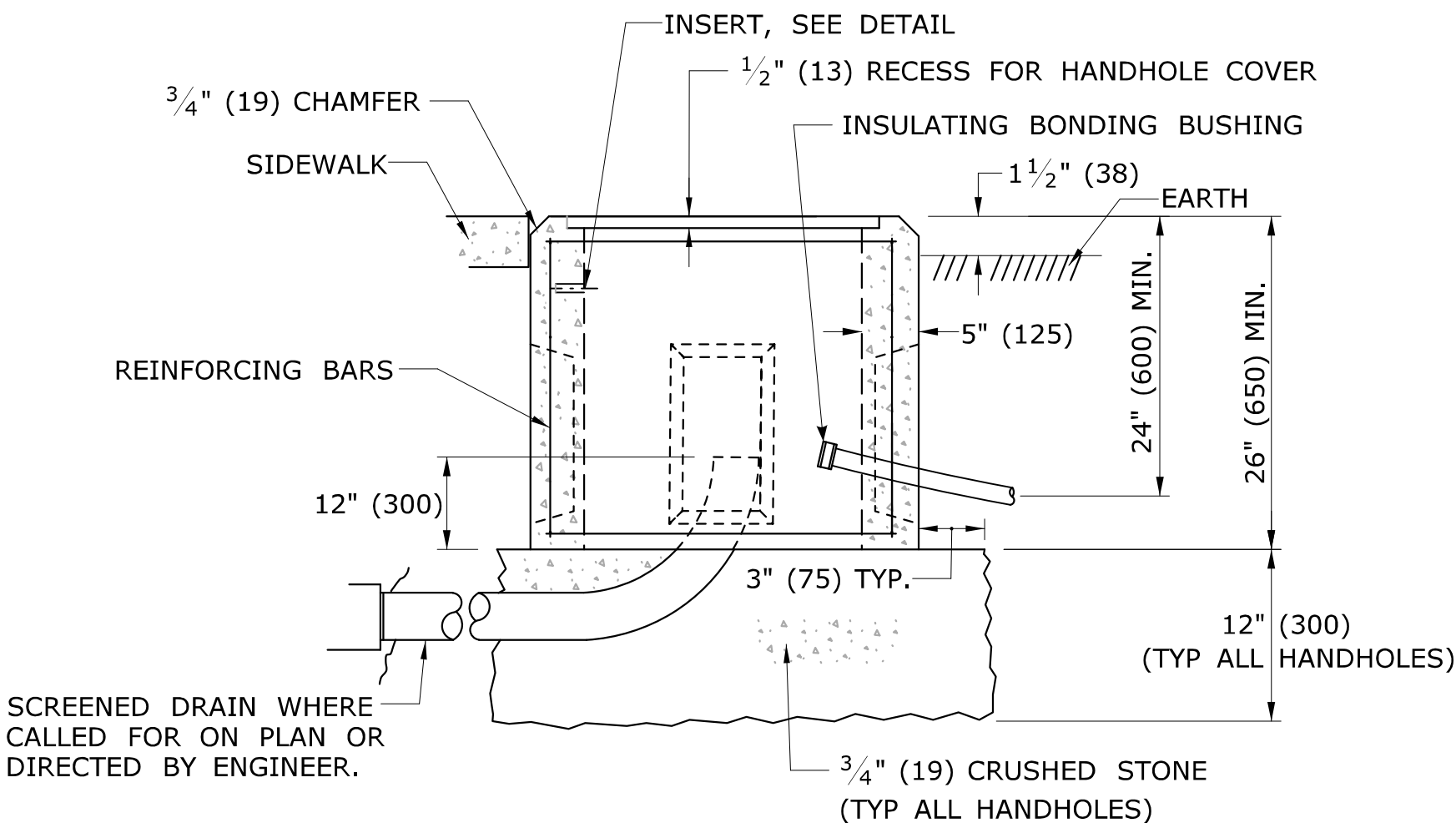
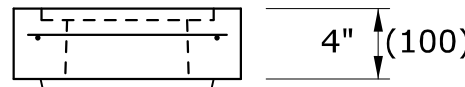


INSERT DETAIL

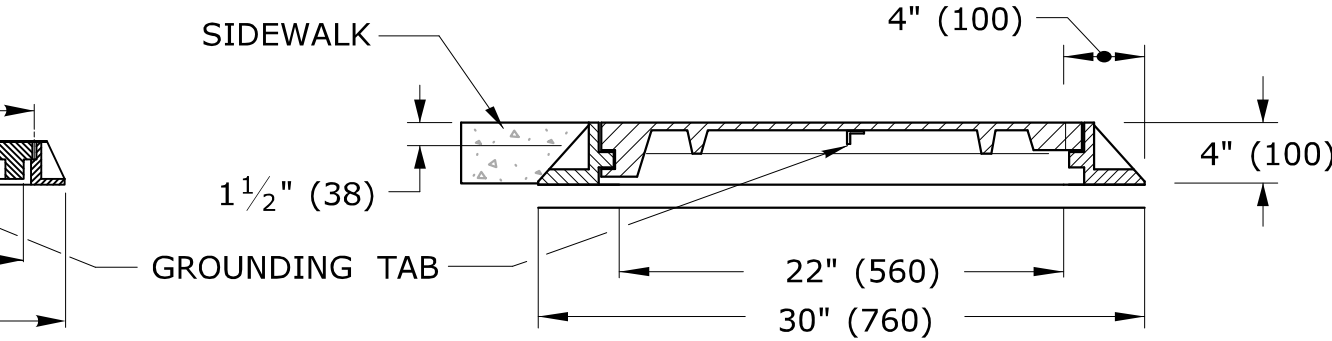
TYP IN TWO PLACES FOR ALL HANDHOLES



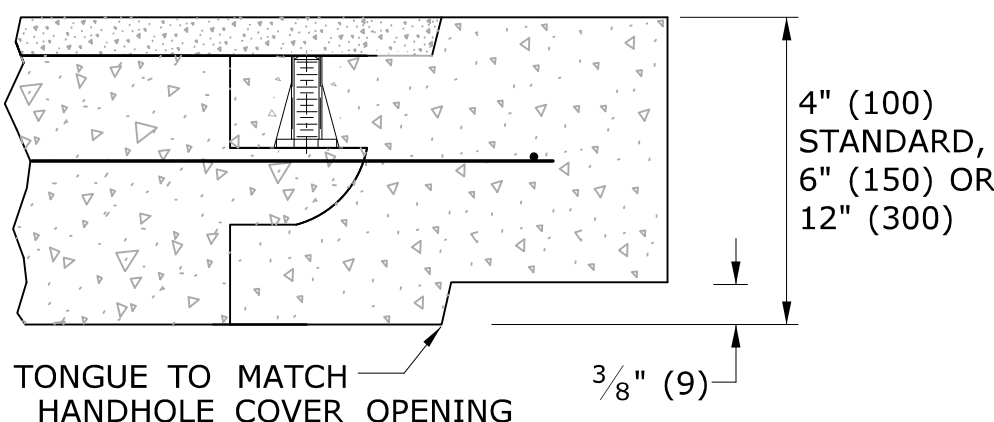
**HANDHOLE
EXTENSION**



CONCRETE HANDHOLE TYPE I

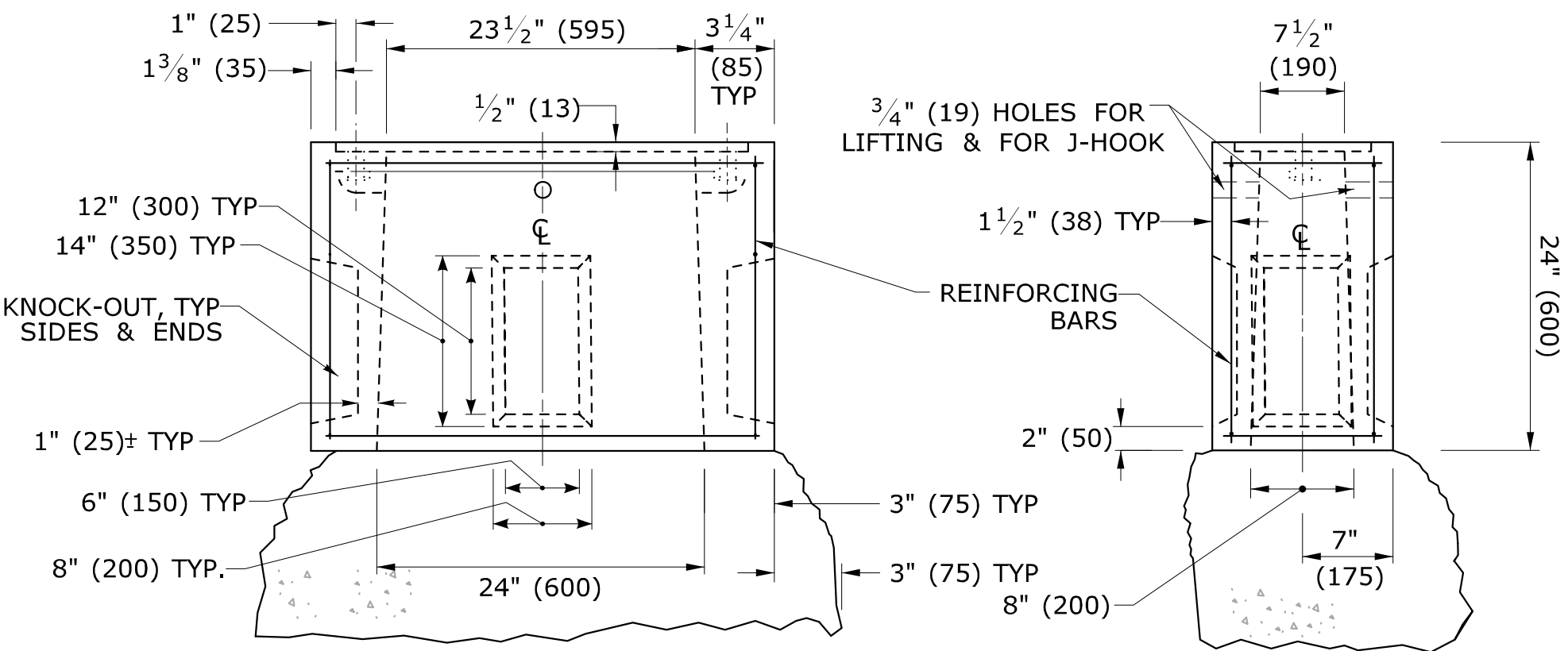


**CAST IRON
HANDHOLE COVERS**



**SECTION A-A
HANDHOLE EXTENSIONS**

4 - #8 REINFORCING BARS REQ'D



**BASE SECTION
CONCRETE HANDHOLE TYPE II**

HANDHOLE NOTES:

1. MINIMUM CLASS "C" CONCRETE.
2. COMPLETE TYPE II HANDHOLE:
IN EARTH AREAS, CONSISTS OF A BASE SECTION WITH 4" (100) HANDHOLE EXTENSION,
IN SIDEWALK AREAS, CONSISTS OF A BASE SECTION WITH 4" (100) CAST IRON COVER.
3. PLAN VIEW DIMENSIONS, SECTION VIEW, & DETAILS, SAME FOR BASE SECTION, EXTENSIONS & BANK ADAPTER.
4. GROUT AROUND ALL CONDUITS.

5. INSTALL 30" (750) SIDE PARALLEL TO ROAD UNLESS OTHERWISE NOTED.
6. INSTALL HANDHOLES APPROX. 12" (300) BEHIND CURB OR
IF NO CURB, 24" (600) BEHIND EDGE OF ROAD UNLESS OTHERWISE SPECIFIED.
7. CAST THE WORD "TRAFFIC" INTO TOP EDGE OF HANDHOLE, 1 1/2" (38) LETTERS.
8. WHERE AN EXISTING CONCRETE SIDEWALK SLAB ABUTTING A HANDHOLE IS DAMAGED OR CUT DURING INSTALLATION, REPLACE THE ENTIRE SIDEWALK SECTION.
9. 12-#3 REINFORCING BARS REQUIRED FOR ALL HANDHOLES. (8 HORIZONTAL, 4 VERTICAL)

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:
PROPOSED HANDHOLE
EXISTING HANDHOLE

REV.	DATE	REVISION DESCRIPTION
2	4-2014	REVISED HANDHOLES NOTES, ADDED NOTE #6. ADDED "J" HOOK TO INSERT DETAIL.
1	4-2012	CAST IRON COVER: CHANGED BOLT TO PICK HOLE. ADDED EXTENSIONS, C-CHANNEL, CONDUCTOR CONNECTOR & MINOR REVISIONS.

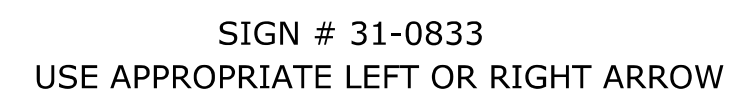
THE INFORMATION, INCLUDING ESTIMATED
QUANTITIES OF WORK, SHOWN ON THESE
SHEETS IS BASED ON LIMITED
INVESTIGATIONS BY THE STATE AND IS
IN NO WAY WARRANTED TO INDICATE
THE CONDITIONS OF ACTUAL QUANTITIES
OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 4/11/2014

DIMENSIONS ARE IN ENGLISH ("') & METRIC UNITS (mm). METRIC DIMENSIONS ARE ROUNDED: - OVER 1" TO NEAREST 5 mm. - UNDER 1" TO NEAREST 1 mm.	 STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION	NOT TO SCALE
Filename: CTDOT_TRAFFIC_STD.DGN	Model: TR-1010_01	

SUBMITTED BY: Tracy L. Fogarty, P.E. 2014.04.25 16:01:09-04'00'	NAME/DATE/TIME:
APPROVED BY: Charles S. Harlow, P.E. 2014.04.29 14:26:25-04'00'	NAME/DATE/TIME:

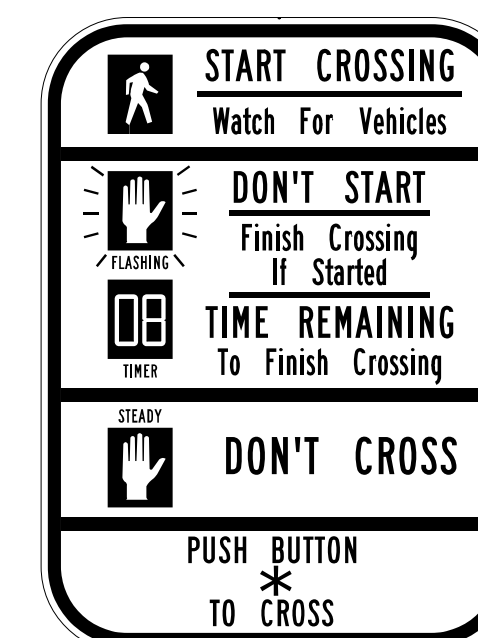
CTDOT STANDARD SHEET	STANDARD SHEET TITLE: CONCRETE HANDHOLE	STANDARD SHEET NO.: TR-1010_01
OFFICE OF ENGINEERING		



FOR CROSSING
WITH SIDE STREET GREEN

GENERAL NOTES:

3'-6" (1050) FROM FINISHED GRADE SUCH AS SIDEWALK TO CENTER OF PUSH BUTTON.
PUSH BUTTON INSTALLATIONS SHALL CONFORM TO THE REQUIREMENTS OF THE AMERICANS
WITH DISABILITIES ACT (ADA) STANDARDS FOR ACCESSIBLE DESIGN, CURRENT EDITION GOVERNS.
4'-4" (1300) PEDESTAL TO INCLUDE ALLOY CAP SECURED WITH STAINLESS STEEL SET SCREW.






*USE APPROPRIATE ARROW UNLESS OTHERWISE NOTED ON PLAN.

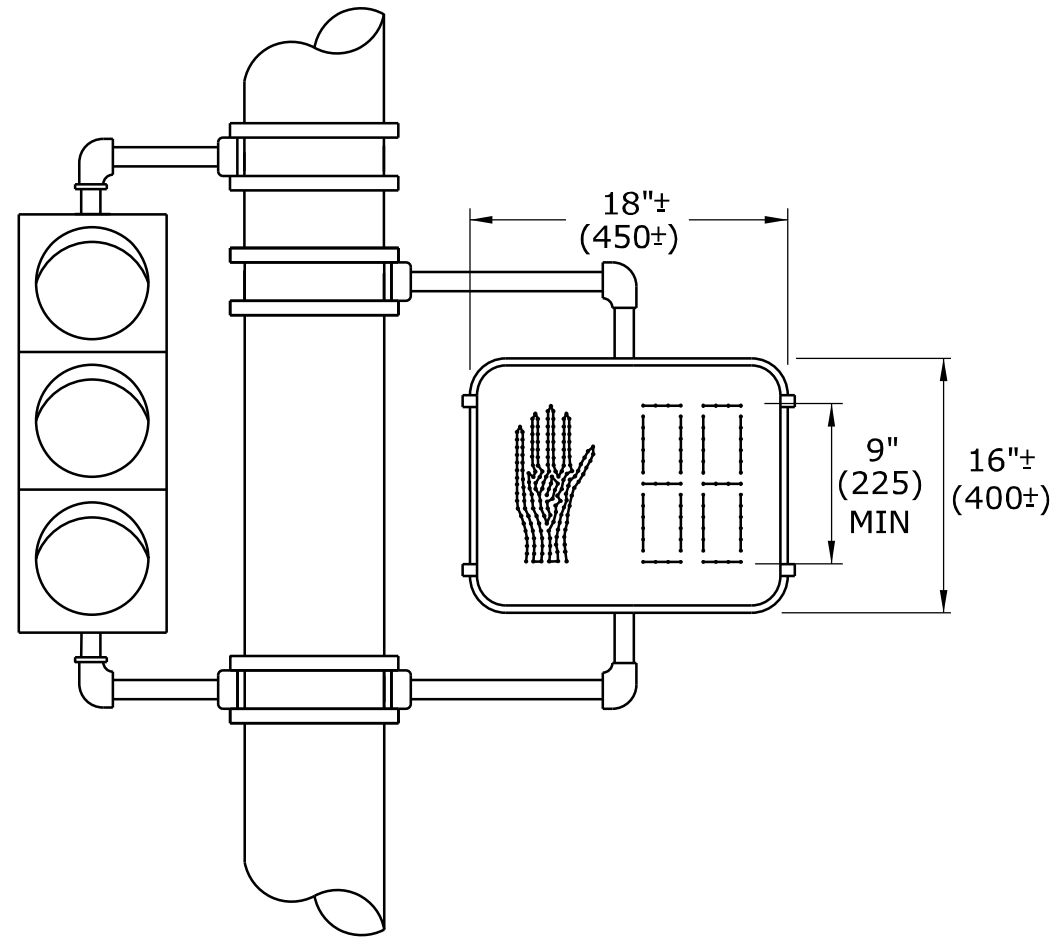
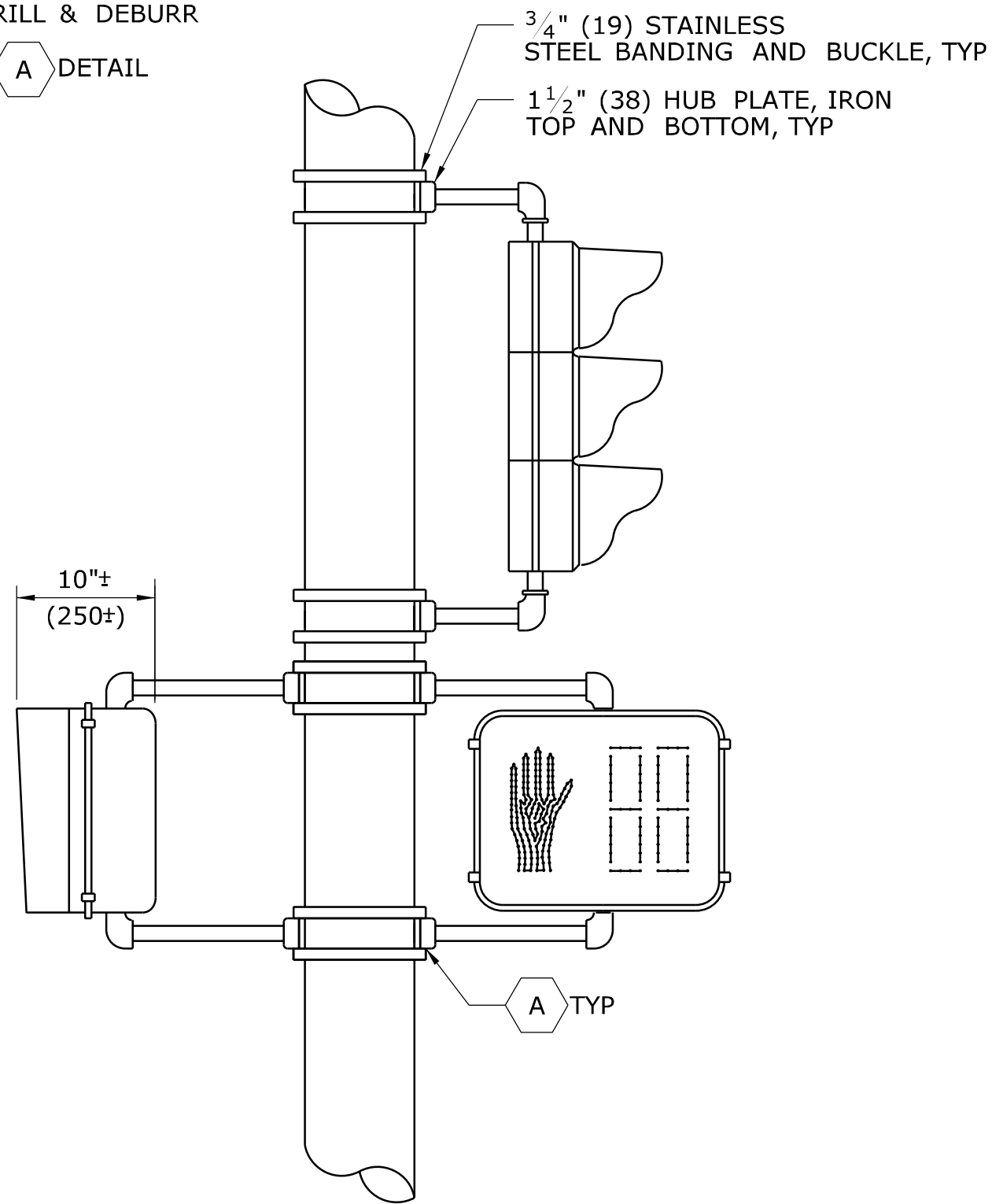
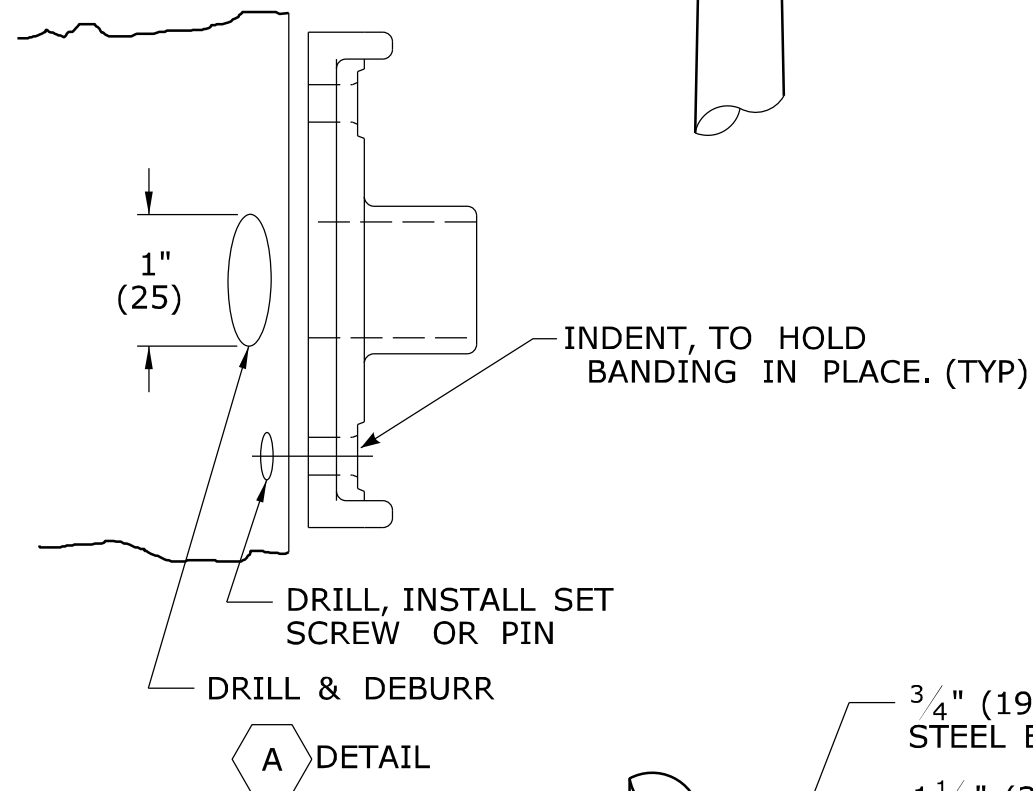
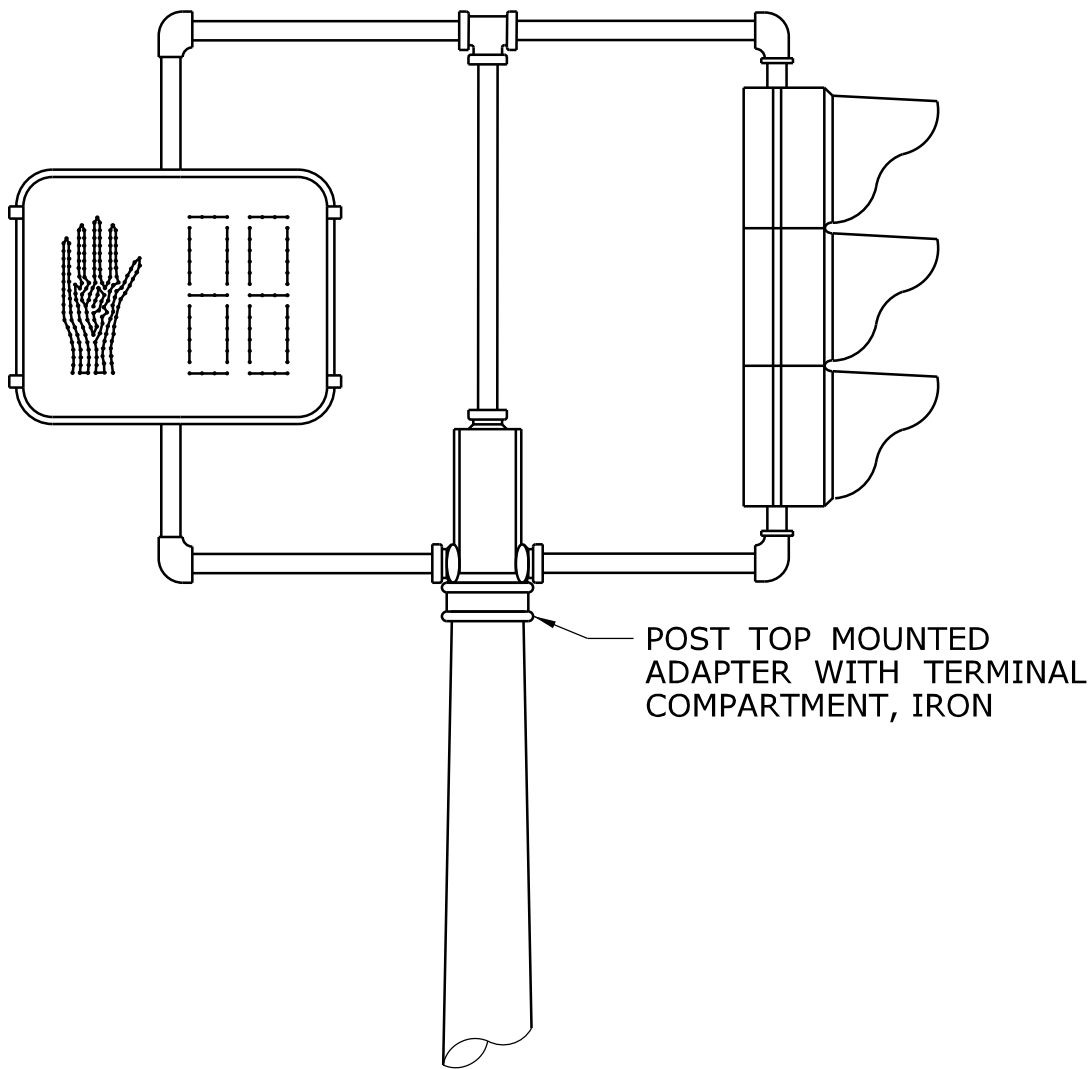
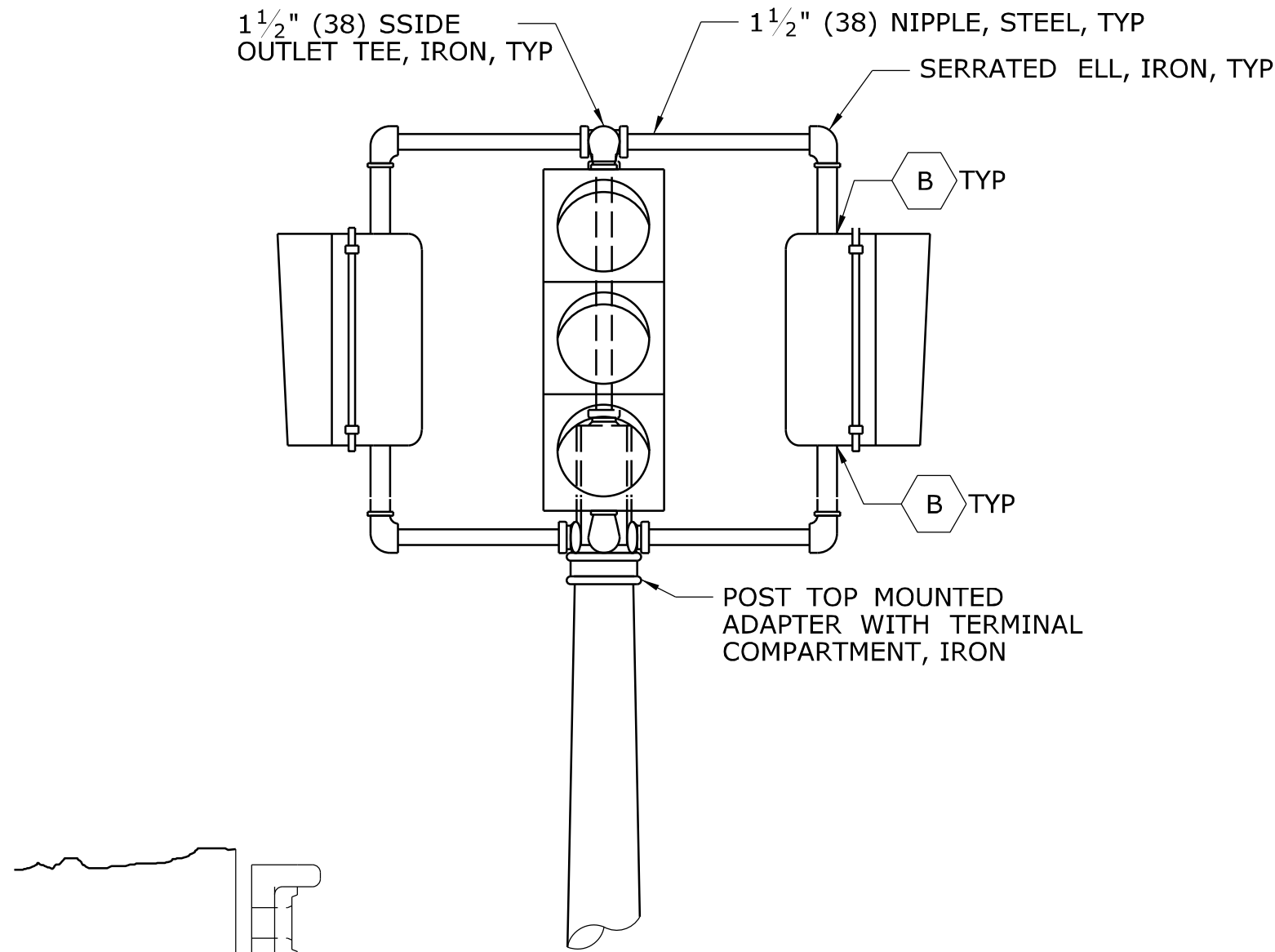
FOR NEW PUSHBUTTON HOUSING,
USE 9" x 15" SIGN NO. 31-0856.

FOR EXISTING PUSHBUTTON HOUSING,
WITH 9" x 12" SIZE, USE SIGN NO. 31-0845.

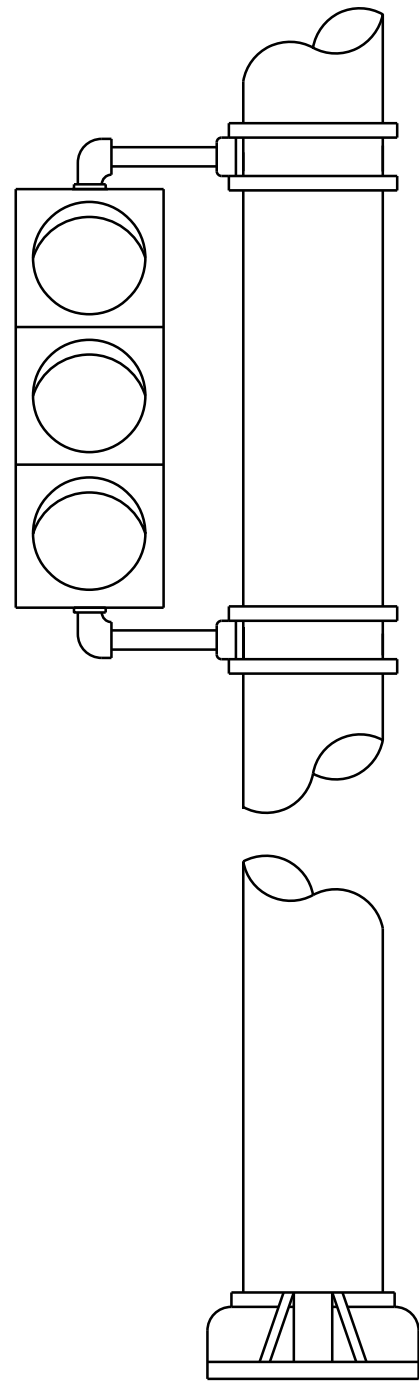
LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:

	PEDESTRIAN PUSH BUTTON
	PEDESTRIAN PUSH BUTTON, PEDESTAL MOUNTED
	PEDESTRIAN PUSH BUTTON, POLE MOUNTED

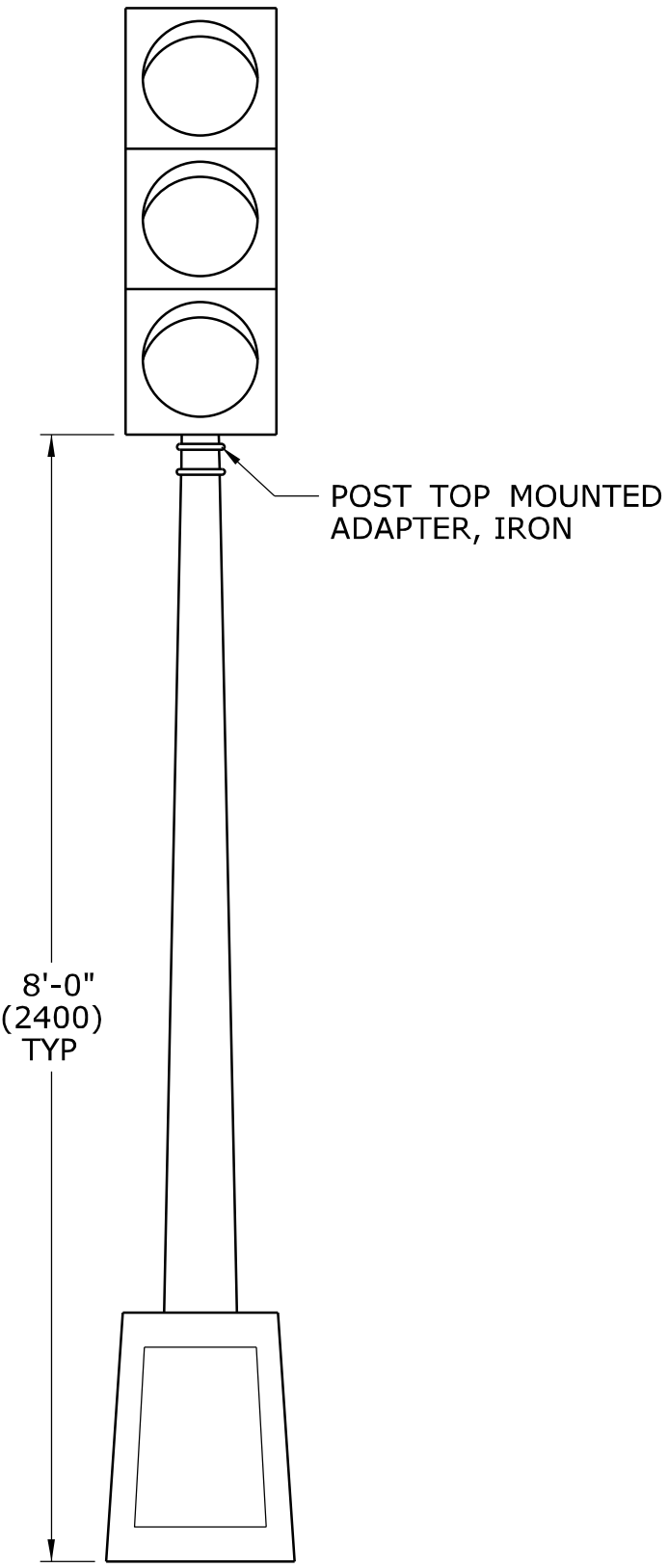
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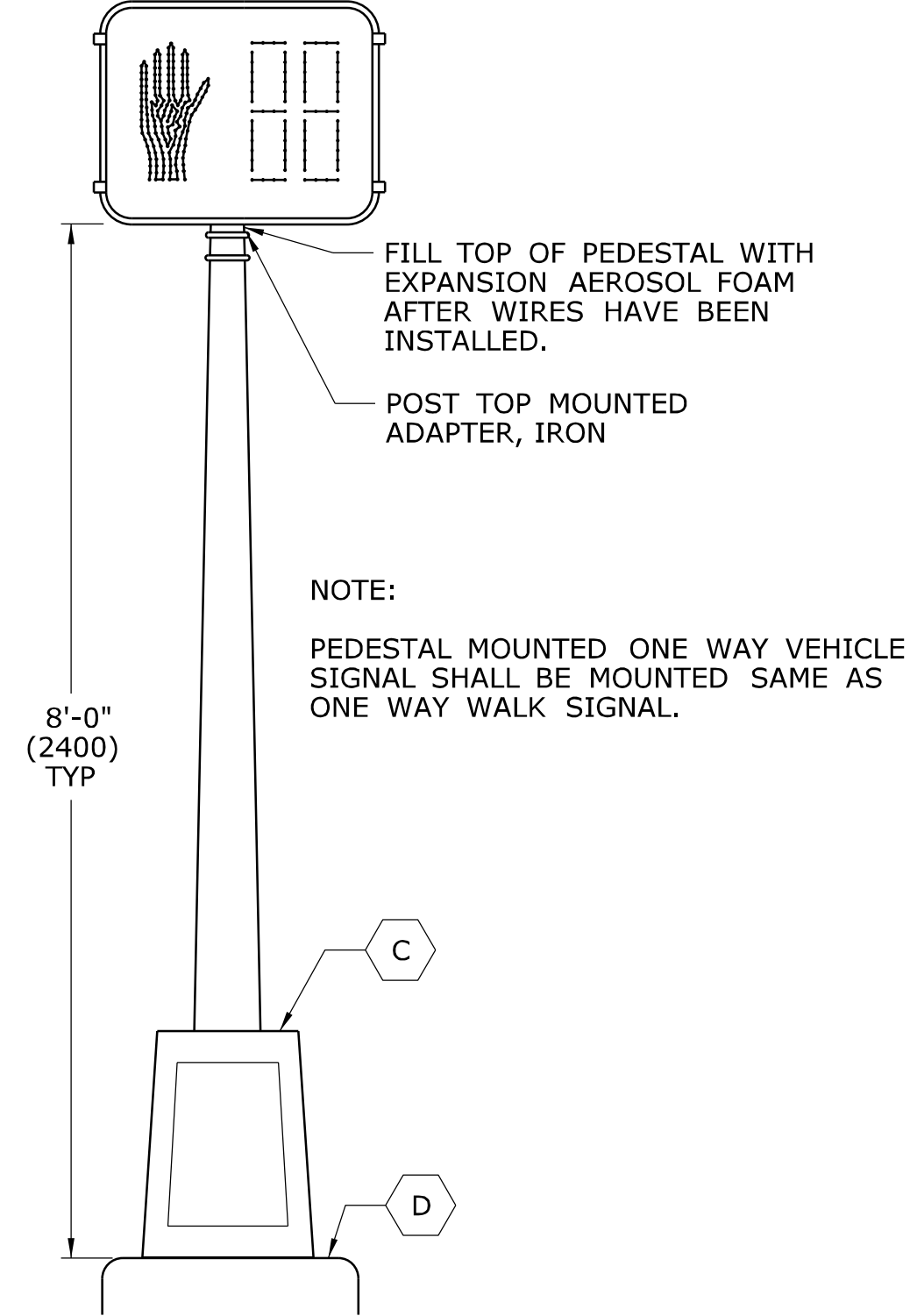
ONE WAY TRAFFIC SIGNAL
PEDESTAL MOUNTED



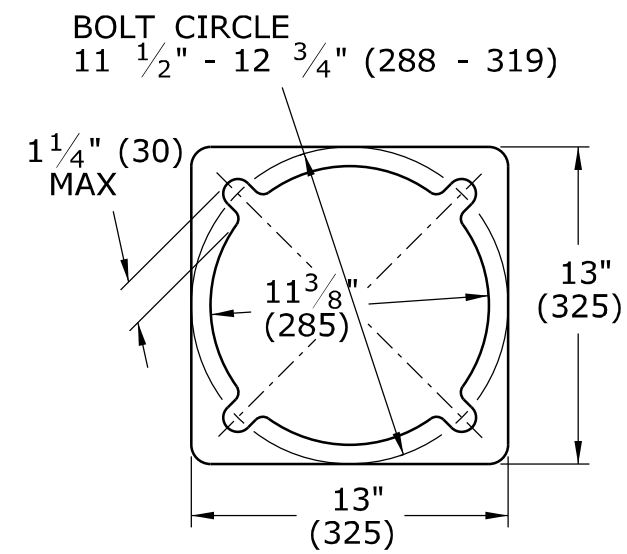
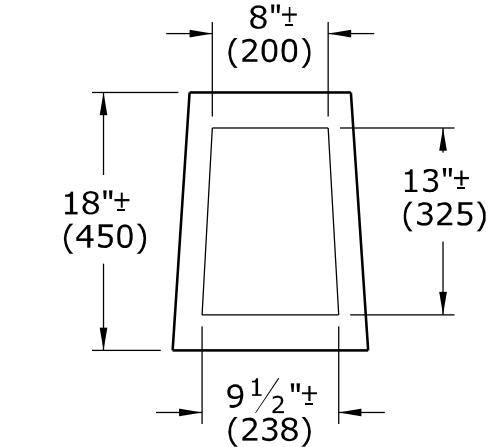
ONE WAY TRAFFIC SIGNAL
POLE MOUNTED



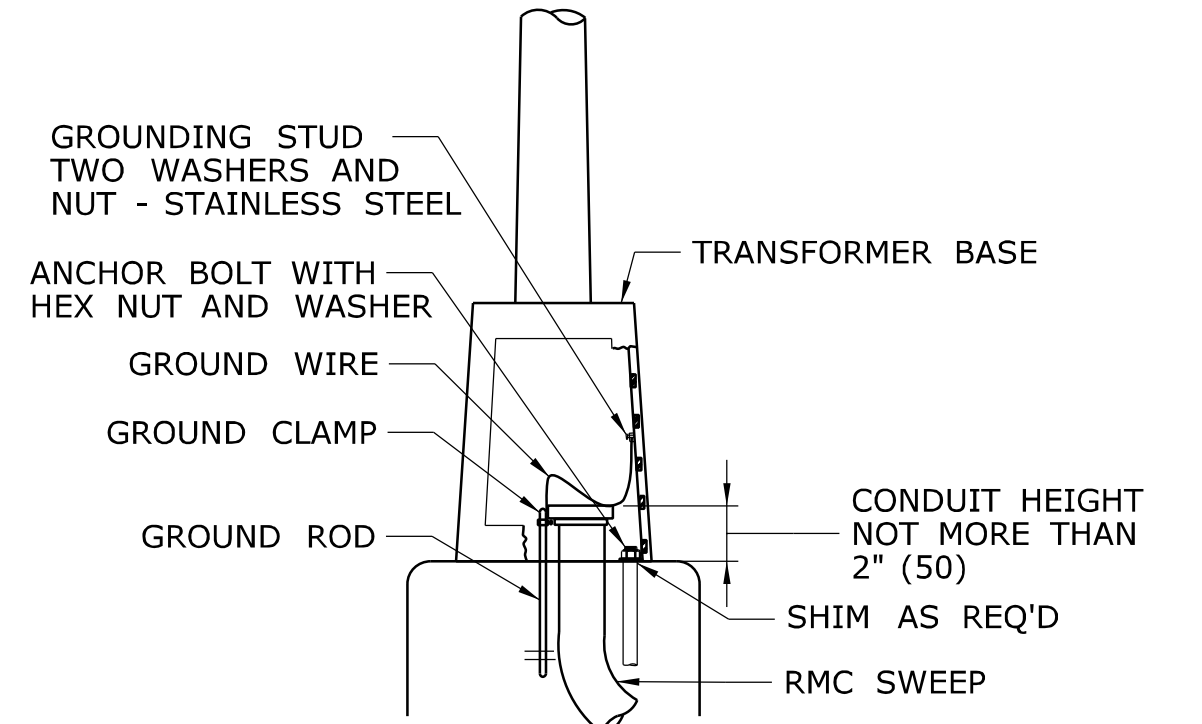
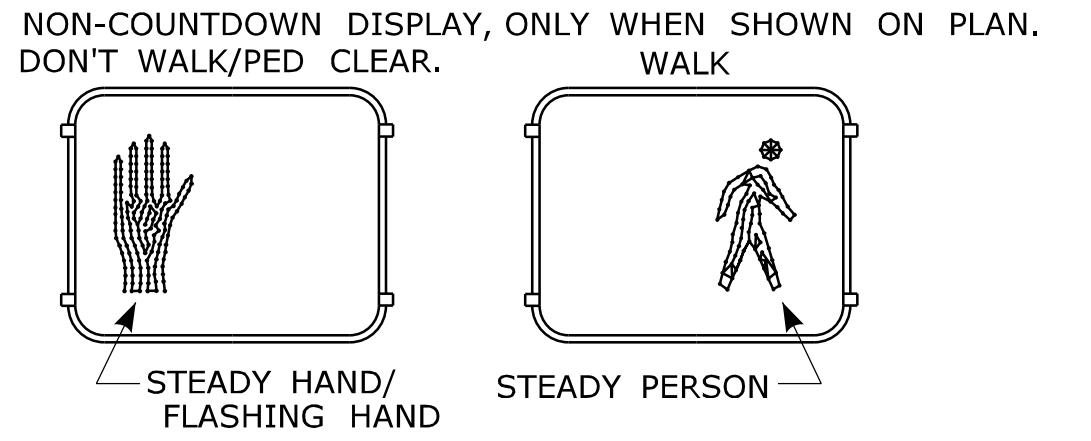
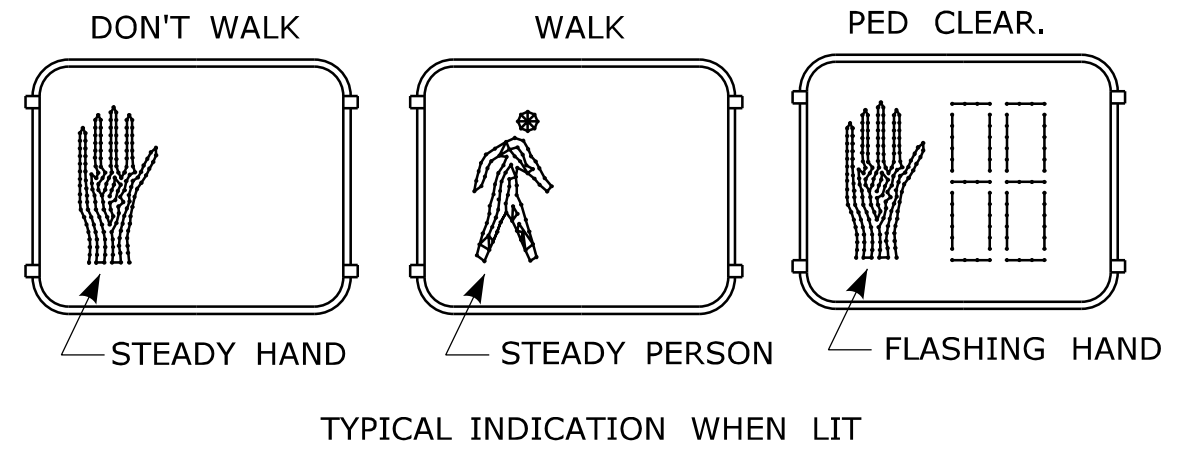
ONE WAY WALK SIGNAL
PEDESTAL MOUNTED



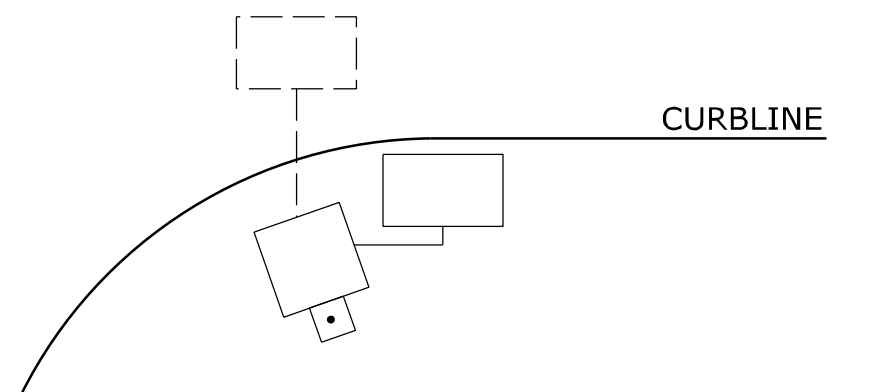
ALUMINUM PEDESTAL
DOOR OPENING DETAIL



PEDESTAL BASE PLAN



ALUMINUM PEDESTAL
INSTALLATION DETAIL









WHEN PEDESTALS OR SPAN POLES ARE INSTALLED CLOSE TO THE CURB, SIDE MOUNT PEDESTRIAN OR TRAFFIC SIGNALS TO AVOID VISOR DAMAGE FROM TURNING VEHICLES.

NOTES:

- A SECURE LOWER HUB PLATE WITH STAINLESS STEEL SET SCREW OR PIN PRIOR TO BANDING TO PREVENT MOVEMENT. INSTALL CABLE THROUGH BOTTOM OF HUB PLATE.
- B REFER TO CTDOT TRAFFIC STANDARD SHEET, TR-1105-01, TRAFFIC SIGNALS & CABLE ASSIGNMENTS.
- C IF THREADED, MIN 1" (25) THREADED INTO BASE, SECURED WITH STAINLESS STEEL SET SCREWS.
- D BASE DESIGNED AS BREAK-AWAY.

INCANDESCENT WALK SIGNAL LAMPS ARE 67 WATTS, RATED AT 8000 HOURS LAMP LIFE. LED WALK SIGNAL LAMPS ARE MAXIMUM 15 WATTS, WARRANTED AT 5 YEAR LIFE.

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:			
	STEEL SPAN POLE, MAST ARM ASSEMBLY SHAFT		PEDESTRIAN SIGNAL
	ALUMINUM PEDESTAL		PEDESTAL MOUNTED, TRAFFIC & PEDESTRIAN SIGNALS
	TRAFFIC SIGNAL		POLE MOUNTED, TRAFFIC & PEDESTRIAN SIGNALS

REV.	DATE	REVISION DESCRIPTION
2	4-2012	MINOR REVISIONS.
1	1-2010	INCLUDED COUNTDOWN PEDESTRIAN SIGNALS.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 4/14/2012

DIMENSIONS ARE IN ENGLISH (") & METRIC UNITS (mm). METRIC DIMENSIONS ARE ROUNDED: - OVER 1" TO NEAREST 5 mm - UNDER 1" TO NEAREST 1 mm.

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_TRAFFIC_STD.dgn Model: TR-1102_01

SUBMITTED BY: NAME/DATE/TIME: Tracy L. Fogarty Tracy L. Fogarty 2012.05.01 12:55:27-04'00'





APPROVED BY: NAME/DATE/TIME: Timothy M. Wilson 2012.05.09 10:24:58-04'00'

CTDOT
STANDARD SHEET

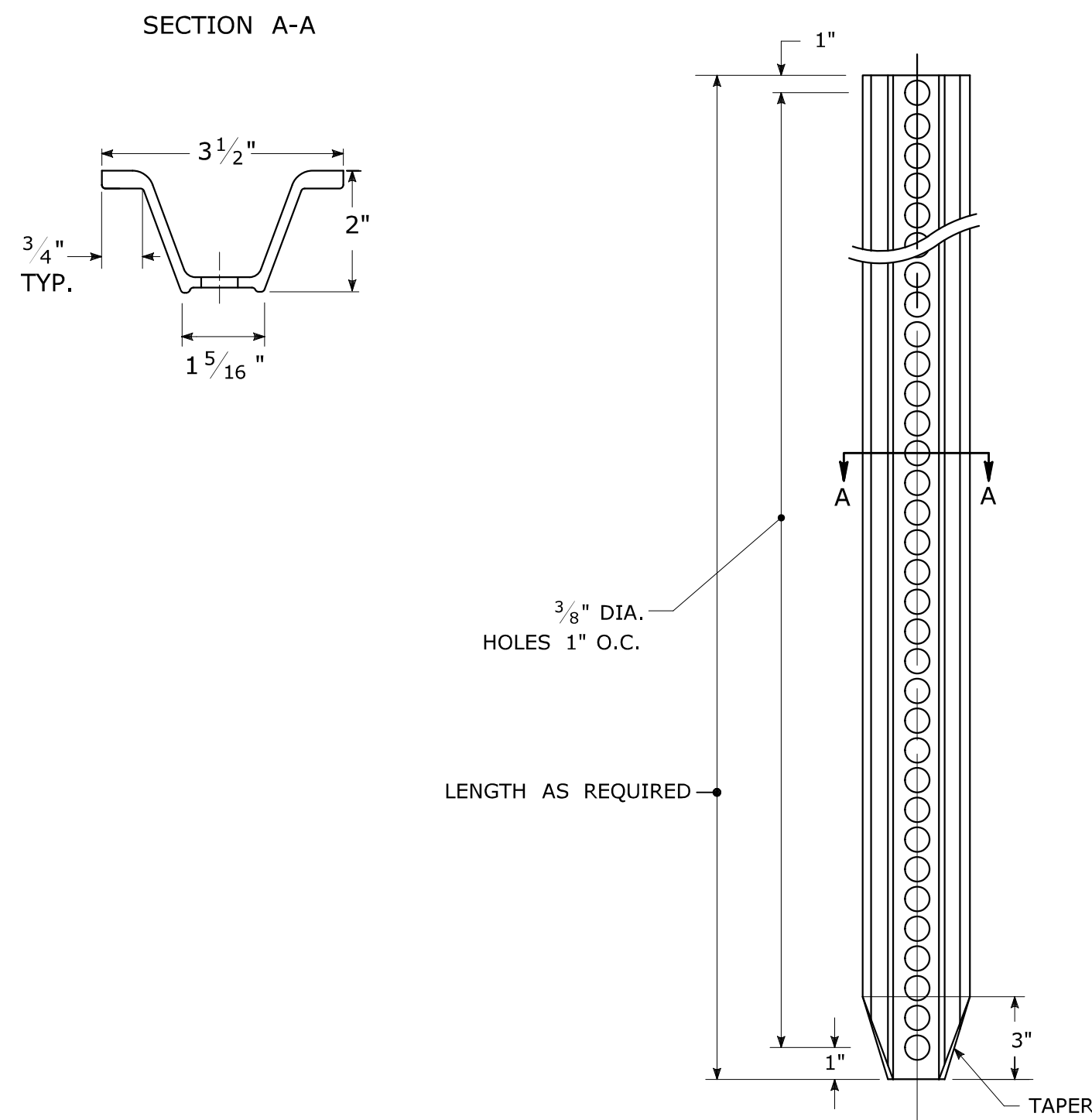
OFFICE OF ENGINEERING

STANDARD SHEET TITLE: PEDESTALS, PEDESTRIAN SIGNALS

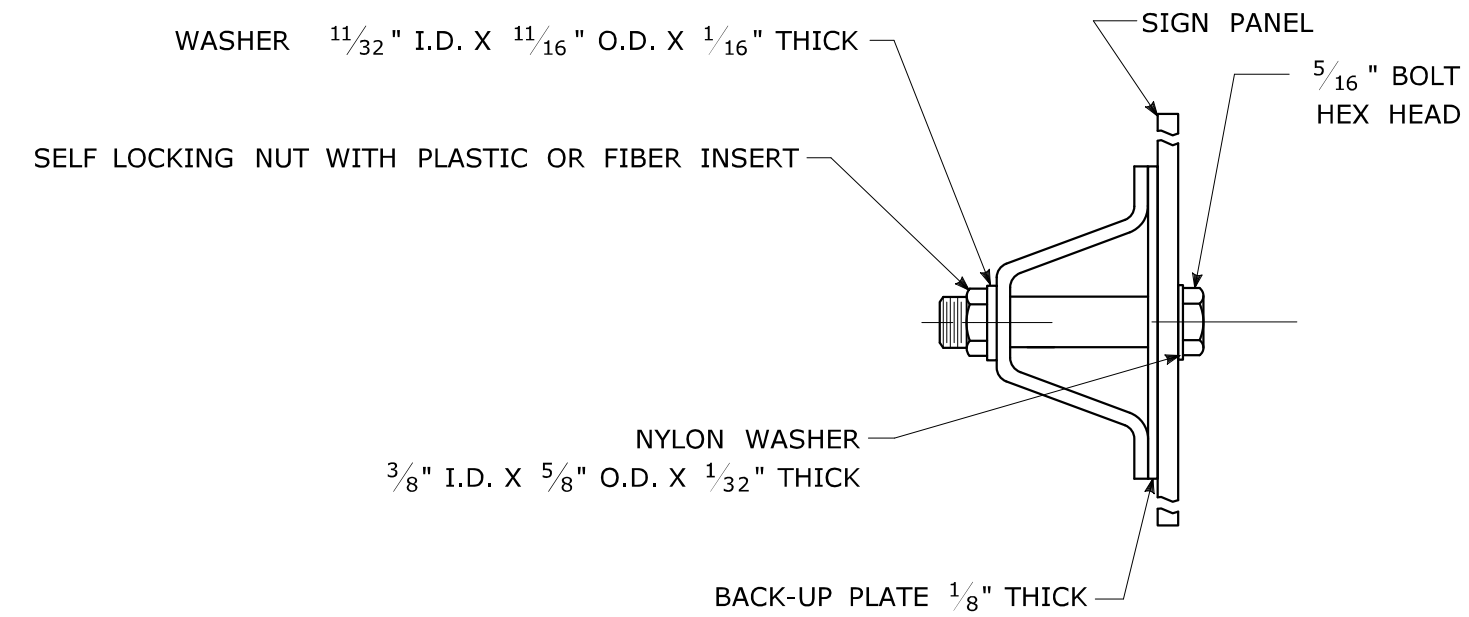
STANDARD SHEET NO.: TR-1102_01

			THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.	NOT TO SCALE	<div><div>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</div></div> <div>Filename: TR_1208_01_1_2018.dgn Model: TR-1208_01</div>	SUBMITTED BY:	NAME/DATE/TIME:	<div>CTDOT STANDARD SHEET</div> <div>OFFICE OF ENGINEERING</div>	STANDARD SHEET TITLE: <div>SIGN PLACEMENT AND RETROREFLECTIVE STRIP DETAILS</div>	STANDARD SHEET NO.: <div>TR-1208_01</div>
3	8-2018	INCLUDED INCIDENT MANAGEMENT AND MILE MARKER SIGNS.					Mark F. Makuch, P.E. 2018.08.17			
2	4-2017	MINOR REVISIONS.				APPROVED BY:	NAME/DATE/TIME:			
1	2-2011	MINOR REVISIONS.					Mark F. Carlo, P.E. 2018.08.21 07:48:06-04'00'			
REV.	DATE	REVISION DESCRIPTION	Plotted Date: 8/10/2018							

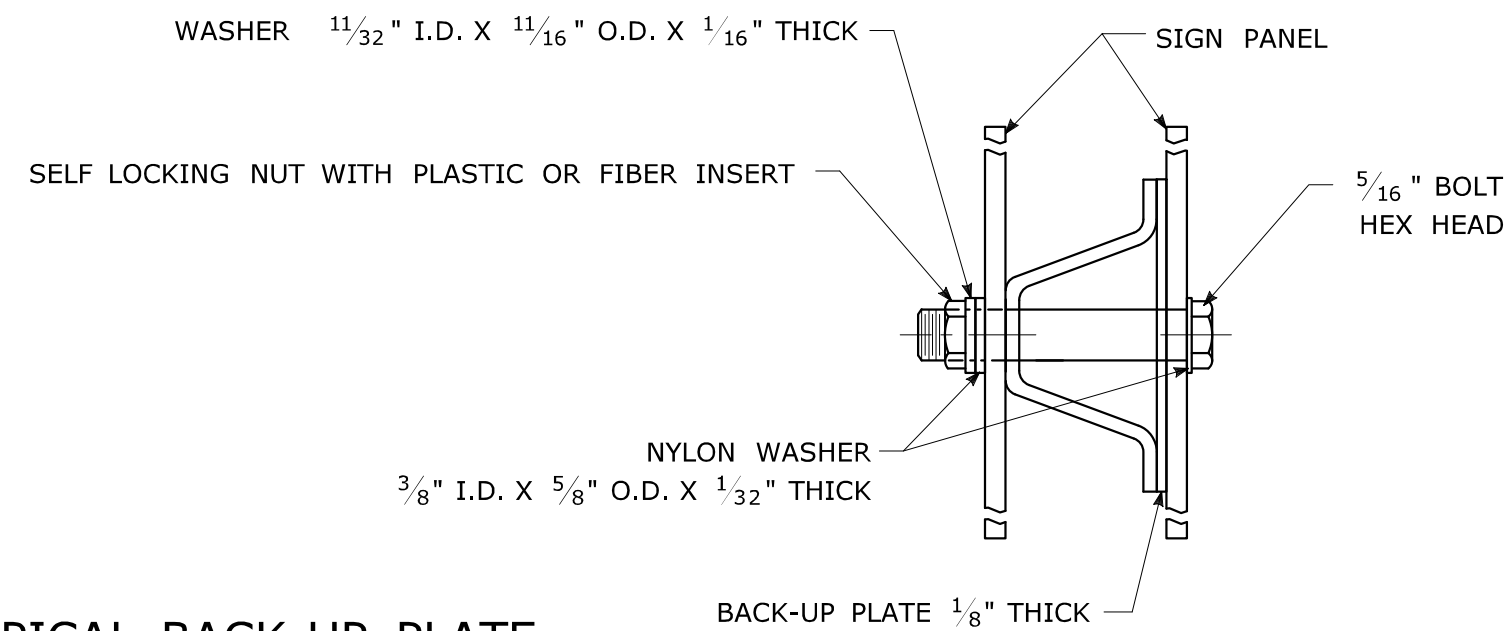
TYPICAL METAL SIGN POSTS



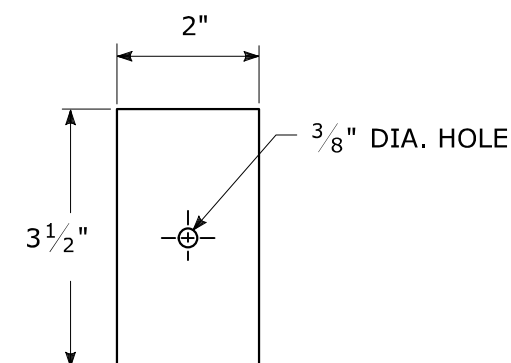
TYPICAL SIGN PANEL ATTACHMENT



TYPICAL BACK TO BACK SIGN PANEL ATTACHMENT

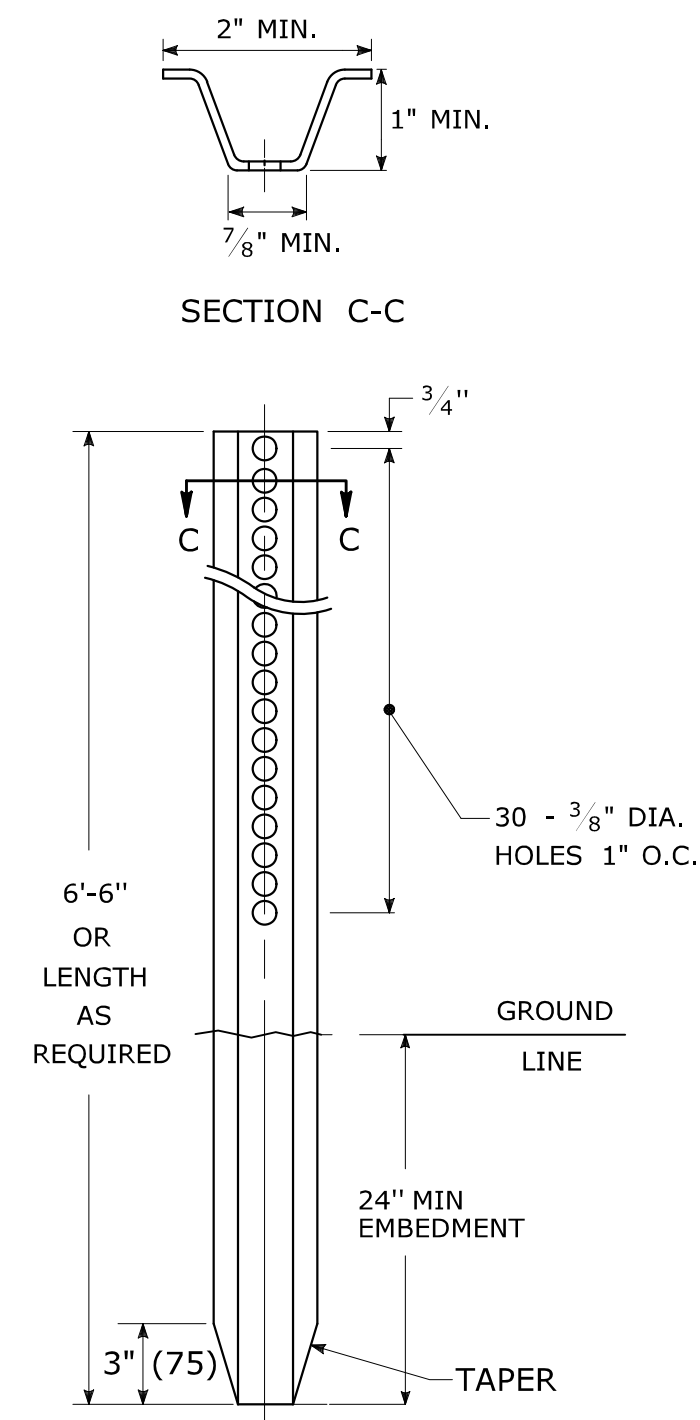


TYPICAL BACK-UP PLATE



BOLTS - STAINLESS STEEL CONFORMING TO ASTM F593,
ALLOY GROUP 1 OR 2 (ALLOY TYPES 304 OR 316).
SELF LOCKING NUTS - STAINLESS STEEL CONFORMING TO ASTM F594,
ALLOY GROUP 1 OR 2 (ALLOY TYPES 304 OR 316).
WASHERS - STAINLESS STEEL CONFORMING TO ASTM A240,
(ALLOY TYPES 304 OR 316).

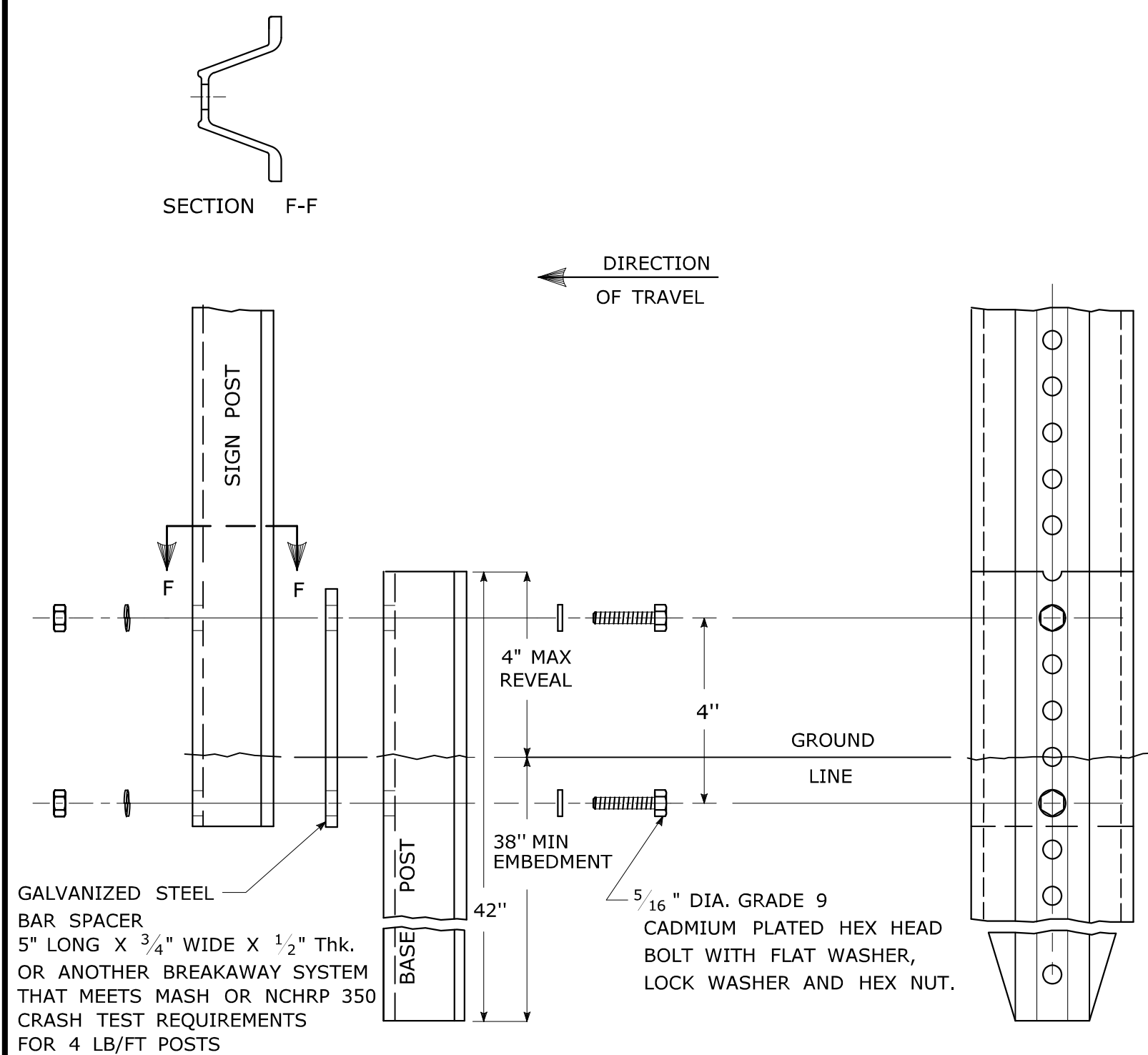
METAL DELINEATOR POST

$$\text{WT./FT.} = 1.12 \text{ LBS./FT. MIN.}$$


GENERAL NOTES:

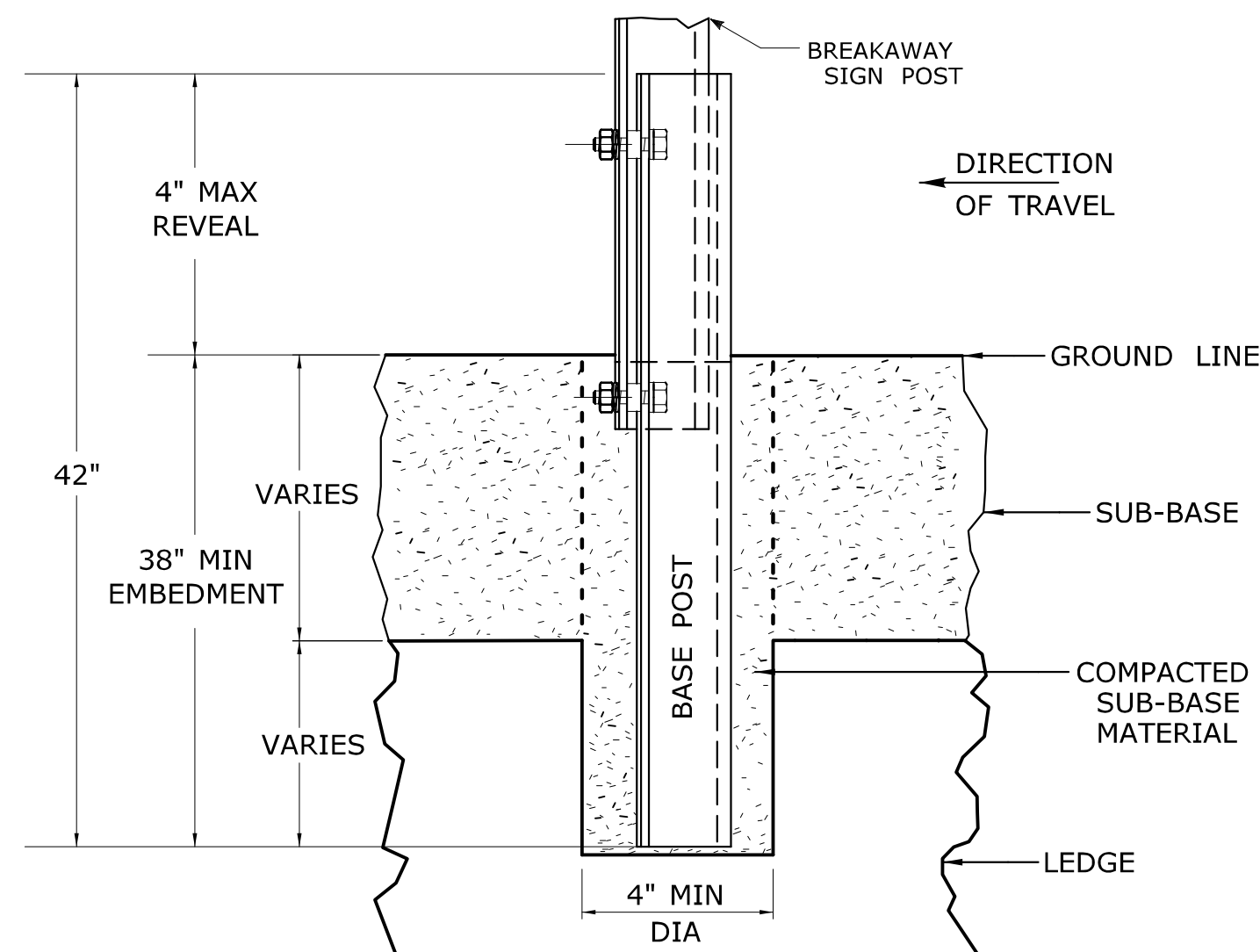
1. STEEL FOR DELINEATOR POSTS SHALL BE ASTM A36 STEEL.
STEEL FOR ALL OTHER POSTS SHALL CONFORM TO THE MECHANICAL REQUIREMENTS OF ASTM A 499 GRADE 80 AND TO THE CHEMICAL REQUIREMENTS OF ASTM A1 CARBON STEEL TEE RAIL HAVING NOMINAL WEIGHT (MASS) OF 91 LBS. OR GREATER PER LINEAR YARD.
2. AFTER FABRICATION, ALL STEEL POSTS, STRAPS AND PLATES SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A123.
3. WASHERS FOR BREAKAWAY INSTALLATIONS SHALL MEET ASTM F436, TYPE 1.
4. SPACER BAR FOR BREAKAWAY INSTALLATION SHALL CONFORM TO THE MECHANICAL REQUIREMENTS OF ASTM A36.
5. ALL BOLTS, NUTS, AND WASHERS FOR BREAKAWAY INSTALLATIONS SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A153.
6. ALL SIGN POSTS SHALL HAVE BREAKAWAY FEATURES THAT MEET AASHTO REQUIREMENTS CONTAINED IN THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS." THE BREAKAWAY FEATURES SHALL BE STRUCTURALLY ADEQUATE TO CARRY THE SIGNS SHOWN IN THE PLANS AT 60 mph WIND LOADINGS. INSTALLATIONS SHALL BE IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS.
7. SIGN POSTS SHALL BE 4 LBS./FT.

BREAKAWAY INSTALLATION

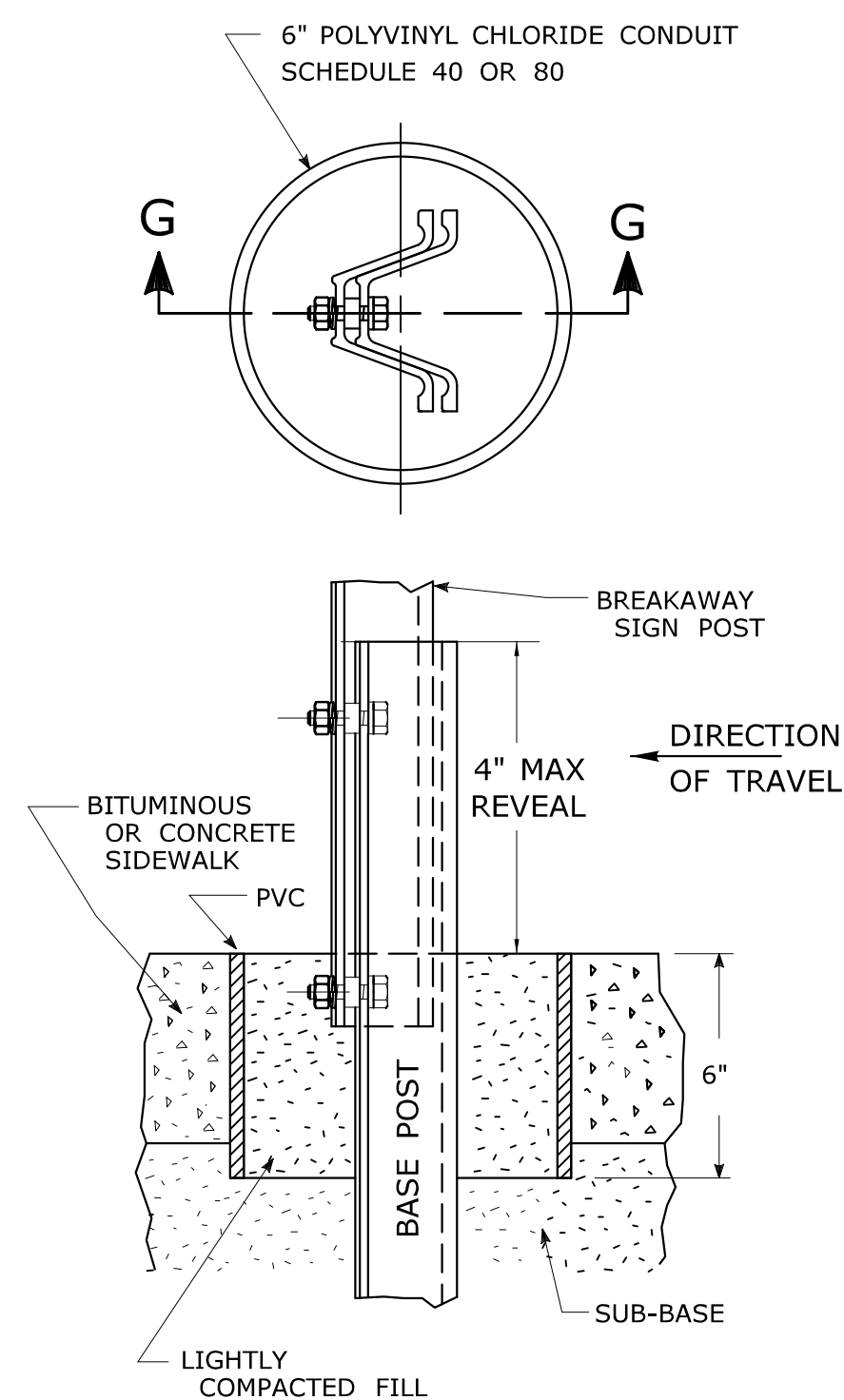


TYPICAL SIGN POST INSTALLATION IN LEDGE

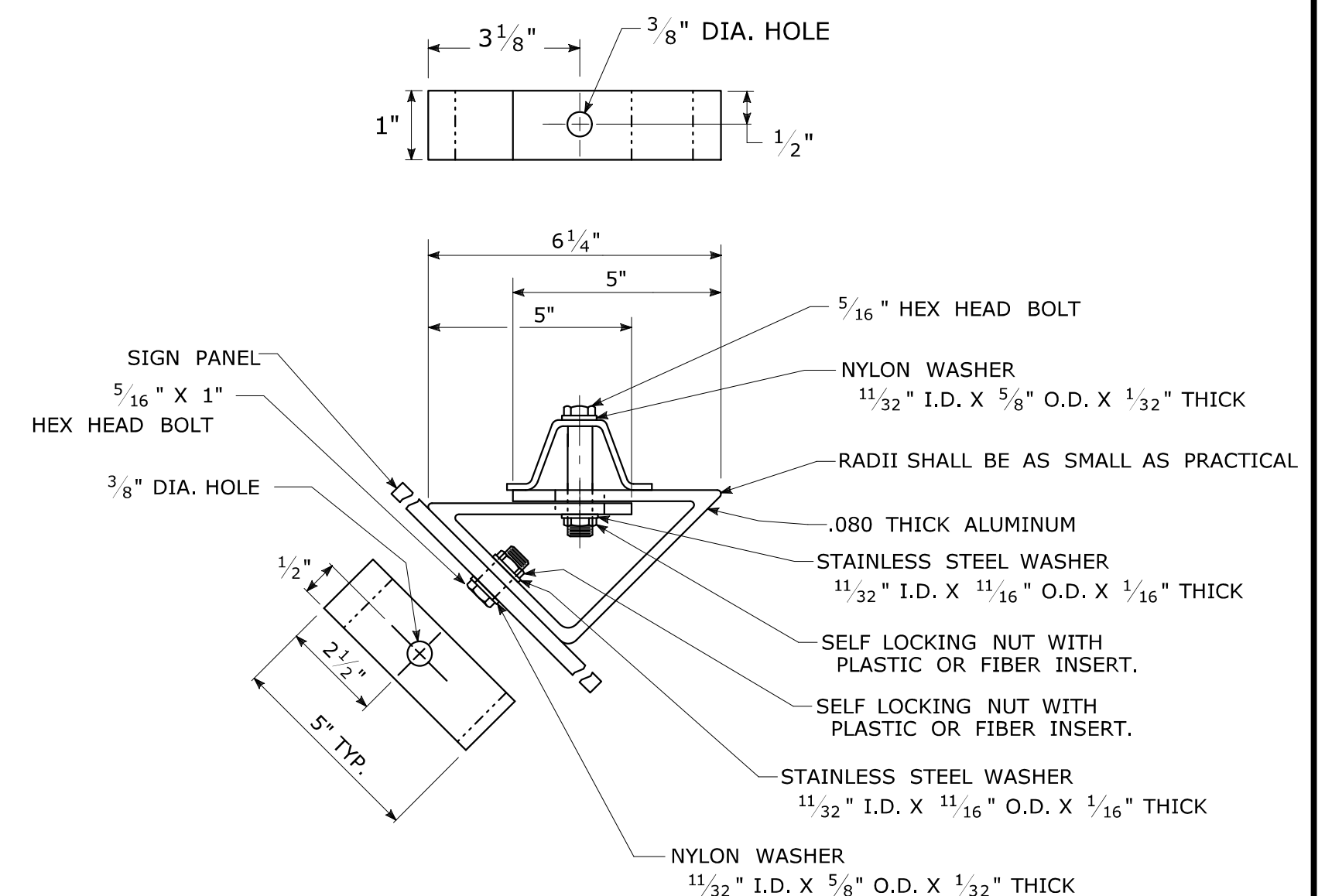
HOLE SHALL BE FILLED WITH SUB-BASE MATERIAL AND COMPACTED WITH A TAMPING BAR, OR TECHNIQUE APPROVED BY THE ENGINEER, PRIOR TO BASE POST INSTALLATION.

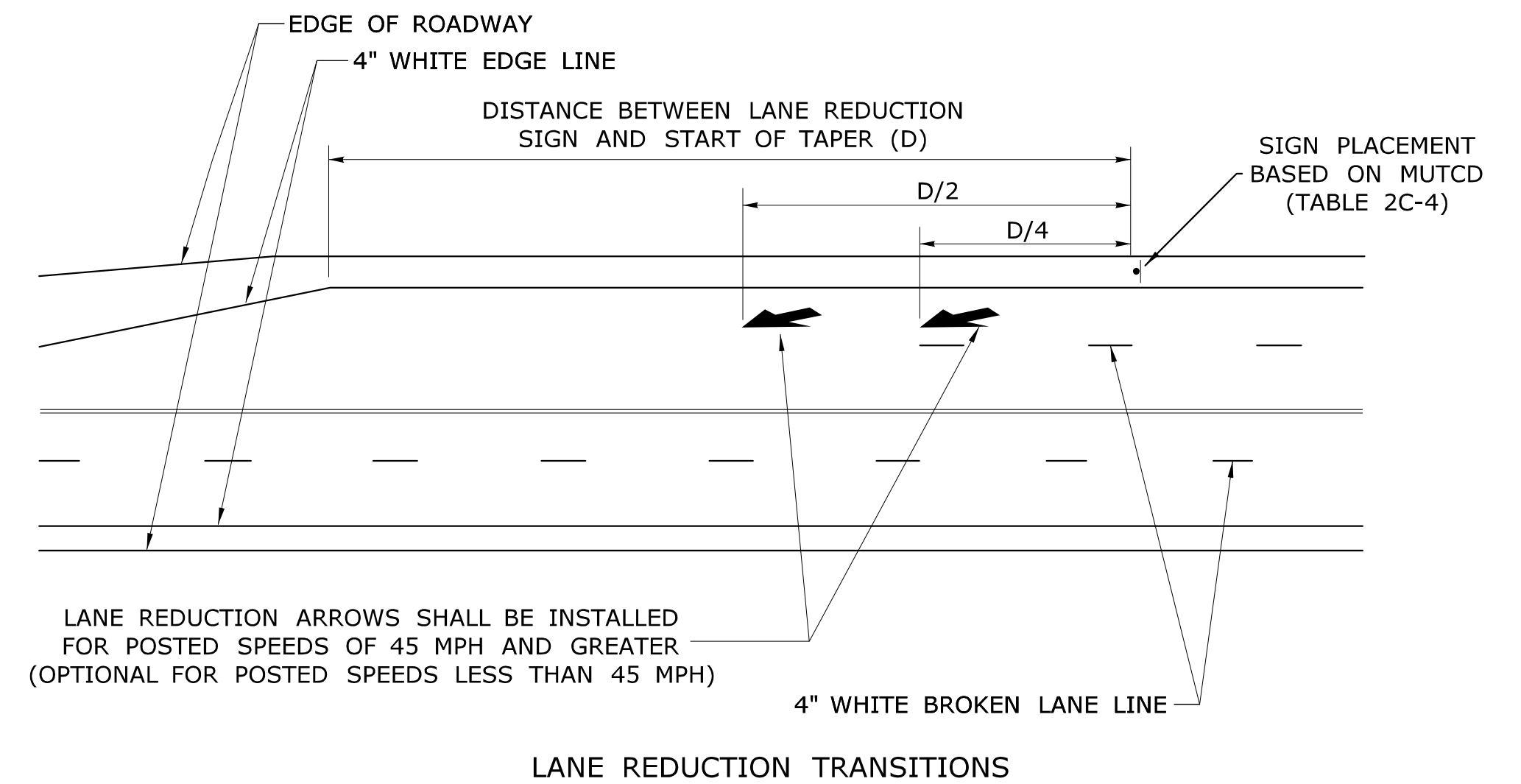
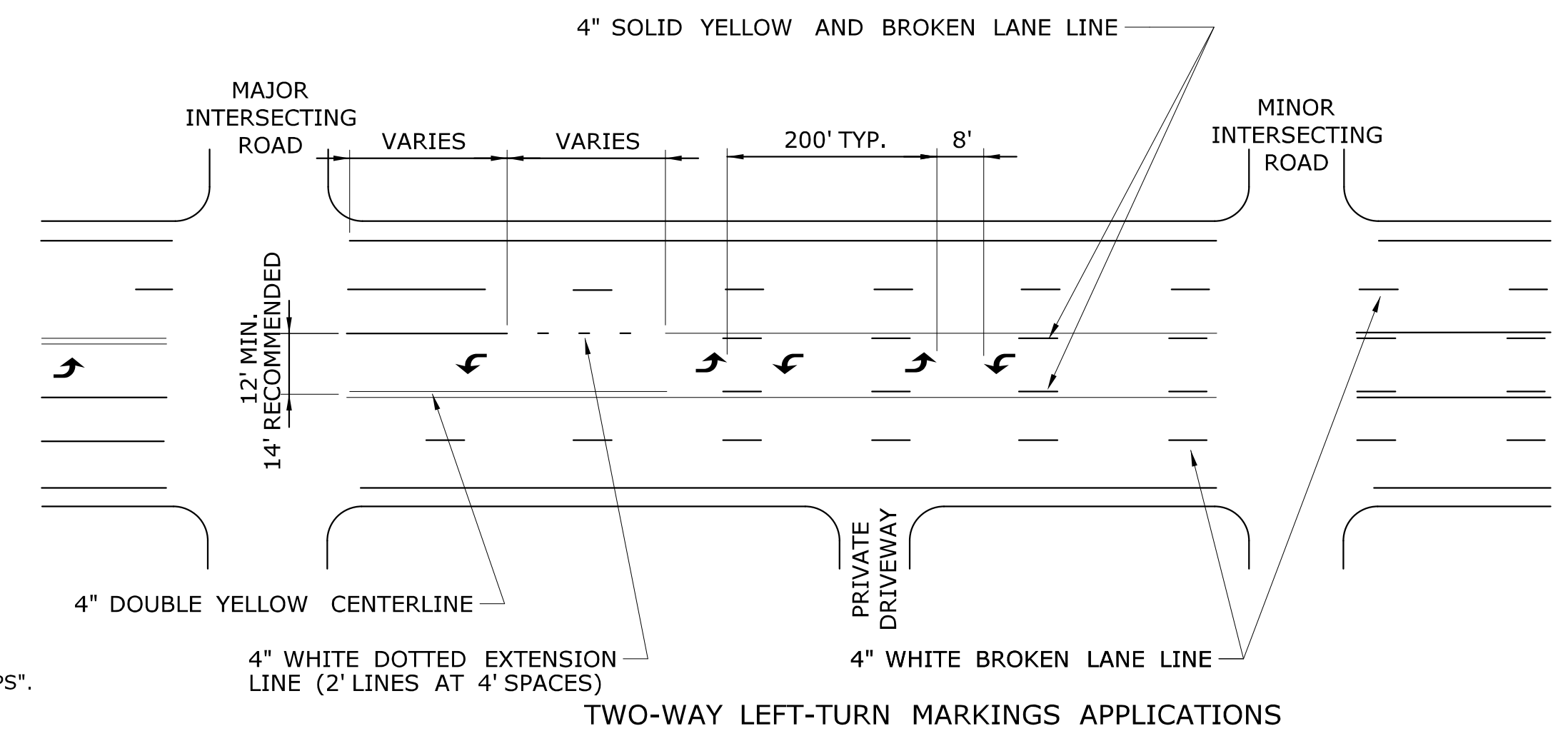
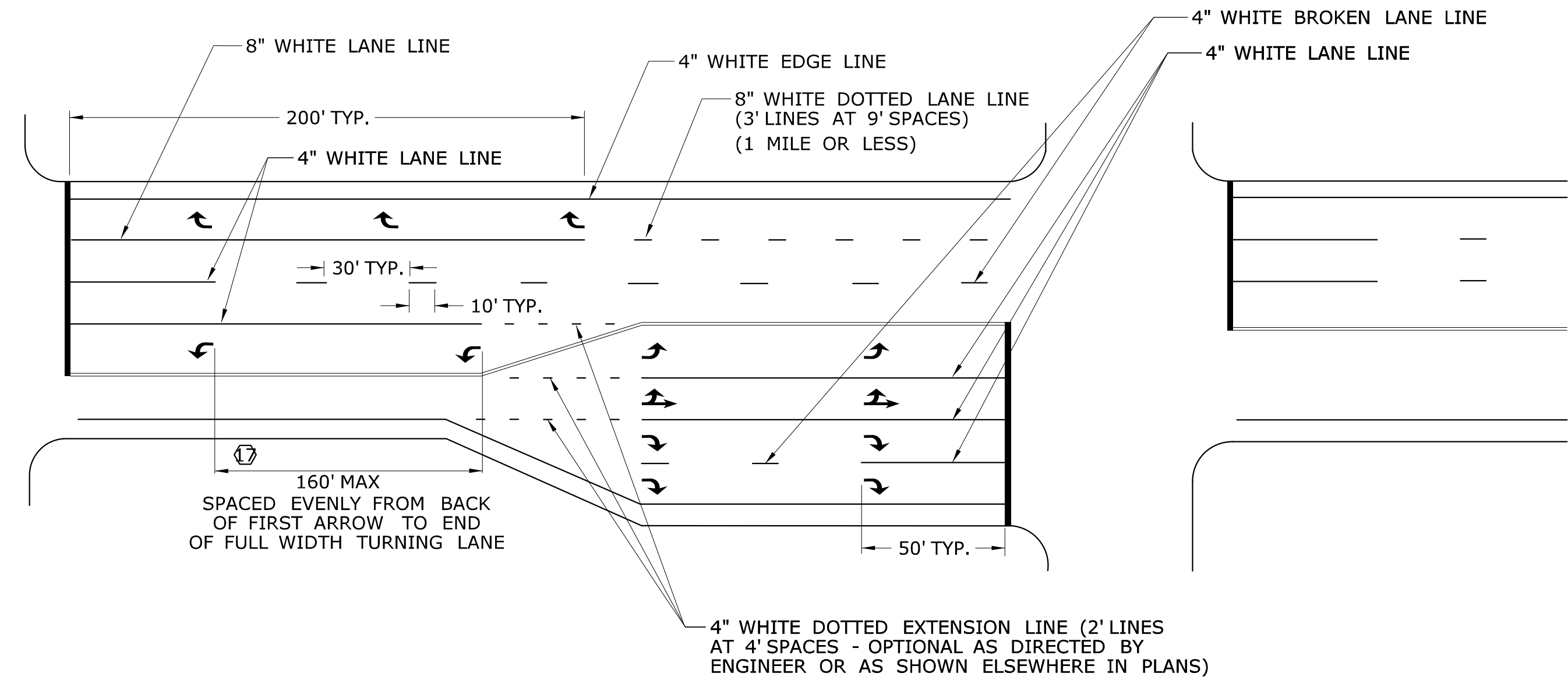


TYPICAL SLEEVE FOR PAVED AREAS



45° MOUNTING BRACKET FOR INSTALLATION OF PARKING SIGNS

[illegible]



- NOTES:
- STOP BARS AND YIELD LINES
1. STOP BARS AND YIELD LINES SHALL BE WHITE.
 2. STOP BARS SHALL BE 12" MIN. UNLESS OTHERWISE NOTED ON PLANS.
 3. STOP BARS TO BE PLACED A MINIMUM OF 4' IN ADVANCE OF THE NEAREST EDGE OF CROSSWALK AND SHOULD BE PLACED 90° TO THE CENTERLINE OF THE ROADWAY. TO
 4. IN THE ABSENCE OF A MARKED CROSSWALK THE STOP BAR SHOULD BE PLACED 90° THE CENTERLINE OF THE ROADWAY, AT THE DESIRED STOPPING POINT AT LEAST 5' AND NO MORE THAN 30' FROM THE NEAREST EDGE OF THE INTERSECTING ROADWAY.
 5. THE STOP SIGN SHOULD BE PLACED IN LINE WITH THE STOP BAR. HOWEVER, IF THE STOP SIGN CANNOT BE LOCATED EXACTLY WHERE VEHICLES ARE EXPECTED TO STOP, THE STOP BAR SHOULD BE PLACED AT THE STOPPING POINT.
 6. FOR STOP BARS AT RAMPS SEE DETAILS "R", "S", "T", & "U" AND NOTES ON TRAFFIC STANDARD SHEET TR-1210 07 "PAVEMENT MARKINGS FOR DIVIDED HIGHWAYS EXIT RAMPS".
 7. FOR YIELD LINE INSTALLATIONS, ONLY FULL TRIANGLES ARE TO BE INSTALLED.
- ⑧ MID-BLOCK CROSSWALKS ARE CROSSWALKS LOCATED MORE THAN 50 FEET FROM A SIGNALIZED OR UNSIGNALIZED INTERSECTION. YIELD LINES ASSOCIATED WITH MIDBLOCK CROSSWALKS SHALL BE INSTALLED AND SHOULD BE LOCATED 20 TO 50 FEET IN ADVANCE OF THE NEAREST CROSSWALK LINE OR AS DIRECTED BY THE ENGINEER.
- WHERE A YIELD LINE EXISTS ON AN APPROACH TO A CROSSWALK, THE APPROPRIATE "YIELD TO PEDESTRIANS" SIGN IS REQUIRED.
- ⑨ FOR CROSSWALKS AT UNSIGNALIZED INTERSECTIONS WITH MINOR STREET STOP CONTROL, YIELD LINES SHALL BE INSTALLED ON MULTI-LANE APPROACHES, BUT NOT SINGLE LANE APPROACHES.
10. THE YIELD SIGN SHOULD BE PLACED IN LINE WITH A YIELD LINE. HOWEVER, IF THE YIELD SIGN CANNOT BE LOCATED EXACTLY WHERE VEHICLES ARE EXPECTED TO YIELD, THE YIELD LINE SHOULD BE PLACED AT THE YIELDING POINT.

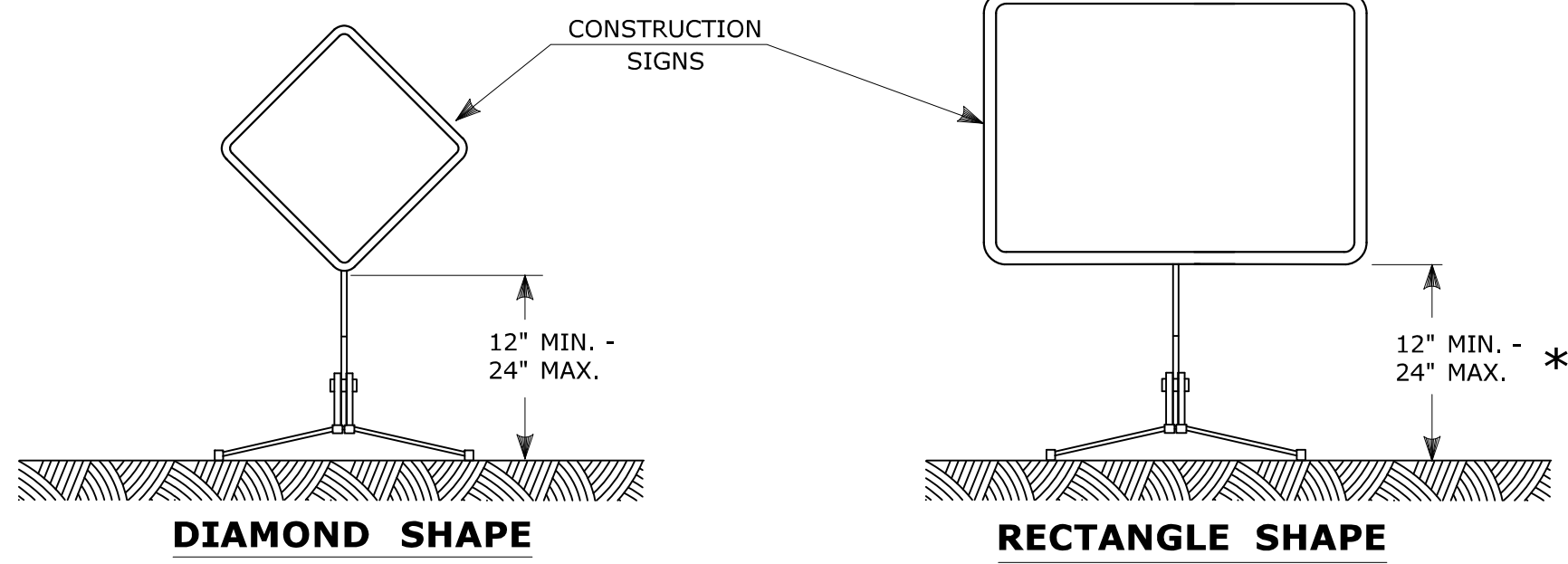
CROSSWALKS

11. CROSSWALK MARKINGS SHALL BE WHITE.
12. AT LOCATIONS WHERE THE CROSSWALK IS SKEWED, BARS TO BE PARALLEL TO C AND ENDS OF BARS TO BE PARALLEL. THE LENGTH OF THE BARS WILL VARY DEPENDING ON THE ANGLE OF SKEW.
13. BARS SHOULD BE NO CLOSER THAN 1' FROM EDGE OF ROAD.
14. ONLY FULL LENGTH BARS ARE TO BE INSTALLED.
15. DECORATIVE CROSSWALKS SHALL BE BANDED FROM CURB TO CURB WITH A MINIMUM 12" WIDE WHITE TRANSVERSE LINE ALONG EACH EDGE.
16. 24" WIDE SPACE TO BE CENTERED ON YELLOW CENTERLINE.

PAVEMENT MARKINGS FOR TURNING LANES

- 17 INSTALL AT LEAST TWO ARROWS PER LANE WHERE STORAGE LENGTH IS GREATER THAN 150 FEET.

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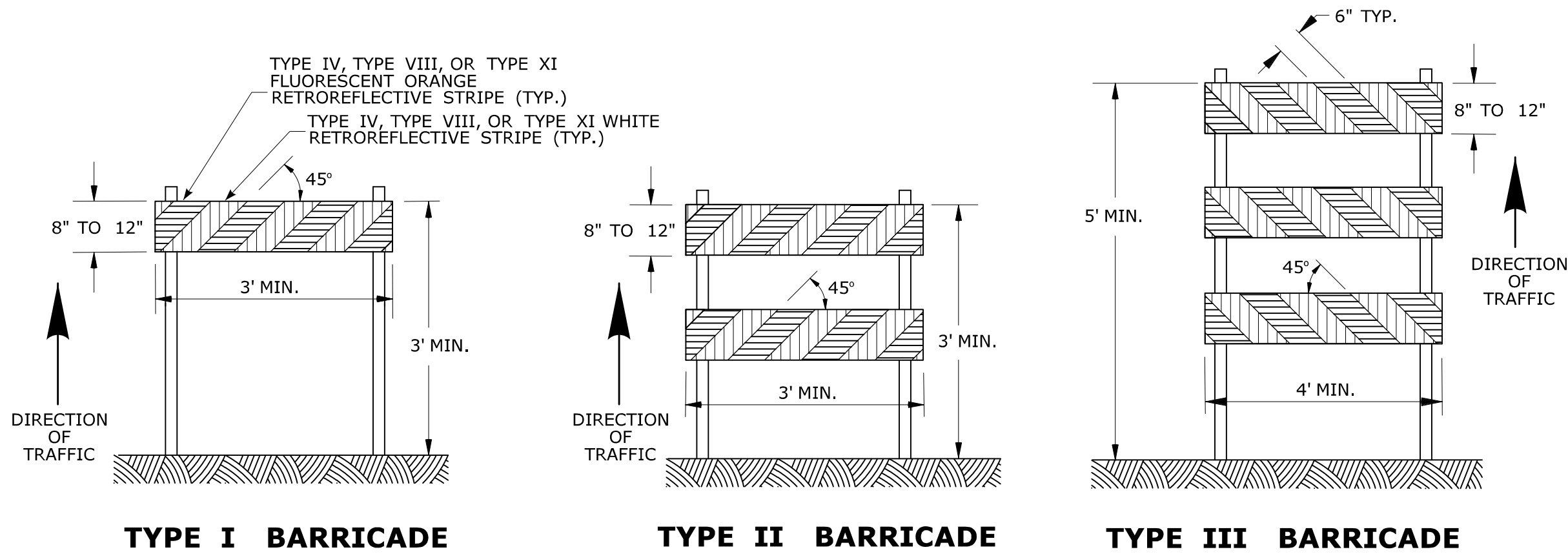


PORTABLE CONSTRUCTION SIGNS

NOTES FOR PORTABLE SIGN SUPPORTS:

- SIGNS AND THEIR PORTABLE SUPPORTS SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 2 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- MOUNTING HEIGHT OF SIGNS SHALL BE A MINIMUM OF 12" AND A MAXIMUM OF 24". SIGNS SHALL BE MOUNTED HIGHER AS NEEDED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY SUPPORT DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- PORTABLE SIGN SUPPORTS SHALL BE STABILIZED IN A MANNER THAT WILL NOT AFFECT THEIR COMPLIANCE WITH NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 2 DEVICES.
- PORTABLE CONSTRUCTION SIGN SUPPORTS SHOULD NOT BE USED FOR DURATION OF MORE THAN 3 DAYS EXCEPT FOR R9-8 THROUGH R9-11a SERIES, R11 SERIES, W1-6 THROUGH W1-8 SERIES, M4-10, AND E5-1. SEE STANDARD SHEET TR-1220.01 - "SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS" FOR SIGN DETAILS.

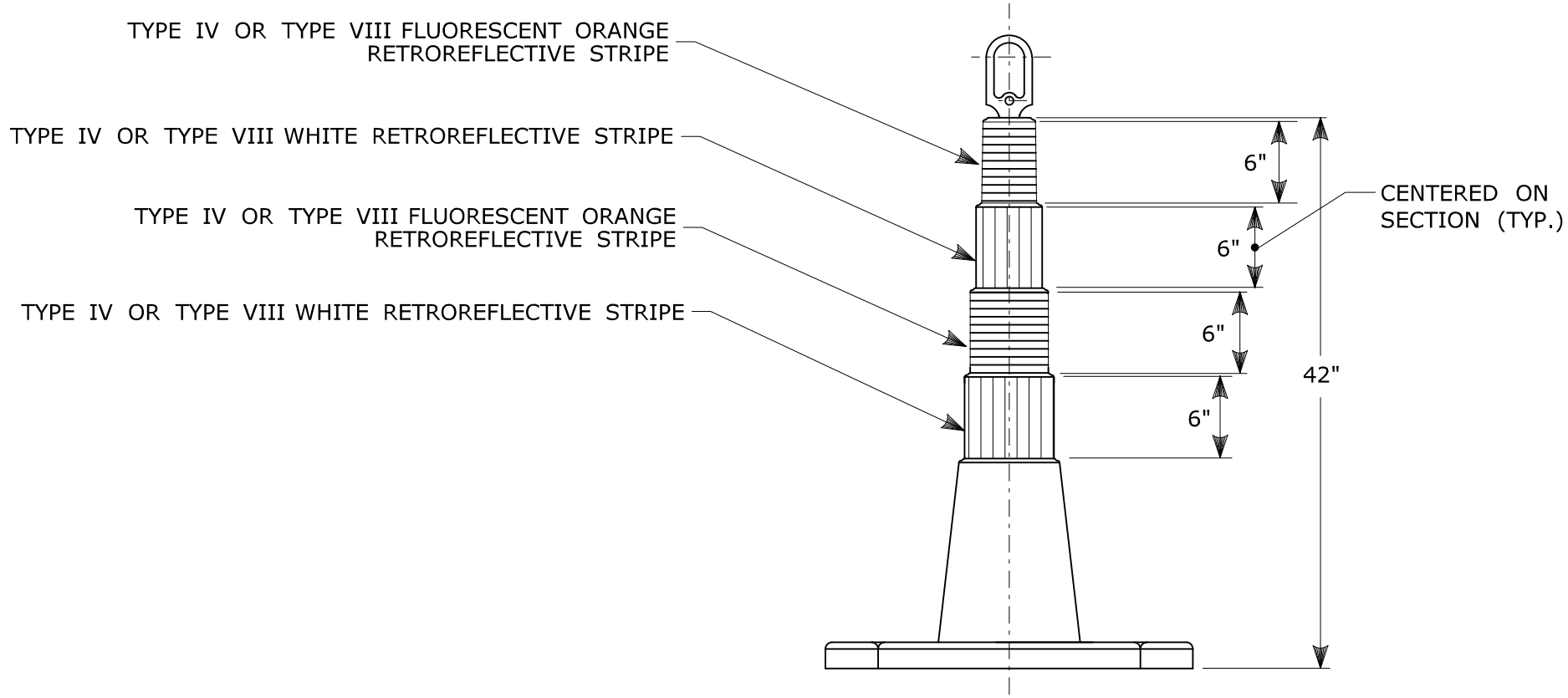
* FOR E5-1 (EXIT SIGNS) USE MIN 48".



CONSTRUCTION BARRICADES

NOTES:

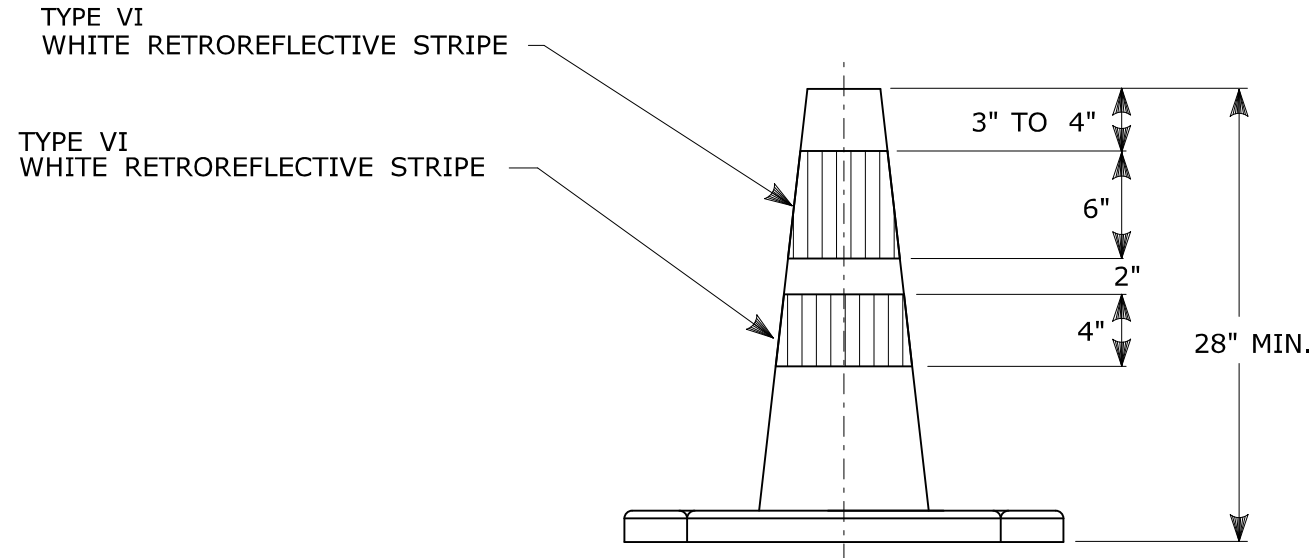
- CONSTRUCTION BARRICADES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH AND THE LATEST EDITION OF THE MUTCD.
- MARKINGS FOR BARRICADE RAILS SHALL BE ALTERNATE FLUORESCENT ORANGE AND WHITE STRIPES SLOPING DOWNWARD IN THE DIRECTION TRAFFIC IS TO PASS. 6" WIDE STRIPES SHALL BE USED.
- THE ENTIRE AREA OF FLUORESCENT ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS. THE SIDES OF BARRICADES FACING TRAFFIC SHALL HAVE RETROREFLECTIVE RAIL FACES.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY BARRICADE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- CORNERS OF BARRICADE RAILS SHALL BE ROUNDED.
- SIGNS MAY ONLY BE INSTALLED ON TYPE III BARRICADES AND SHALL BE PLACED SO AS TO COVER NO MORE THAN ONE BARRICADE RAIL.



42" TRAFFIC CONE

NOTES:

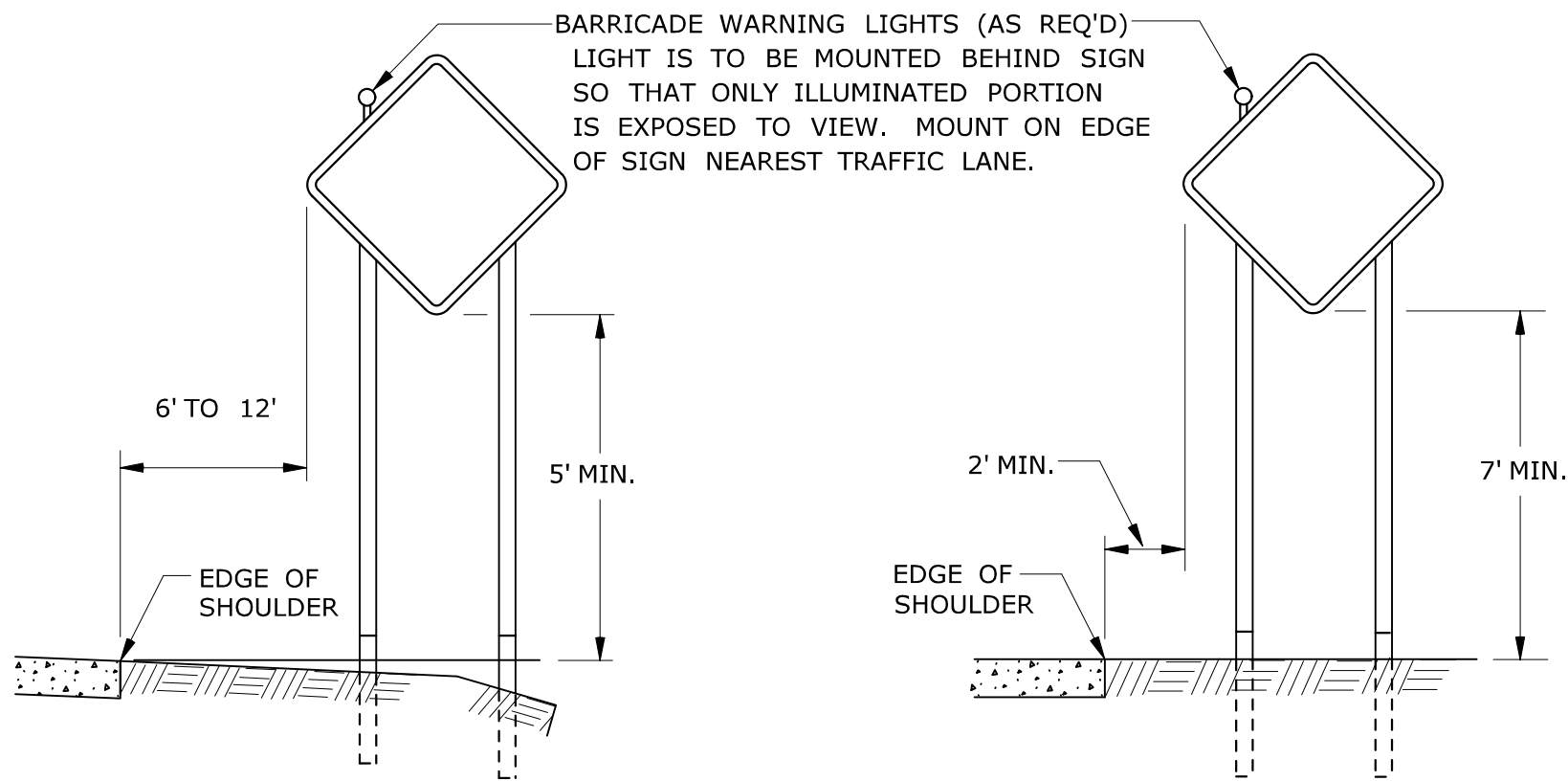
- TRAFFIC CONES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- IF RUBBER CONES ARE USED, THEY SHALL HAVE INTERIOR RIBS FOR RIGIDITY.
- IF PLASTIC CONES ARE USED, THEY SHALL BE COLOR IMPREGNATED.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY CONE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- THE ENTIRE AREA OF FLUORESCENT ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
- THE SECTIONS OF CONES NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.



TRAFFIC CONE

NOTES:

- TRAFFIC CONES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- IF RUBBER CONES ARE USED, THEY SHALL HAVE INTERIOR RIBS FOR RIGIDITY.
- IF PLASTIC CONES ARE USED, THEY SHALL BE COLOR IMPREGNATED.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY CONE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- THE ENTIRE AREA OF WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
- TRAFFIC CONES NOT USED AT NIGHT MAY UTILIZE TYPE III SHEETING.
- THE SECTIONS OF CONES NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.



RURAL AREA

URBAN AREA

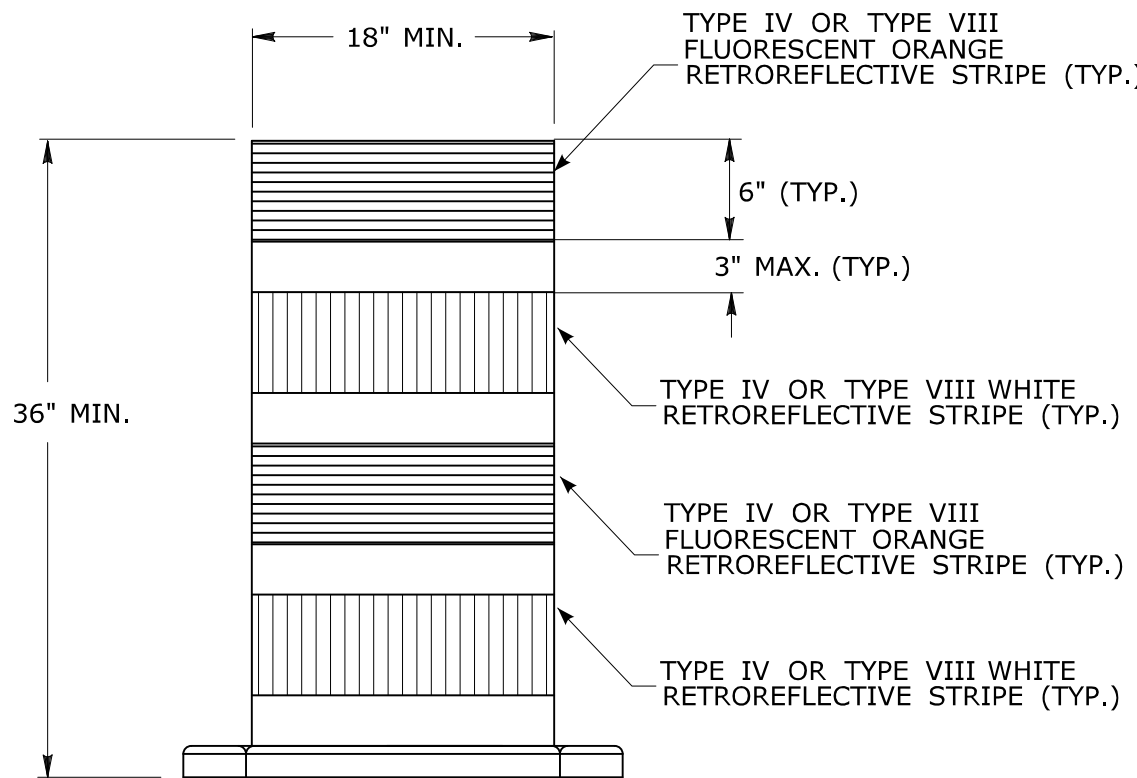
PLACEMENT OF CONSTRUCTION SIGNS
TYPICAL LONG TERM INSTALLATION

NOTES:

SUPPORTS SHALL BE METAL SIGN POSTS AND HAVE BREAK-AWAY FEATURES.

REFER TO STANDARD SHEETS:



- TR-1208.01 - "SIGN PLACEMENT AND RETROREFLECTIVE STRIP DETAILS."
TR-1208.02 - "METAL SIGN POSTS AND SIGN MOUNTING DETAILS."



TRAFFIC DRUM
FRONT VIEW

NOTES:

- TRAFFIC DRUM SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY DRUM DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- THE ENTIRE AREA OF FLUORESCENT ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
- THE SECTIONS OF DRUMS NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.

			THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.		 STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION		SUBMITTED BY: <i>Mark F. Makuch</i>	NAME/DATE/TIME: Mark F. Makuch, P.E. 2018.08.17 09:12:43-04'00'	CTDOT STANDARD SHEET	CONSTRUCTION SIGN SUPPORTS AND CHANNELIZING DEVICES	STANDARD SHEET TITLE:	STANDARD SHEET NO.:
3	8-2018	UPDATED SHEETING TYPE AND COLOR.		NOT TO SCALE			APPROVED BY: <i>Mark F. Carlini</i>	NAME/DATE/TIME: Mark F. Carlini, P.E. 2018.08.21 07:49:51-04'00'			OFFICE OF ENGINEERING	TR-1220_02
2	8-2015	UPDATED PER MUTCD AND FORM 816 JAN 2015 REVISION.										
1	2-2011	MINOR REVISIONS.										
REV.	DATE	REVISION DESCRIPTION	Plotted Date: 8/10/2018				Filename: TR-1220.02_3.2018.dgn	Model: TR-1220.02				

*ONLY STANDARD SHEETS MARKED WITH AN "✓" ARE IN THIS PROJECT #

**REVISED OR ADDED

✓*	SHEET NO.	TITLE	APPROVAL DATE**
	HW-286_01	DRAINAGE TRENCH EXCAVATION	7-15-20
	HW-506_01	ENDWALLS, SLOPE PAVED INLETS AND OUTLETS	1-26-12
	HW-506_02	TYPE "D-G" & "L" ENDWALLS	7-13-12
	HW-506_03	ENDWALLS FOR PIPE - ARCH	9-18-09
	HW-586_01	CATCH BASIN AND DROP INLET TYPES "C" AND "C-L"	7-15-20
	HW-586_02	CATCH BASIN TOPS (TYPES "C" AND "C-L") FOR DOUBLE GRATE TYPE I	7-15-20
	HW-586_03	CATCH BASIN TOPS (TYPES "C" AND "C-L") FOR DOUBLE GRATE TYPE II	7-15-20
	HW-586_04	PRECAST CATCH BASIN AND ROUND STRUCTURE	7-15-20
	HW-586_05	PRECAST CATCH BASIN TYPES FOR DOUBLE GRATE TYPE I	7-15-20
	HW-586_06	PRECAST CATCH BASIN TYPES FOR DOUBLE GRATE TYPE II	7-15-20
	HW-586_07	CATCH BASIN TOPS TYPE "C" AND "C-L"	7-15-20
	HW-586_08	CATCH BASIN FRAMES AND GRATES	7-15-20
	HW-586_09	CATCH BASIN LOCK DOWN TOPS	7-15-20
	HW-586_10a	MANHOLE FRAME AND COVER	7-15-20
	HW-586_10b	MANHOLE FRAME AND GRATE	7-15-20
	HW-586_10c	REINFORCED PRECAST CONCRETE MANHOLE	7-15-20
	HW-586_10d	MANHOLE NON-PRECAST CONCRETE UNIT	7-15-20
	HW-686_01	C.C.M. PIPE INSTALLATION	7-15-20
	HW-686_02	PIPE ENDS	7-15-20
	HW-751_01	UNDERDRAINS AND UNDERDRAIN OUTLETS	7-12-12
	HW-803_01a	PAVED APRONS	6-07-17
	HW-803_01b	PAVED DITCHES AND PAVED CHANNELS	6-07-17
	HW-811_01	CONCRETE CURBING	6-07-17
	HW-813_01	GRANITE STONE TRANSITION CURBING	7-24-13
	HW-813_02	STONE CURBING	6-07-17
	HW-815_01	BITUMINOUS CONCRETE CURBING	6-07-17
	HW-821_01a	TRANSITION - 45" (1145) F-SHAPE TO 45" (1145) VERTICAL SHAPE SHEET 1	1-26-12
	HW-821_01b	TRANSITION - 45" (1145) F-SHAPE TO 45" (1145) VERTICAL SHAPE SHEET 2	10-18-10
	HW-821_01c	TRANSITION - 45" (1145) F-SHAPE TO 45" (1145) VERTICAL SHAPE SHEET 3	1-26-12
	HW-821_02a	45" F-SHAPE PRECAST CONCRETE BARRIER CURB SHEET 1	1-27-20
	HW-821_02b	45" F-SHAPE PRECAST CONCRETE BARRIER CURB SHEET 2	1-27-20
	HW-821_03a	TRANSITION - 32" (813) JERSEY SHAPE TO 45" (1145) VERTICAL SHAPE SHEET 1	1-26-12

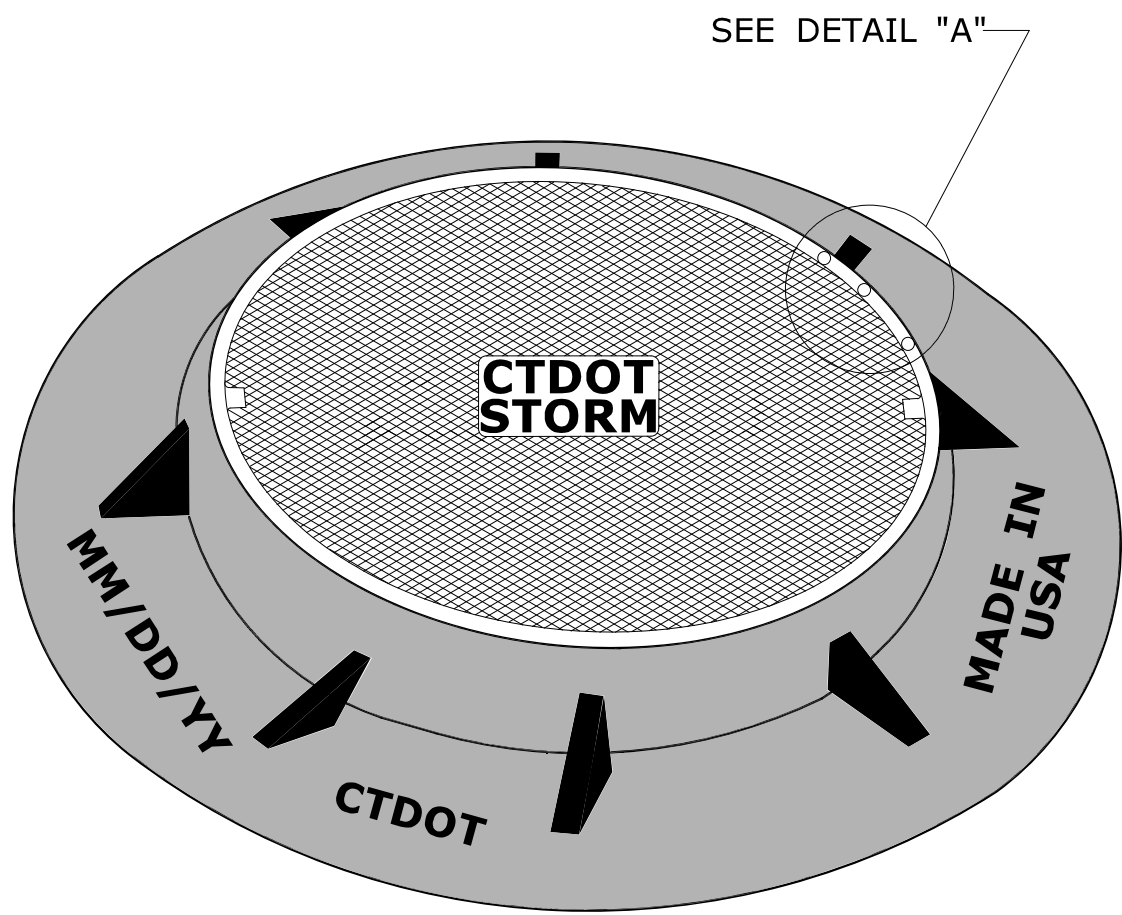
✓*	SHEET NO.	TITLE	APPROVAL DATE**
	HW-821_03b	TRANSITION - 32" (813) JERSEY SHAPE TO 45" (1145) VERTICAL SHAPE SHEET 2	10-18-10
	HW-821_03c	TRANSITION - 32" (813) JERSEY SHAPE TO 45" (1145) VERTICAL SHAPE SHEET 3	10-18-10
	HW-821_03d	TRANSITION - 32" (813) JERSEY SHAPE TO 45" (1145) VERTICAL SHAPE SHEET 4	10-18-10
	HW-821_03e	TRANSITION - 32" (813) JERSEY SHAPE TO 45" (1145) F-SHAPE	7-24-13
	HW-821_04a	MERRITT PARKWAY NARROW MEDIAN BARRIER	6-09-11
	HW-821_04b	MERRITT PARKWAY - 2' (610) WIDE MEDIAN BARRIER AND ROADSIDE BARRIER	7-24-13
	HW-821_05a	TRANSITION - 45" (1145) F-SHAPE TO 54" (1372) VERTICAL SHAPE SHEET 1	1-26-12
	HW-821_05b	TRANSITION - 45" (1145) F-SHAPE TO 54" (1372) VERTICAL SHAPE SHEET 2	1-26-12
	HW-821_06	54" (1372) VERTICAL SHAPE BARRIER	2-06-12
	HW-821_07	MISCELLANEOUS DETAILS FOR BARRIER TRANSITIONS	7-12-12
	HW-821_08a	F-SHAPE CONC. BARRIER CURB (21"x45") TRANSITION FOR THRIE-BEAM	1-09-20
	HW-821_08b	F-SHAPE CONC. BARRIER CURB (21"x45") TRANSITION FOR THRIE-BEAM - REINF.	1-09-20
	HW-821_09a	SINGLE SLOPE CONC. BARRIER CURB (20"x42") TRANS. FOR THRIE-BEAM	1-09-20
	HW-821_09b	SINGLE SLOPE CONC. BARRIER CURB (20"x42") TRANS. FOR THRIE-BEAM - REINF.	1-09-20
	HW-821_10a	VERTICAL FACE CONC. (21"x54") TRANSITION FOR THRIE-BEAM	1-09-20
	HW-821_10b	VERTICAL FACE CONC. (21"x54") TRANSITION FOR THRIE-BEAM - REINF.	1-09-20
	HW-821_11a	42" SINGLE SLOPE PRECAST CONCRETE BARRIER CURB -SHEET 1	1-27-20
	HW-821_11b	42" SINGLE SLOPE PRECAST CONCRETE BARRIER CURB -SHEET 2	1-27-20
	HW-822_01	TEMPORARY PRECAST CONCRETE BARRIER CURB	7-24-13
	HW-905_01	STONE WALL FENCE	1-25-19
	HW-906_01	WIRE FENCE	1-25-19
	HW-910_01	W-BEAM METAL BEAM RAIL HARDWARE	6-09-11
	HW-910_02	METAL BEAM RAIL (TYPE R-B 350) GUIDERAIL	6-09-11
	HW-910_03	METAL BEAM RAIL (TYPE MD-B 350) GUIDERAIL	6-09-11
	HW-910_04	METAL BEAM RAIL (TYPE R-B 350) SYSTEMS 5, 5A, & 6	6-09-11
	HW-910_05	METAL BEAM RAIL R-B 350 SPAN TYPE I, II, III SECTIONS	7-24-13
	HW-910_06	R-B 350 BRIDGE ATTACHMENT SAFETY SHAPE PARAPET	6-09-11
	HW-910_07	R-B 350 BRIDGE ATTACHMENT VERTICAL SHAPE PARAPET	1-25-19
	HW-910_08	R-B 350 BRIDGE ATTACHMENT TRAILING END	6-09-11
	HW-910_09a	MISCELLANEOUS GUIDERAIL TRANSITIONS SHEET 1	1-26-12
	HW-910_09b	MISCELLANEOUS GUIDERAIL TRANSITIONS SHEET 2	7-25-12
	HW-910_10	METAL BEAM RAIL 8" (203) X 6" (152) BOX BEAM	7-24-13
	HW-910_11	CURVED GUIDERAIL TREATMENT DETAIL	7-25-12

*ONLY STANDARD SHEETS MARKED WITH AN "✓" ARE IN THIS PROJECT #

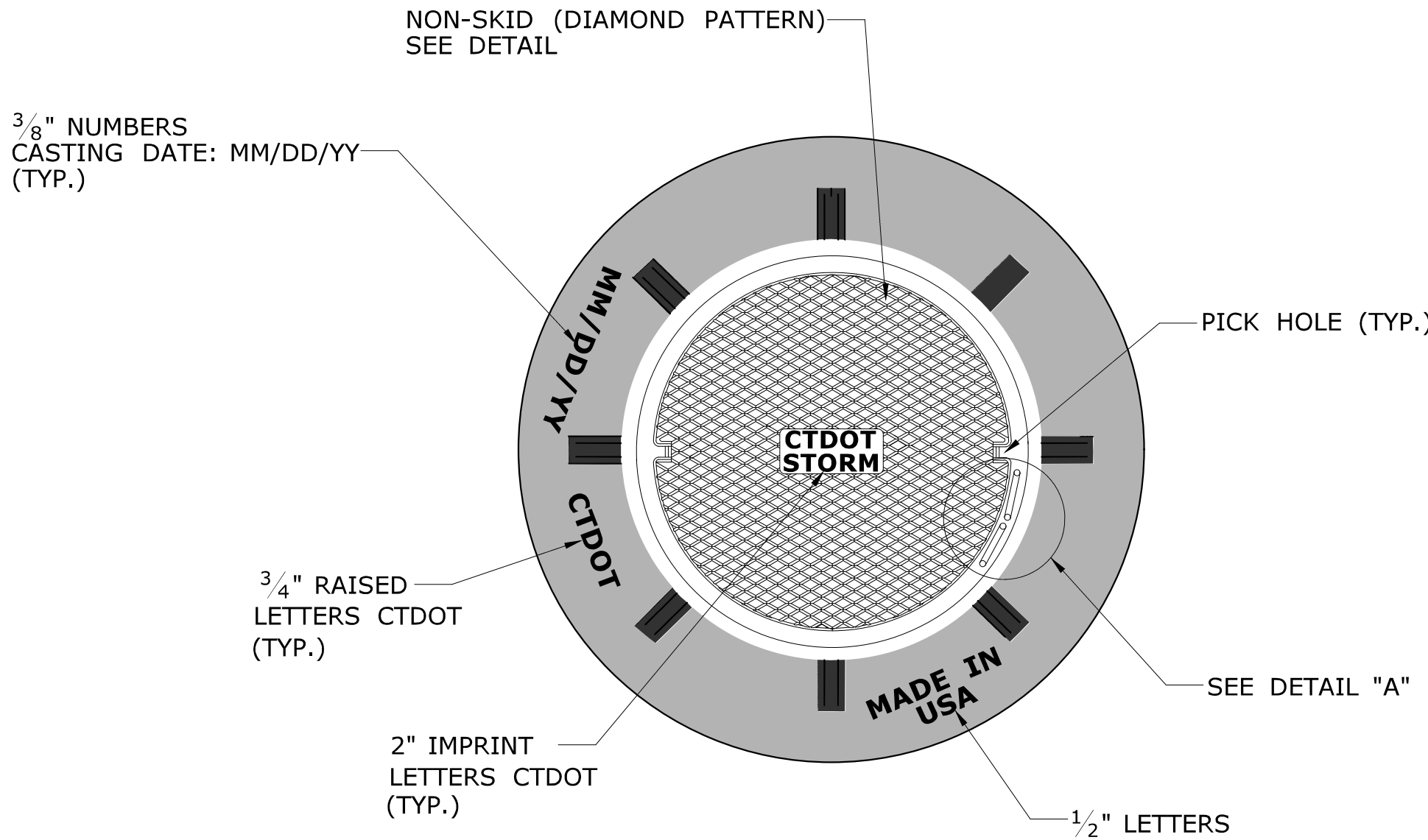
**REVISED OR ADDED

✓*	SHEET NO.	TITLE	APPROVAL DATE**	✓*	SHEET NO.	TITLE	APPROVAL DATE**
	HW-910_12a	MERRITT PARKWAY GUIDERAIL LEADING END ATTACHMENTS AND SYSTEMS 2&3	7-24-13		HW-921_01	DRIVEWAY RAMPS AND SIDEWALKS	6-07-17
	HW-910_12b	MERRITT PARKWAY GUIDERAIL HARDWARE DETAILS	7-24-13		HW-949_01a	LANDSCAPE PLANTING	6-15-19
	HW-910_12c	MERRITT PARKWAY GUIDERAIL TRAILING END ATTACHMENTS	7-24-13		HW-949_01b	TREE STAKING	6-15-19
	HW-910_12d	MERRITT PARKWAY MEDIAN GUIDERAIL AND END ANCHOR	6-09-11		HW-1800_01	GRADING PLAN FOR IMPACT ATTENUATION SYSTEMS (FLARED AND TANGENTIAL)	1-25-19
	HW-910_13a	THRIE-BEAM METAL BEAM RAIL HARDWARE	7-24-13		HW-1800_02	GRADING PLAN FOR IMPACT ATTENUATION SYSTEMS (MEDIAN/GORE)	1-25-20
	HW-910_13b	THRIE-BEAM TRANSITIONS	7-24-13				
	HW-910_14a	THRIE-BEAM 350 BRIDGE ATTACHMENT	6-09-11				
	HW-910_14b	THRIE-BEAM 350 GUIDERAIL TRANSITION TO R-B 350 GUIDERAIL	6-09-11				
	HW-910_15	MD-B 350 MEDIAN BARRIER SAFETY SHAPE ATTACHMENT TYPE I	6-09-11				
	HW-910_16	MD-B 350 MEDIAN BARRIER SAFETY SHAPE ATTACHMENT TYPE II	6-09-11				
	HW-910_17	R-B TERMINAL SECTION	7-24-13				
	HW-910_18	METAL BEAM RAIL (TYPE MD-I) GUIDERAIL	10-18-10				
	HW-910_19a	METAL BEAM RAIL (MODIFIED TYPE R-I) AND END ANCHORAGE TYPE I	7-24-13				
	HW-910_19b	METAL BEAM RAIL (MODIFIED TYPE R-I) AND END ANCHORAGE TYPE II	7-24-13				
	HW-910_19c	METAL BEAM RAIL (MODIFIED TYPE R-I) SYSTEMS 2 AND 3	7-24-13				
	HW-910_20	MASH W-BEAM HARDWARE	1-05-18				
	HW-910_21	METAL BEAM RAIL (R-B MASH) GUIDERAIL	1-25-19				
	HW-910_22	METAL BEAM RAIL (MD-B MASH) GUIDERAIL	1-05-18				
	HW-910_23	METAL BEAM RAIL (R-B MASH) HALF & QUARTER POST SPACING GUIDERAIL	1-05-18				
	HW-910_24	METAL BEAM RAIL SPAN SECTION TYPES II AND III	1-05-18				
	HW-910_25	METAL BEAM RAIL TRANSITION 350 TO MASH	1-05-18				
	HW-910_26	THRIE-BEAM ATTACHMENT HARDWARE	1-09-20				
	HW-910_27	THRIE-BEAM ATTACHMENT	1-09-20				
	HW-911_01	R-B END ANCHORAGE TYPE I AND II	1-25-19				
	HW-911_02	MD-B END ANCHORAGE TYPE I	1-05-18				
	HW-911_03	ANCHOR IN EARTH CUT SLOPE & ANCHOR IN ROCK CUT SLOPE	10-18-10				
	HW-911_05	MERRITT PARKWAY GUIDERAIL END ANCHORS	7-24-13				
	HW-913_01a	CHAIN LINK FENCE	5-06-19				
	HW-913_01b	CHAIN LINK FENCE HARDWARE	5-06-19				
	HW-913_02	CHAIN LINK FENCE GATES	5-06-19				
	HW-918_01a	THREE CABLE GUIDERAIL (I-BEAM POSTS) SHEET 1	7-24-13				
	HW-918_01b	THREE CABLE GUIDERAIL (I-BEAM POSTS) SHEET 2	1-26-12				
	HW-918_01c	THREE CABLE GUIDERAIL (I-BEAM POSTS) SHEET 3	7-24-13				

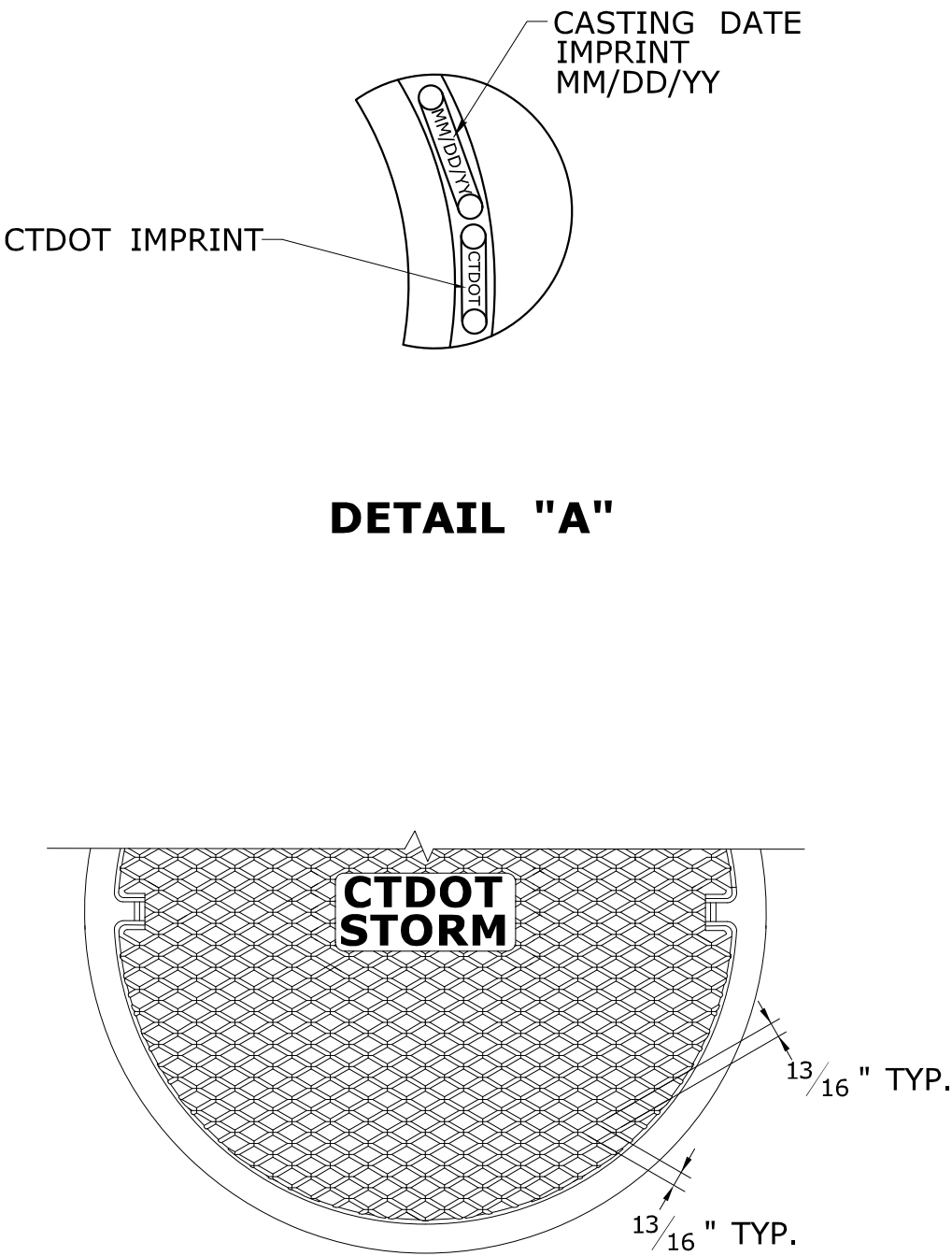
GENERAL NOTES:
1. ALL DIMENSIONS ARE SUBJECT TO MANUFACTURING TOLERANCES.



MANHOLE FRAME AND COVER

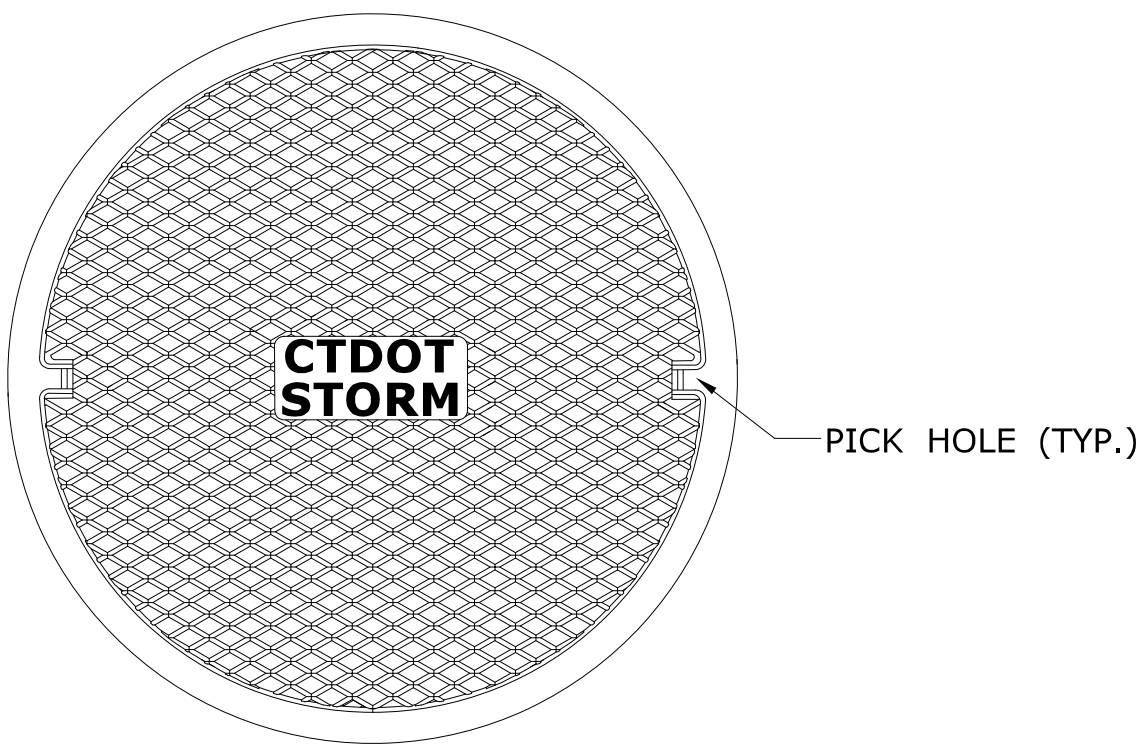


PLAN

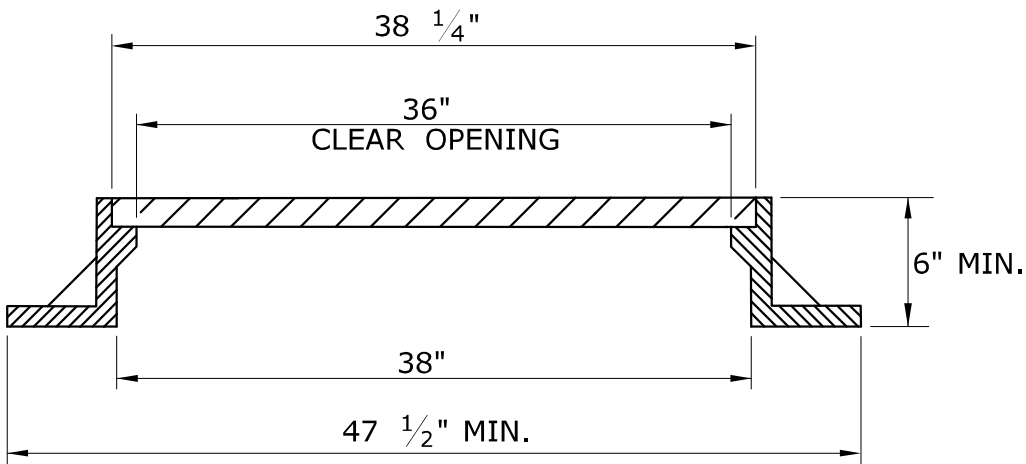


DETAIL "A"

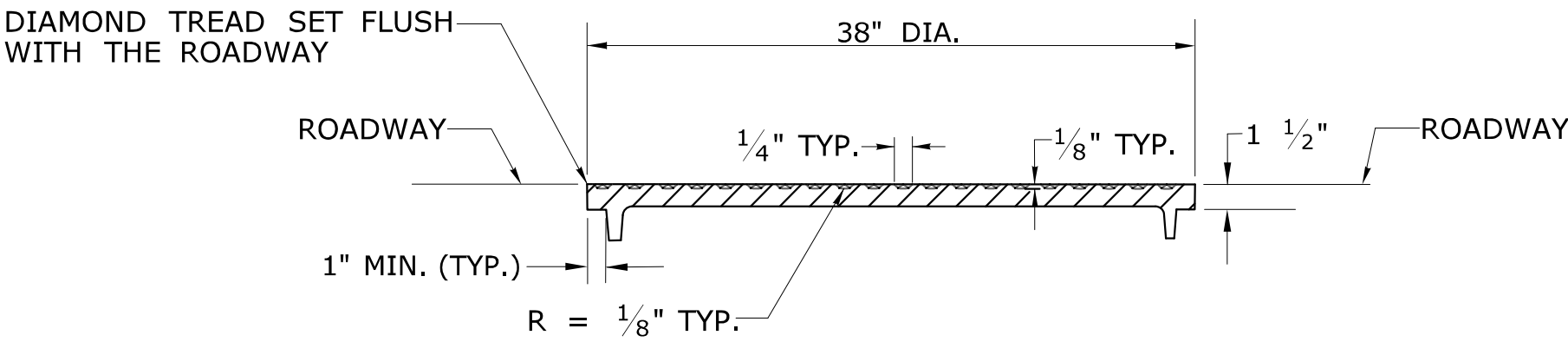
DIAMOND PATTERN PLAN



MANHOLE COVER PLAN

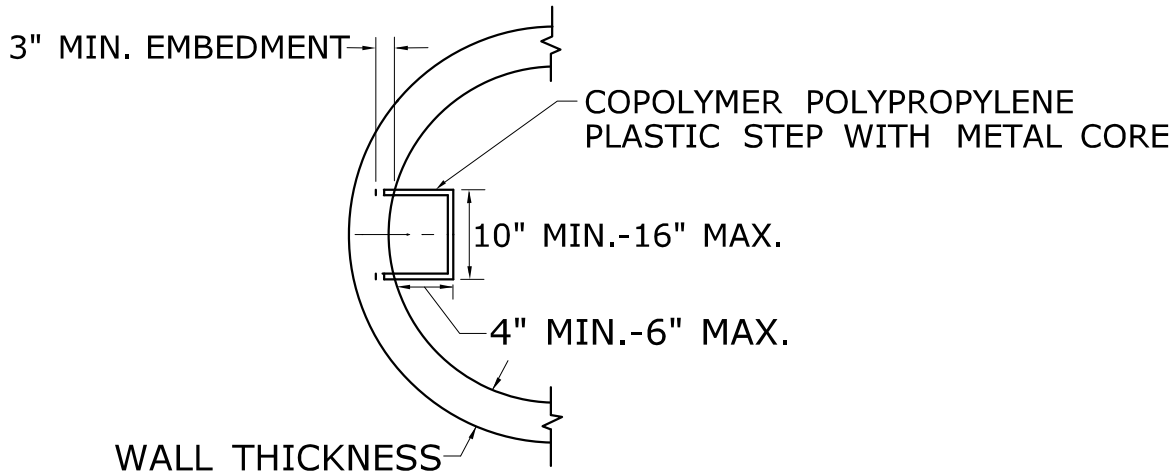


MANHOLE FRAME AND COVER

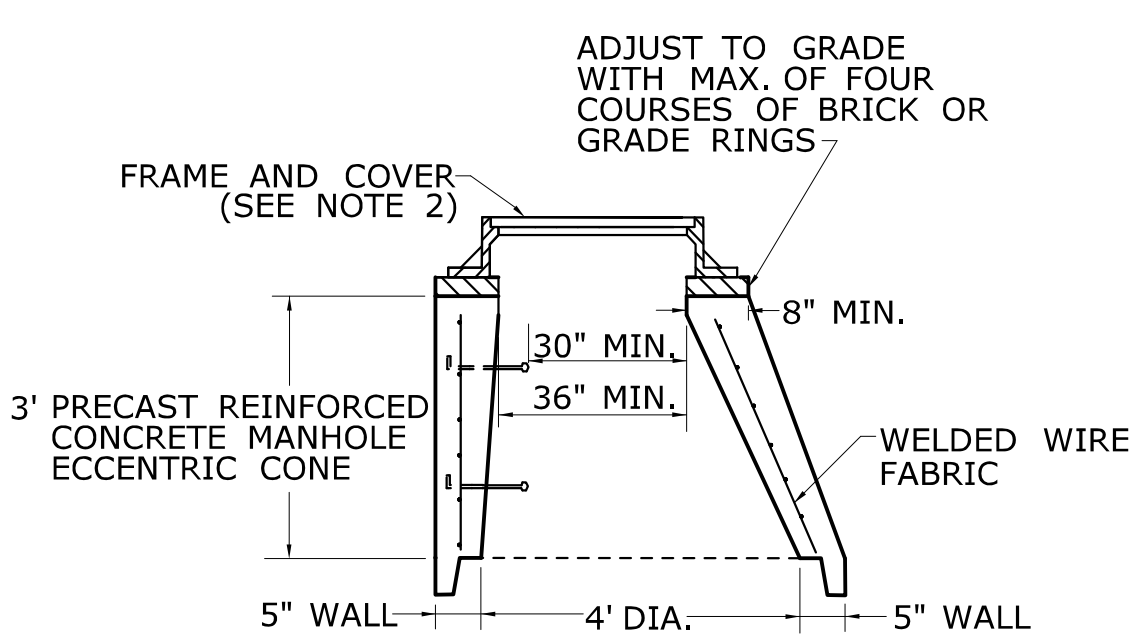


MANHOLE COVER WITH DIAMOND PATTERN

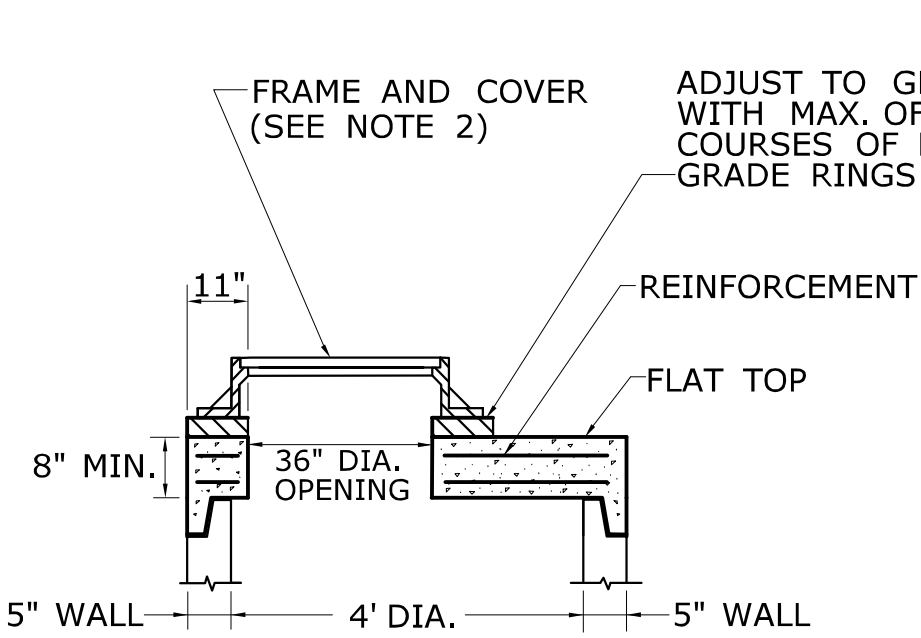
- GENERAL NOTES:**
1. ALL DIMENSIONS ARE SUBJECT TO MANUFACTURERS TOLERANCE.
 2. SEE SHEET NO. HW-586_10a, OR HW-586_10b FOR MANHOLE FRAME, GRATE AND COVER DETAIL.



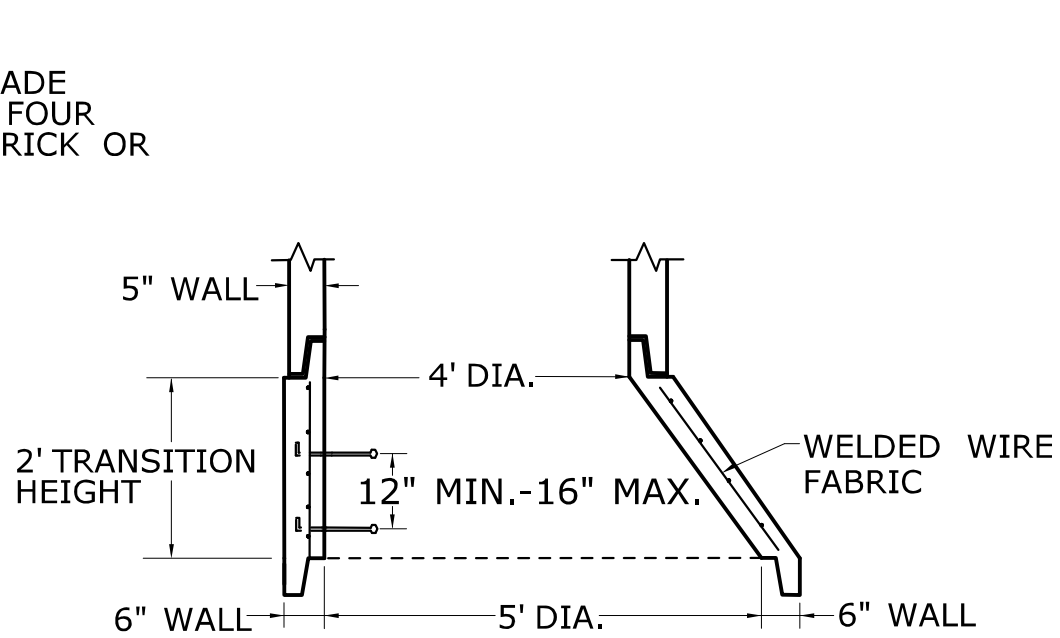
STEP DETAIL



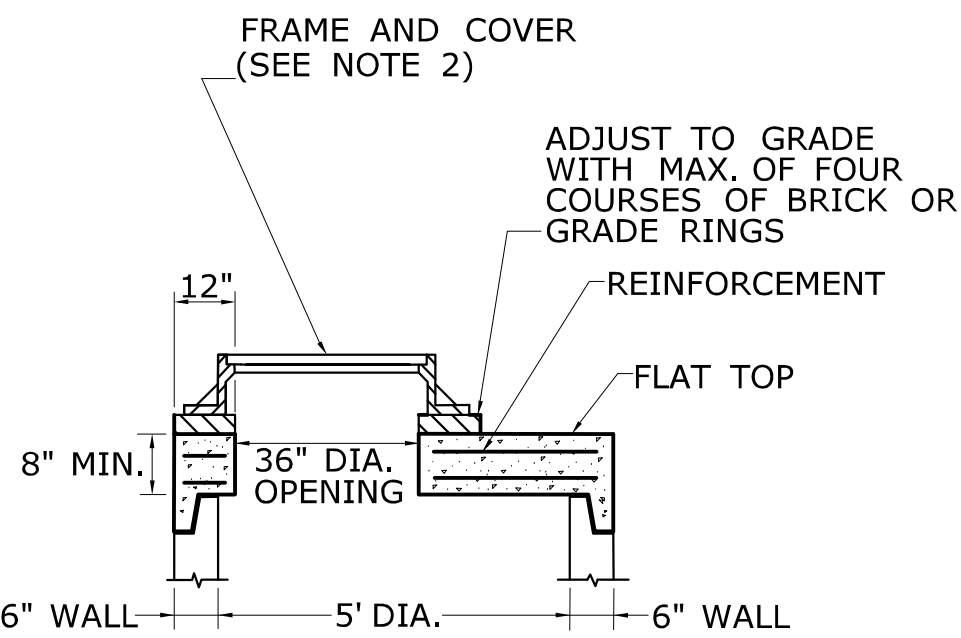
ECCENTRIC CONE SECTION



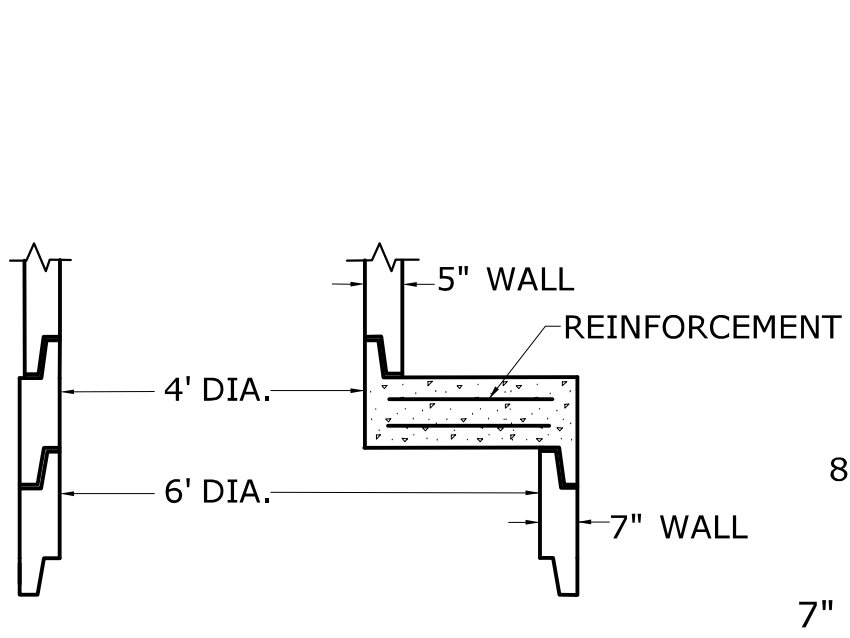
FLAT SLAB TOP FOR RISER SECTION



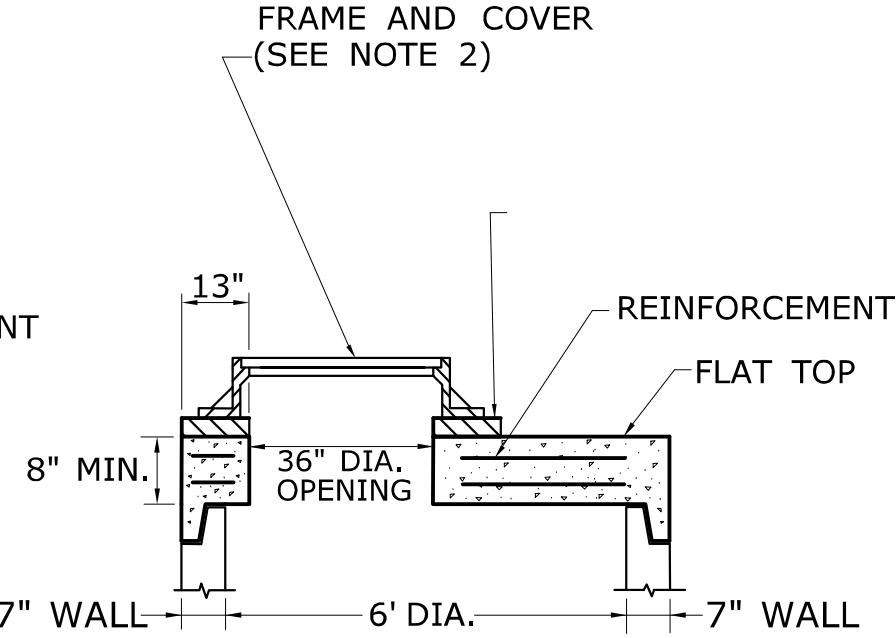
TRANSITION SECTION



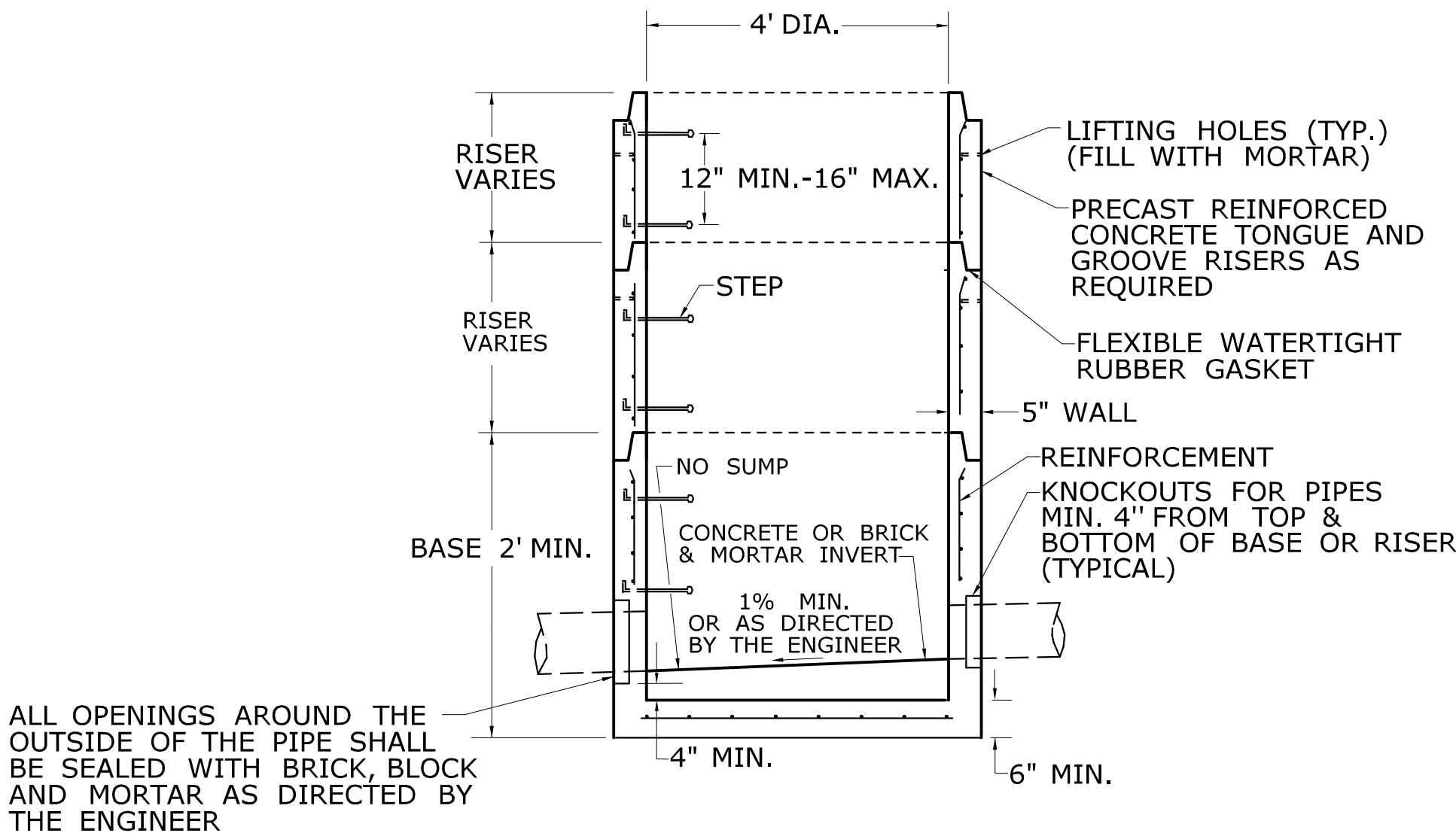
FLAT SLAB TOP FOR RISER SECTION



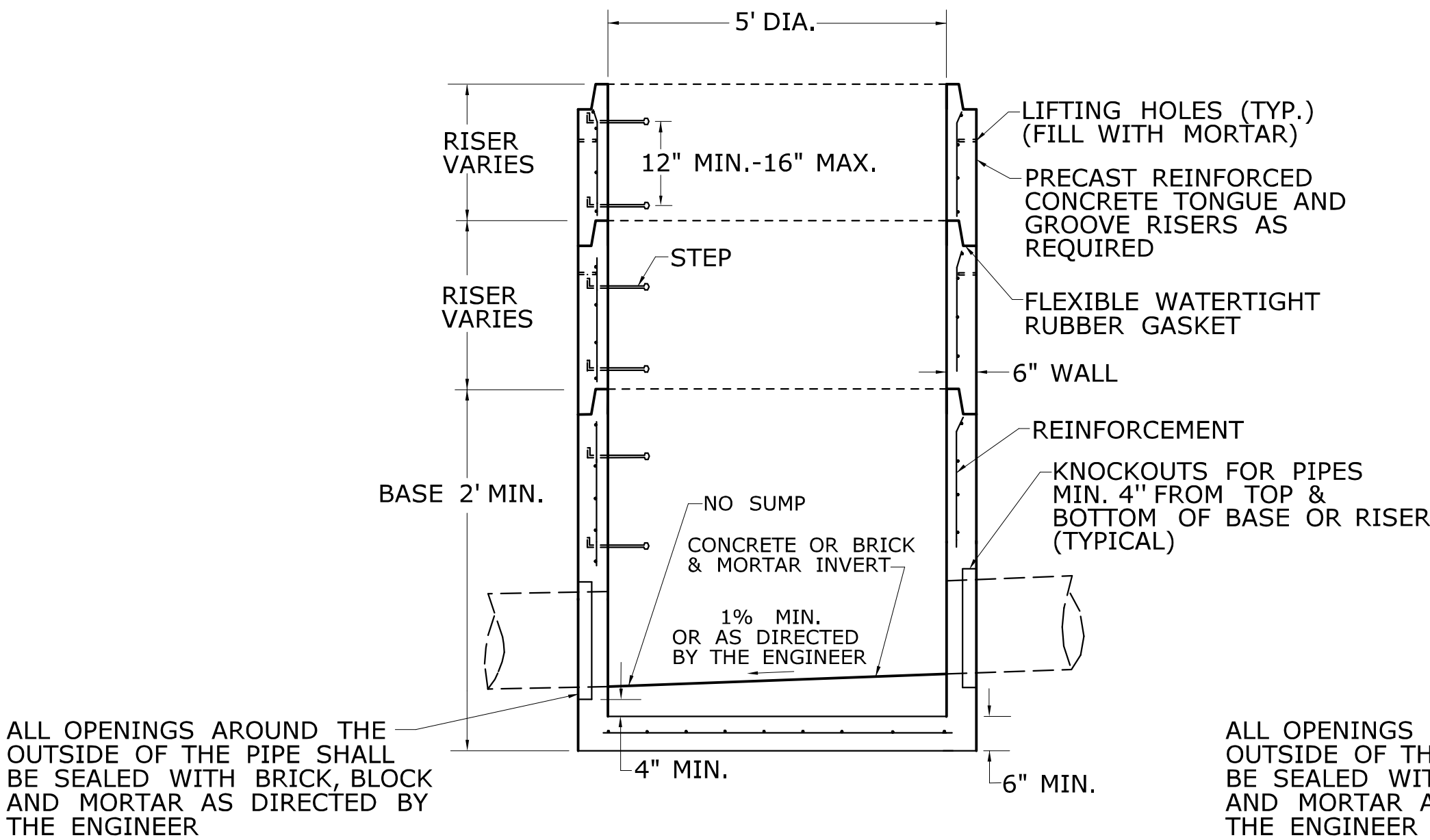
REDUCER SECTION



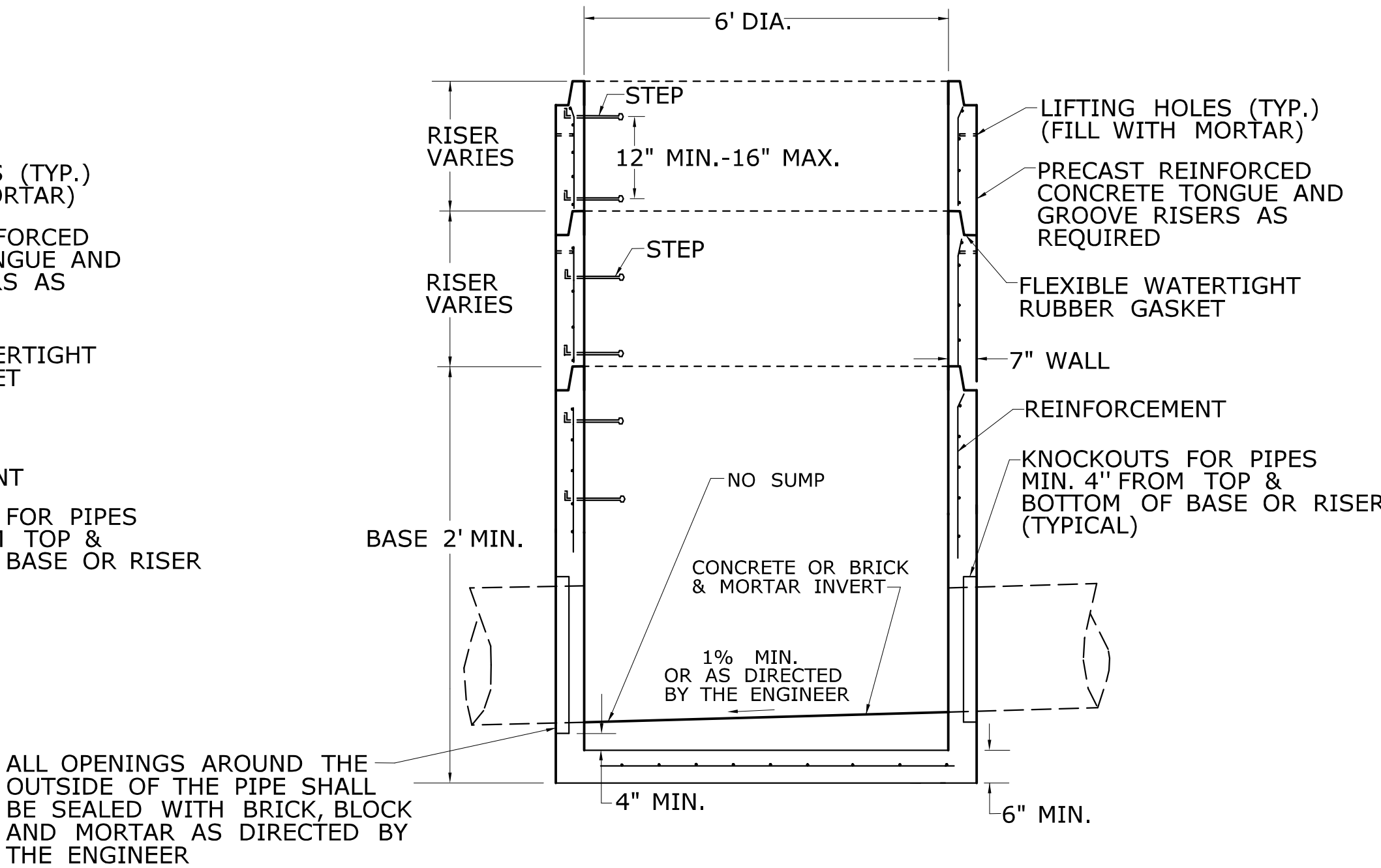
FLAT SLAB TOP FOR RISER SECTION



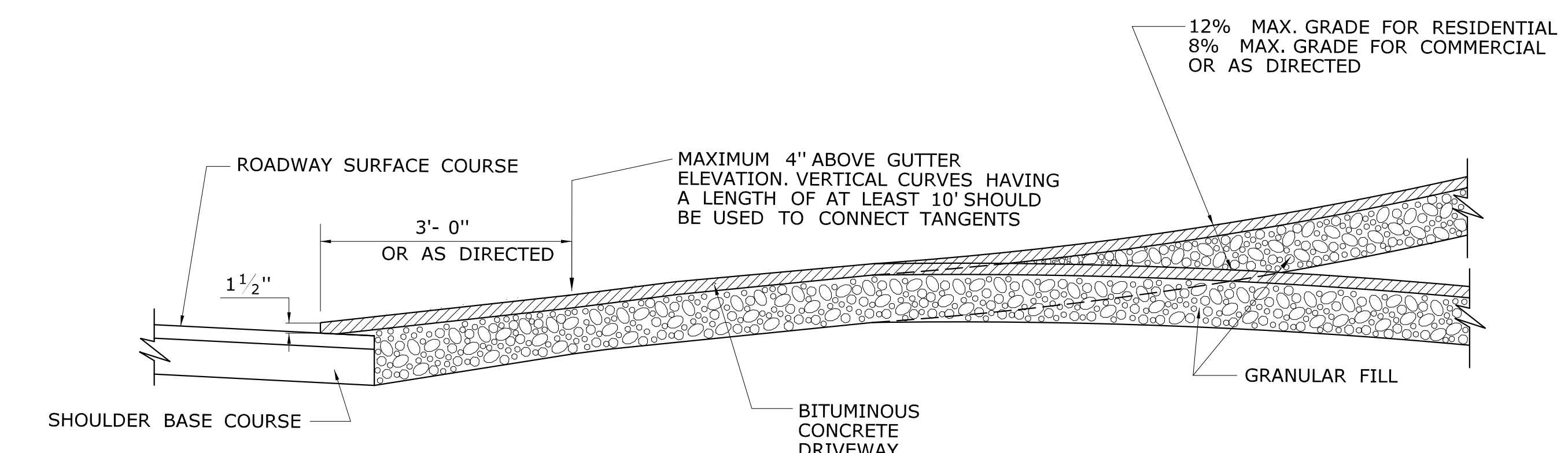
SECTION 4' DIAMETER REINFORCED PRECAST CONCRETE MANHOLE



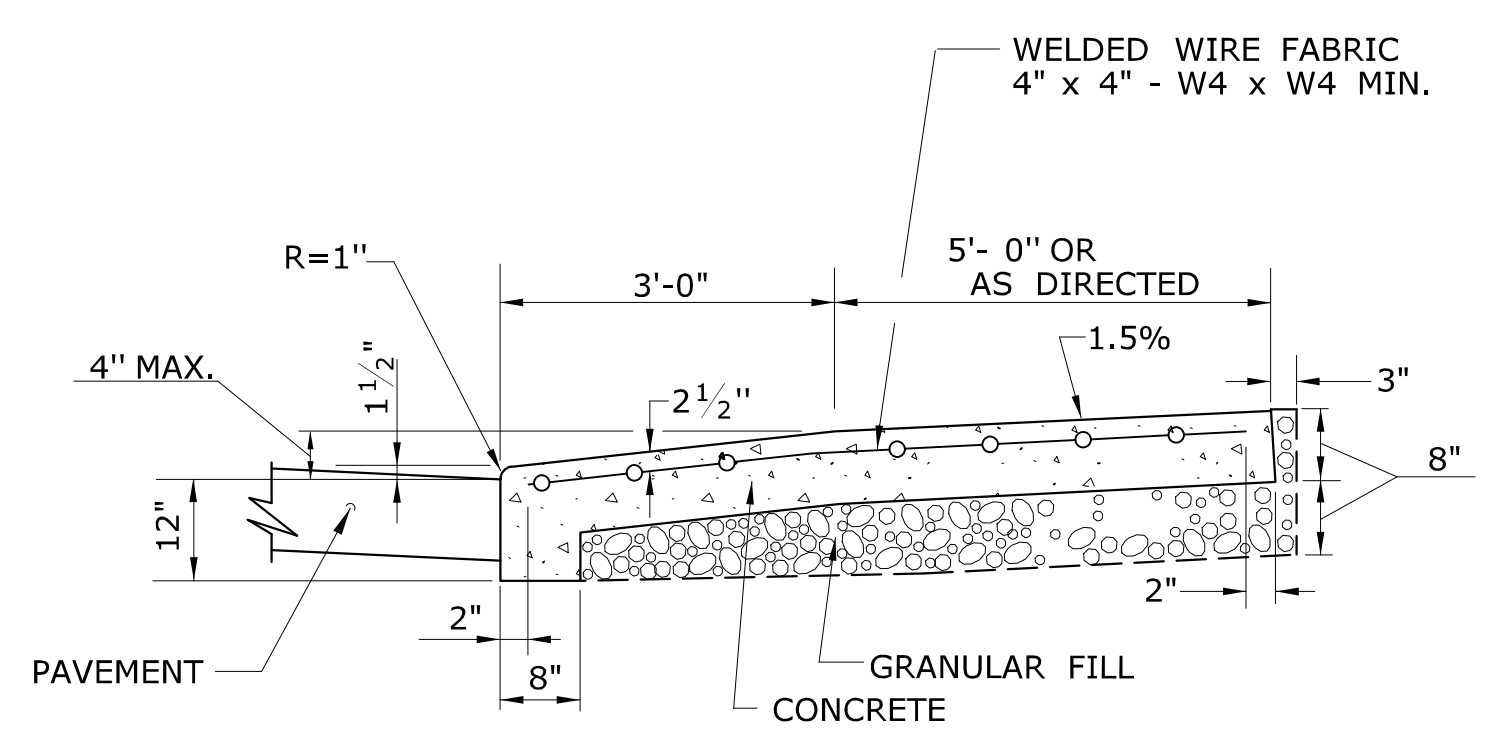
SECTION 5' DIAMETER REINFORCED PRECAST CONCRETE MANHOLE



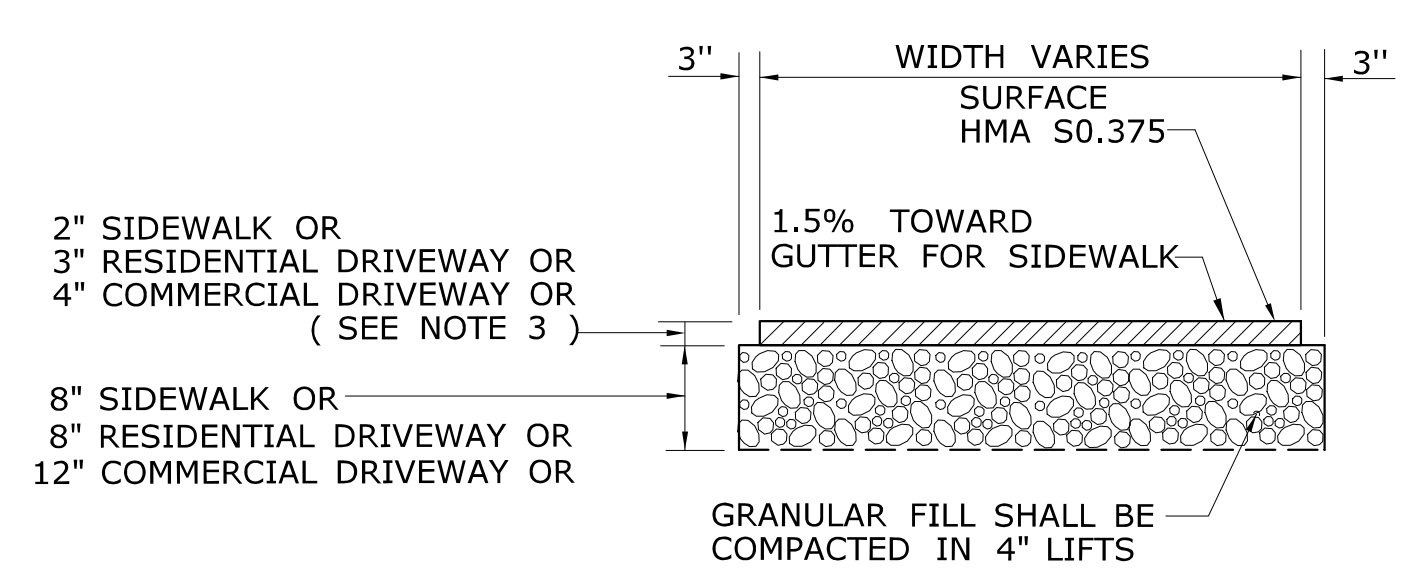
SECTION 6' DIAMETER REINFORCED PRECAST CONCRETE MANHOLE



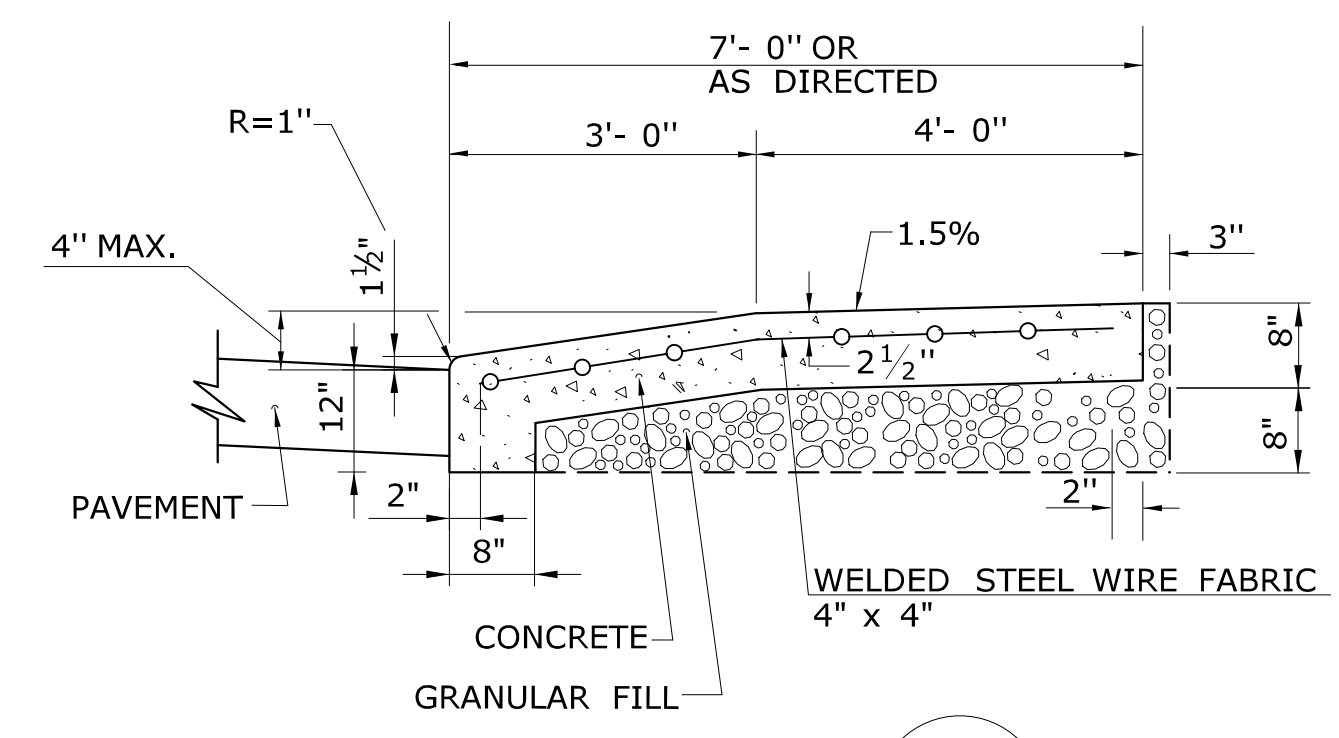
SECTION A



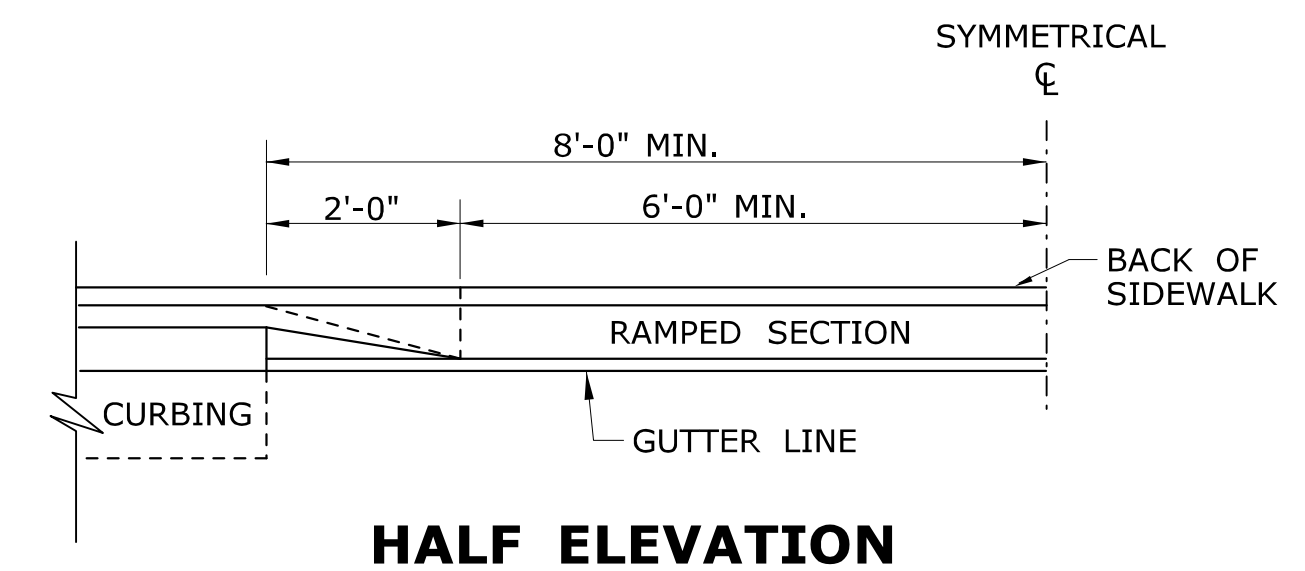
SECTION C



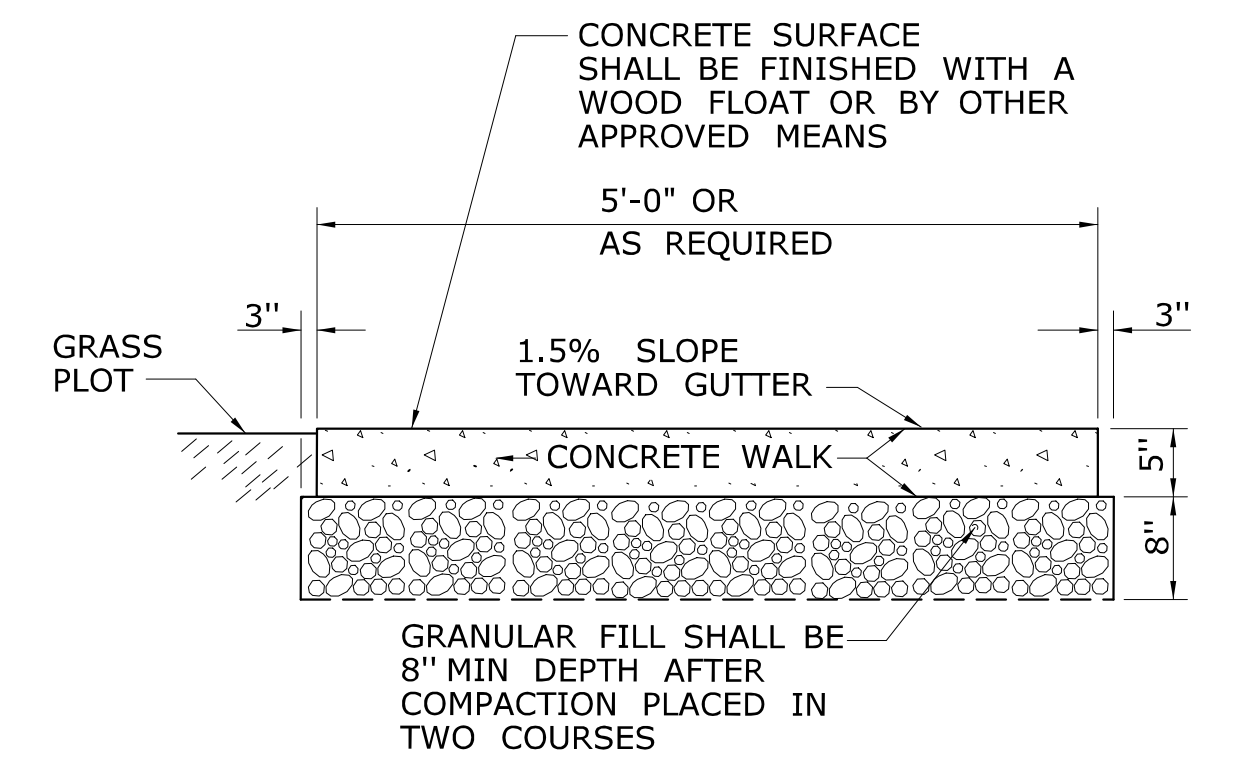
TYPICAL SECTION
BITUMINOUS CONCRETE
SIDEWALK AND DRIVEWAY



SECTION B

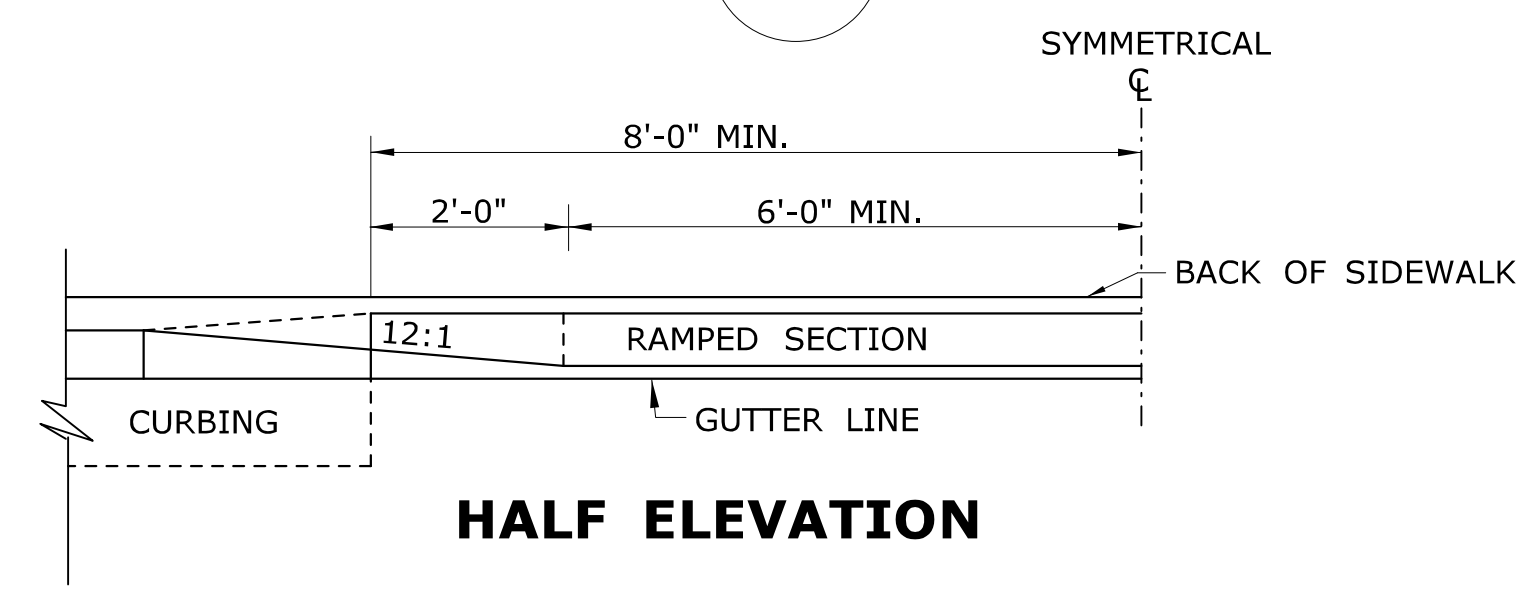


HALF ELEVATION

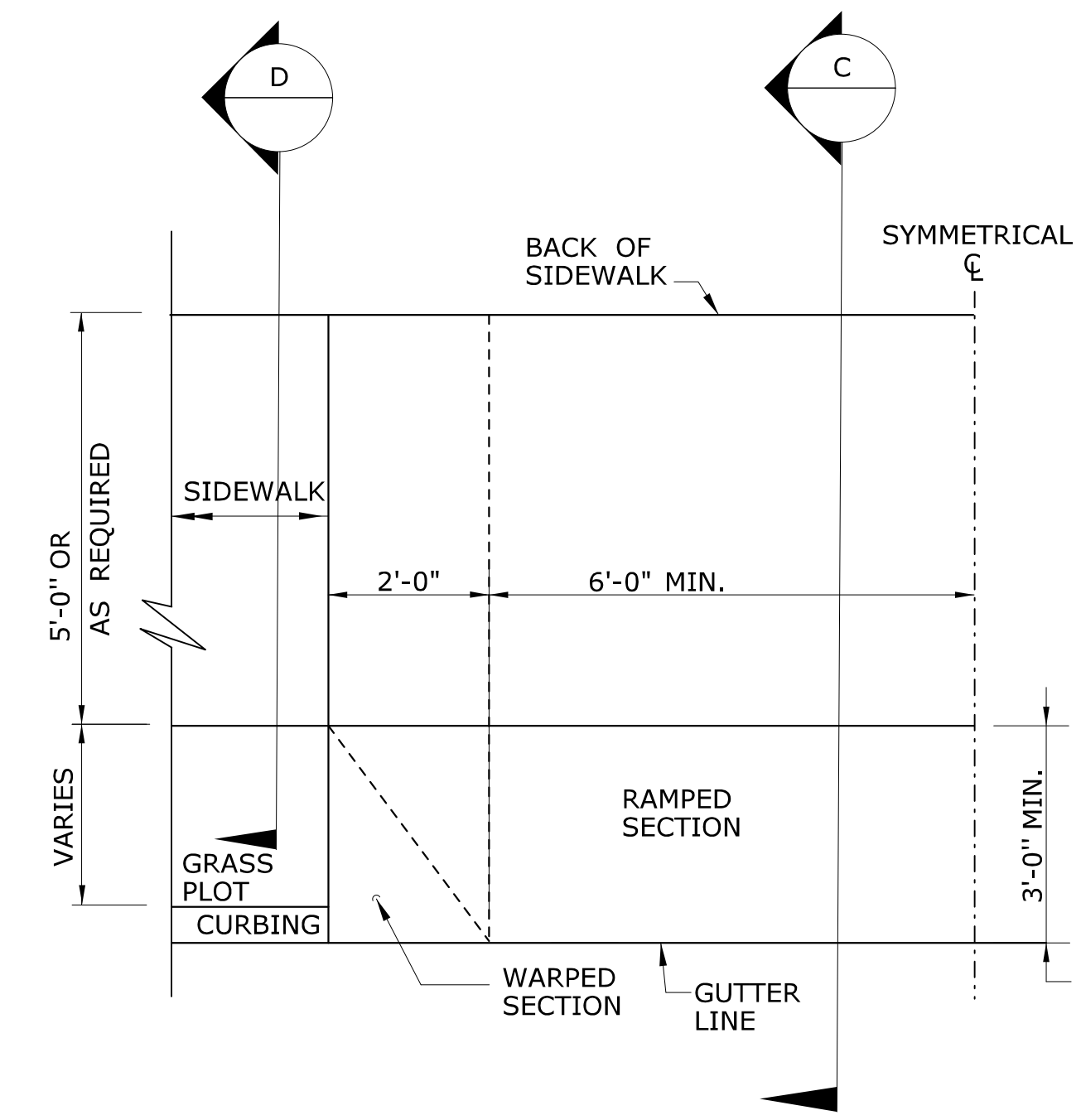


SECTION D

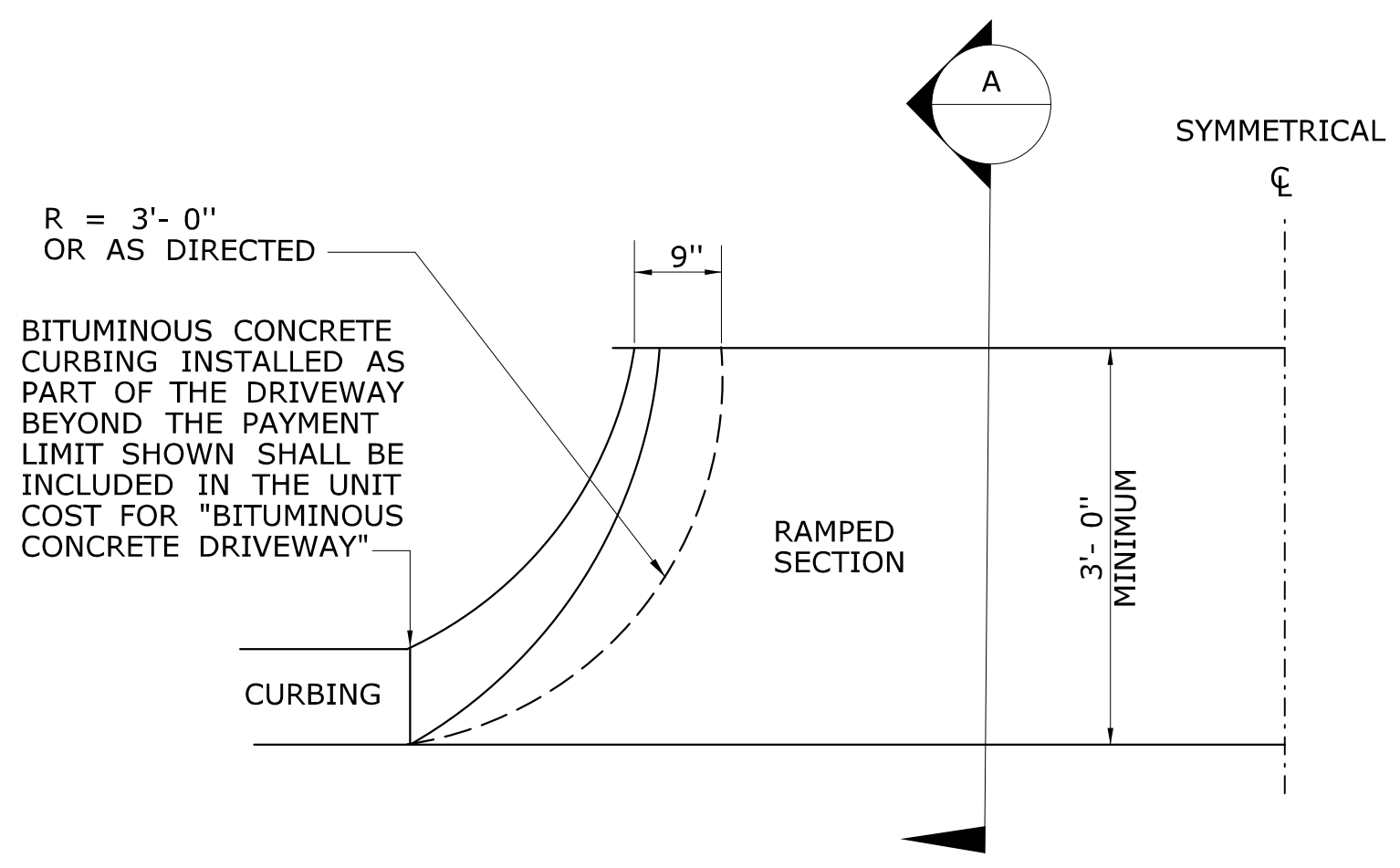
5' WIDE CONCRETE
SIDEWALK WITH GRASS PLOT



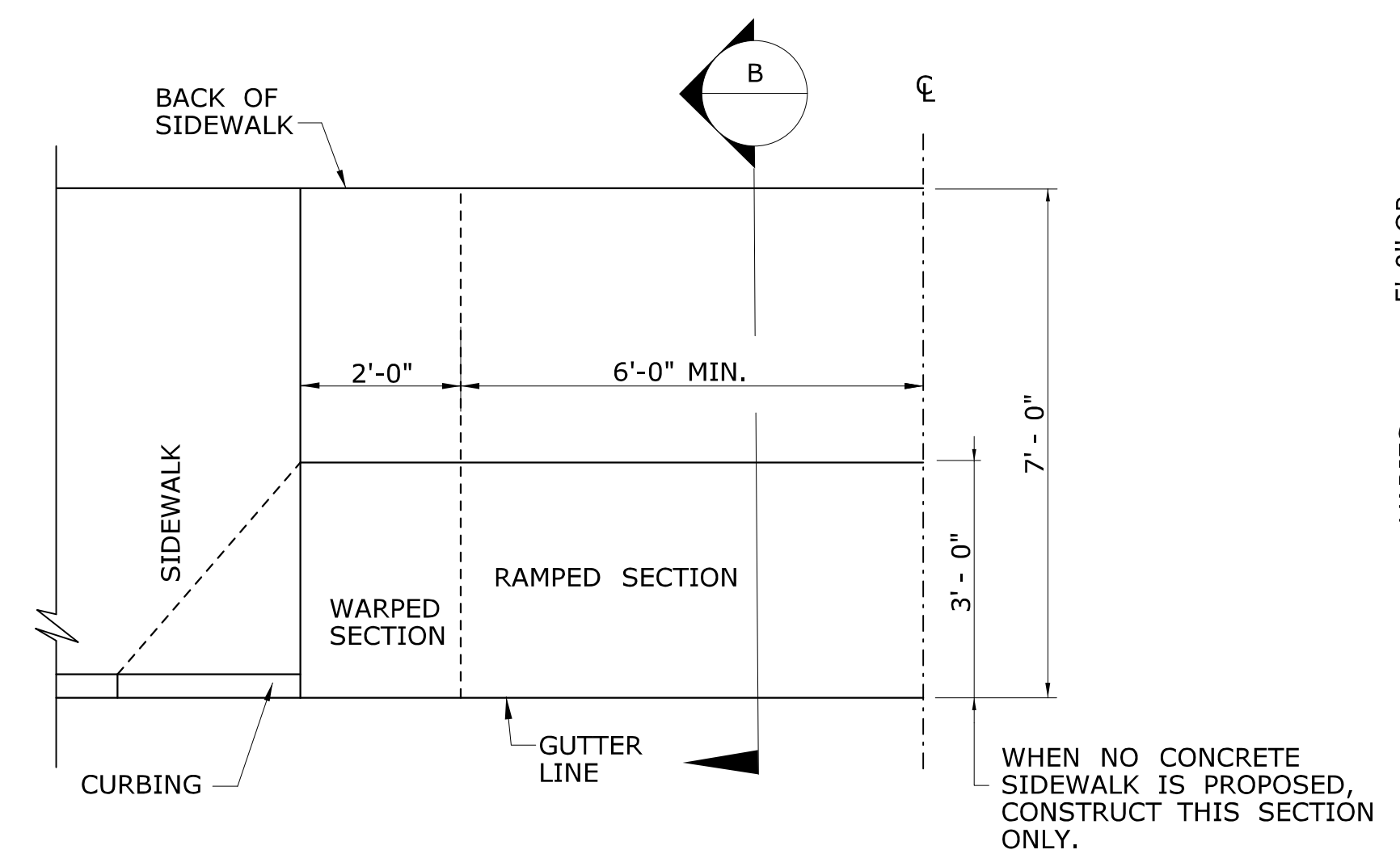
HALF ELEVATION



HALF PLAN OF
CONCRETE DRIVEWAY RAMP WHERE
CURB IS SEPARATED FROM
SIDEWALK BY GRASS PLOT



HALF BITUMINOUS CONCRETE
DRIVEWAY PLAN



HALF PLAN OF
CONCRETE DRIVEWAY RAMP WHERE
SIDEWALK ADJOINS CURBING

GENERAL NOTES:

1. DRIVEWAY ENTRANCE SHALL BE A MINIMUM OF 12' WIDE, EXCLUDING CURBING WHEN PRESENT.
2. WELDED WIRE FABRIC MATS WITH REINFORCING AT CLOSER SPACING MAY BE USED.
3. SURFACE HMA S0.375 TO BE PLACED IN TWO EQUAL LIFTS FOR BOTH RESIDENTIAL AND COMMERCIAL DRIVEWAYS.